



Approved College Park US I Corridor Sector Plan And Sectional Map Amendment

April 2002



ABSTRACT

TITLE: Approved College Park US 1 Corridor Sector Plan and Sectional Map Amendment

AUTHOR: The Maryland-National Capital Park and Planning Commission

SUBJECT: Approved Sector Plan and Sectional Map Amendment for a portion of Planning Area 66 in Prince George's County

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ABSTRACT: This sector plan amends the 1989 *Approved Master Plan* and the 1990 *Adopted Sectional Map Amendment for Langley Park, College Park and Greenbelt (Planning Areas 65, 66 and 67)* for the sector area only. The plan also amends the 1982 *Master Plan of Transportation* and the 1975 *Countywide Trails Plan*. It was developed by the commission with the assistance of the College Park US 1 Corridor Advisory Planning Group. This document describes existing plans and policies, and analyzes land use, environment, zoning, population, housing, economic conditions, and impacts on public facilities. The plan incorporates Smart Growth planning principles and is in harmony with the concepts put forth in the *Commission 2000 Biennial Growth Policy Plan* report (July 2000) and the Biennial Growth Policy Plan adopted by the County Council in November 2000. It also implements concepts and recommendations contained within the City of College Park's 1995 *Comprehensive Plan*, which ended a three-year planning process to determine the city's future and desired development character for the US 1 corridor and town center. The document establishes a flexible land use and zoning concept that can respond to changing real estate market conditions. A Development District Overlay Zone (DDOZ) containing design standards and a table of permitted and prohibited uses are incorporated as part of the sectional map amendment (SMA). Together the DDOZ and SMA implement the sector plan's land use and development character recommendations. An economic development strategy offers guidance in implementation of the desired land use and development character recommendations.



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April 2002



**The Maryland-National Capital Park & Planning Commission
Prince George's County Planning Department
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Upper Marlboro, Maryland 20772
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The Commission has three major functions:

- The preparation, adoption, and, from time to time, amendment or extension of the General Plan for the physical development of the Maryland-Washington Regional District;
- The acquisition, development, operation, and maintenance of a public park system; and
- In Prince George's County only, the operation of the entire County public recreation program.

The Commission operates in each county through a Planning Board appointed by and responsible to the County government. All local plans, recommendations on zoning amendments, administration of subdivision regulations, and general administration of parks are responsibilities of the Planning Boards.

The Prince George's County Department of Planning (M-NCPPC):

- Our mission is to help preserve, protect and manage the County's resources by providing the highest quality planning services and growth management guidance and by facilitating effective intergovernmental and citizen involvement through education and technical assistance.
- Our vision is to be a model planning department comprised of responsive and respected staff who provide superior planning and technical services and work cooperatively with decision-makers, citizens and other agencies to continuously improve development quality and the environment and act as a catalyst for positive change.

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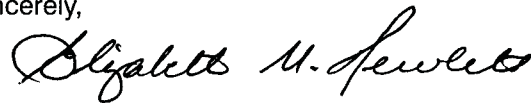
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FOREWORD

The Prince George's County Planning Board is pleased to make available the Approved Sector Plan and Sectional Map Amendment (SMA) for the College Park US 1 Corridor (part of Planning Area 66). The plan contains recommendations concerning the future of downtown College Park's town center and the US 1 commercial corridor, including the East Campus of The University of Maryland. Contained also in the plan is an Economic Development Strategy that addresses possible programs, projects, strategies and actions that are important to successful implementation of the sector plan's land use recommendations by the community-at-large.

A joint Prince George's County Planning Board/Prince George's District Council (County Council) public hearing on the May 2001 Preliminary Sector Plan was held on June 11, 2001. The Planning Board adopted the sector plan on October 18, 2001, and transmitted recommendations to the District Council. A second joint public hearing was held on February 5, 2002, to consider amendments to the sector plan proposed by both the Planning Board and District Council. Both public hearings were advertised to everyone who owns property or runs a business in the sector plan area. All comments and recommendations presented at the public hearings became matters of public record and were summarized and reviewed by both the Planning Board and District Council in their deliberations prior to their separate actions on the sector plan and SMA. Following the second public hearing, the Planning Board transmitted comments on the amendments to the District Council on May 21, 2002. The District Council approved the sector plan and SMA on April 30, 2002.

Sincerely,



Elizabeth M. Hewlett
Chairman
Prince George's County Planning Board

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PRINCE GEORGE'S COUNTY ECONOMIC DEVELOPMENT CORPORATION

- Rosalyn Clemens

The planning group's recommendations were strengthened and modified due to the input of many individuals who participated in community meetings.



Introduction



PLAN HIGHLIGHTS

THIS PLAN . . .

- Implements the community’s vision of transforming US 1 into a gateway boulevard with a main street and town center, containing mixed-use development in a pedestrian-friendly and aesthetically appealing environment.
- Recognizes planning efforts by the State Highway Administration to reconstruct US 1 to enhance vehicular, transit, bicycle and pedestrian safety; and create an urban boulevard with landscaped medians and shoulders and other roadway and streetscape enhancements.
- Identifies roadway improvements to accommodate redevelopment along the corridor and ensure a high quality of life in College Park.
- Defines the development character desired in six areas along the corridor.
- Encourages expansion of the town center to include the East Campus area of The University of Maryland (located in the southeastern corner of US 1 and Paint Branch Parkway), linking to the College Park-University of Maryland Metro Station and including the “Knox Boxes” apartment area located west of US 1 along Knox Road.
- Encourages multifamily housing near the campus as a means of reducing commuter traffic and spurring retail and office development.
- Accommodates potential changes in market conditions by permitting a wide range and mix of uses.
- Encourages the preservation and enhancement of the Paint Branch Stream Valley as a greenway corridor linking the university to the Metro station and to residential, employment and recreational areas.
- Establishes a Development District Overlay Zone (DDOZ) to provide a consistent design framework for all new development, while providing regulatory flexibility through site plan review.
- Establishes a Mixed-Use-Infill (M-U-I) Zone to permit a mix of residential and commercial uses in areas recommended by the sector plan.
- Provides an illustrative land use and design concept that suggests a potential development alternative for the physical improvement and redevelopment of the sector plan area.
- Recommends an economic development strategy and action plan to encourage land assembly, redevelopment and revitalization of existing commercial properties.
- Includes a sectional map amendment which changes the existing zoning pattern to achieve consistency with the land use plan and implements the M-U-I Zone and the DDOZ along with appropriate design standards.

ABOUT THIS SECTOR PLAN

AREA DESCRIPTION

The College Park US 1 Corridor Sector Plan is entirely within the City of College Park, Prince George's County, Maryland (Map 1), except for one parcel. The sector plan area comprises approximately 442 acres and extends nearly 3 miles along US 1 (Baltimore Avenue) from the Capital Beltway (north) to the vicinity of Guilford Drive (south) (Map 2). The plan text refers to US 1 and Baltimore Avenue interchangeably.

Baltimore Avenue forms the spine of the corridor and is bordered on both sides by nearly continuous commercial development. This commercial corridor is the main access to The University of Maryland and various destinations within the City of College Park. The eastern and western boundaries of the sector plan are irregular in shape and extend no

more than one or two blocks back from US 1. The strip of commercial businesses is bounded on the east by residential neighborhoods and on the west by The University of Maryland, the Paint Branch Stream Valley Park, and residential areas. The area west of downtown College Park, in the vicinity of Knox Road and Guilford Drive, contains older multifamily rental and condominium units.

The heavily wooded floodplain of Paint Branch forms an east-west open space link along the north side of Paint Branch Parkway. The CSX Railroad tracks are the easternmost sector plan boundary, which is adjacent to the College Park-University of Maryland Metro station. The College Park US 1 Corridor Today chapter provides additional descriptive data and information.

WHY A SECTOR PLAN?

The process leading to publication of this sector plan was begun by the City of College Park in 1993. At that time, the city adopted *A Vision of College Park in the 21st Century*, following an extensive interactive process within the local community. This was followed in 1995 by the city's *Comprehensive Plan*, which includes a section on the US 1 corridor and town center, along with vision statements, strategies and actions needed to implement the plan. The city's vision and plan set the wheels in motion for development of this sector plan.

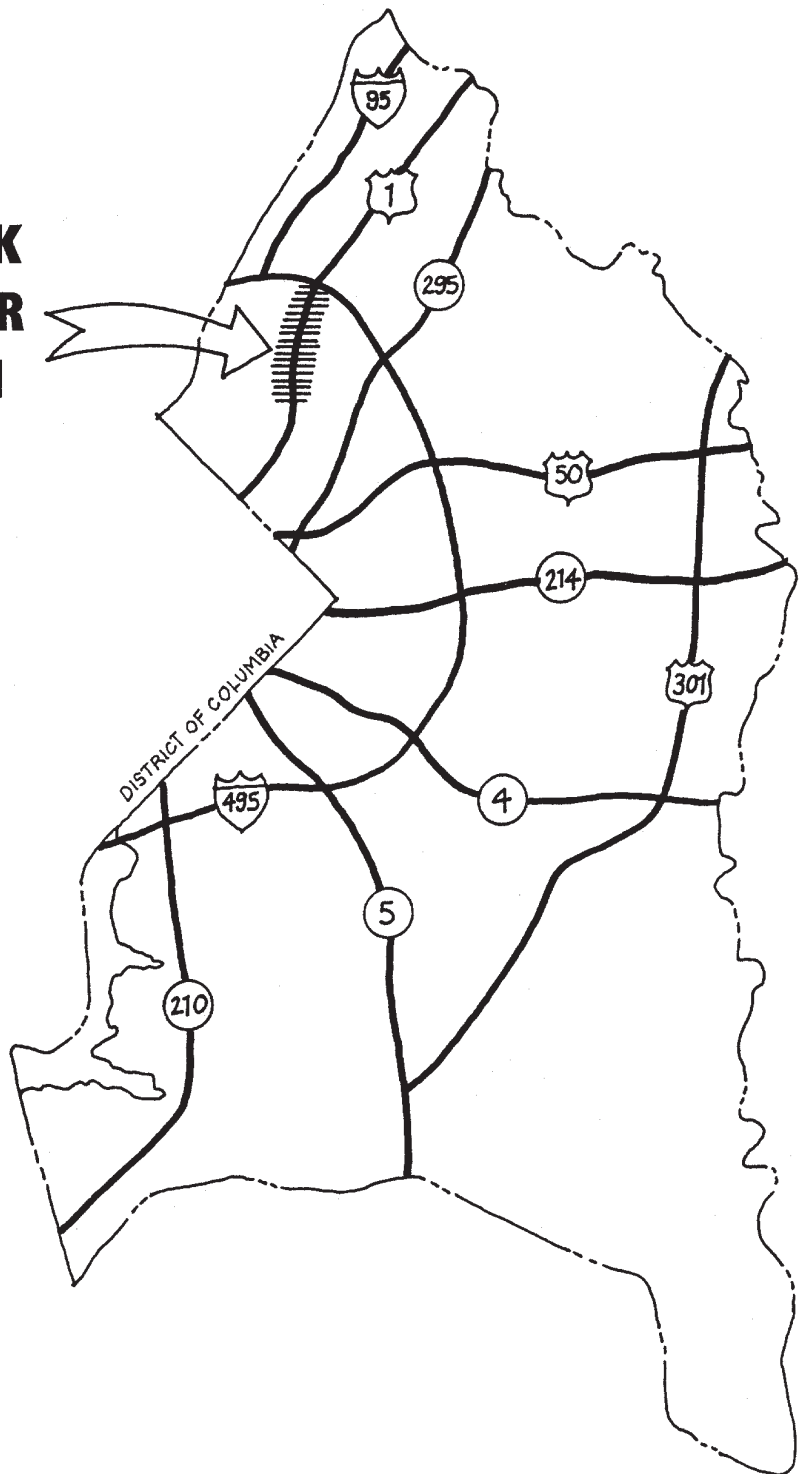
To achieve its vision, the city recognized the need for major infrastructure improvements along the corridor. This was pursued through the State's Consolidated Transportation Program with Prince George's County and the Maryland State Highway Administration (SHA). In 1998 SHA initiated the US 1 Alternatives Study to address traffic safety and manage congestion.

Also in 1998 the city opted to participate in a Visual Preference Survey and Vision Translation Workshop related to the US 1 corridor. The study was conducted by A. Nelessen Associates, Inc., of Princeton New Jersey, and



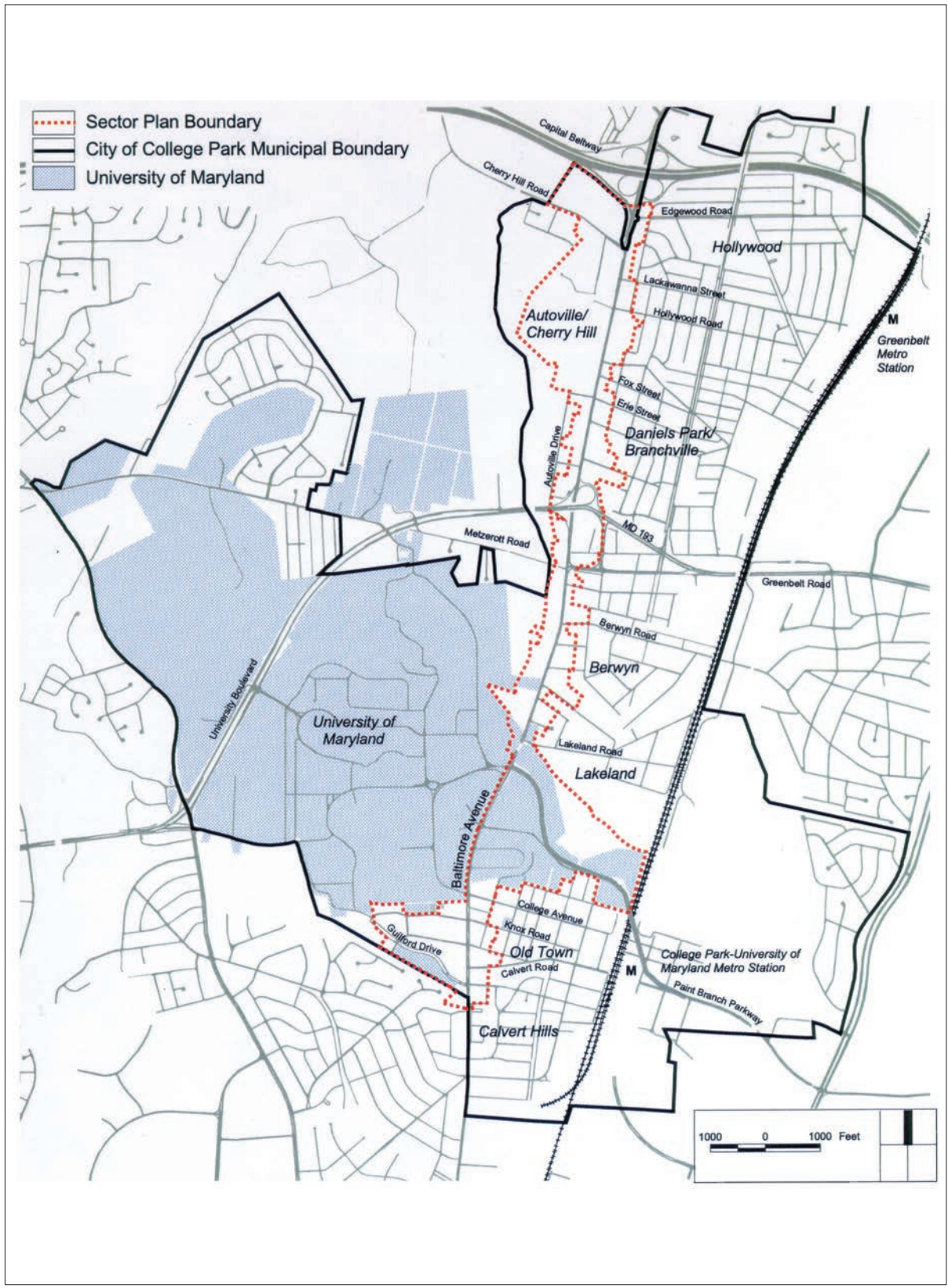
The vision for downtown College Park is to create a town center which is the focus of community activity.

**COLLEGE PARK
US I CORRIDOR
SECTOR PLAN
AREA**



Prince George's County Location Map

Map I



Sector Plan Boundaries

Map 2

was sponsored by the Maryland Department of Transportation as a Smart Growth initiative. The results were published in *College Park, Maryland-Smart Growth Quality Community Survey*. Local residents, merchants and students came together to discuss Baltimore Avenue and to advance the city's earlier vision.

To complement SHA's efforts and to advance the city's vision, the city requested assistance from The Maryland-National Capital Park and Planning Commission (M-NCPPC) to undertake a sector plan to address land use, urban design, and transportation issues. The city also desired to see previous planning concepts implemented and to leverage opportunities presented by recent nearby development. The opening of the College Park-University of Maryland Metro Station, the construction of the Paint Branch Parkway, and the continued growth of the university created expectations of a revitalized residential and business community and an improved streetscape along Baltimore Avenue. However, new development has been limited along the corridor and the revitalization of older areas, as suggested in previous planning concepts, has not occurred.

In response to the city's request, the Prince George's County Planning Department funded preparation of the sector plan in the FY 2000 budget. The project team was assembled in November 1999.

The stated purpose of the sector plan is to transform the US 1 corridor from an old highway commercial strip to a revitalized gateway boulevard and town center. Its focus is upon potential land use needs, trends and opportunities, including new zoning tools and design standards to help promote reinvestment and redevelopment. The sector plan process was chosen as the most appropriate mechanism for evaluating the many previous planning efforts and other changes that have occurred in the US 1 corridor since the last comprehensive planning and zoning cycles in 1989 and 1990, respectively.

The process of amending a master (sector) plan is described in the Prince George's

County Zoning Ordinance. The amendment is developed through a series of steps from preliminary plan formation through approval of the plan by the County Council (see Figure 1). The complete sequence of the plan making process is shown in Appendix B. Also, a sectional map amendment (SMA) has been processed concurrently to implement the plan's land use and zoning recommendations and urban design standards. The SMA establishes appropriate zoning within a Development District containing regulatory design standards for building and site design, parking, screening, buffering and public areas. The full range of standards and their applicability are discussed in the Development Districts Standards chapter.

THE COMMUNITY'S ROLE

The public participation program for this sector plan is summarized in Figure 2. It includes an advisory planning group (APG) of local representatives and the community-at-large. The roles and responsibilities of each are as follows:

- The Advisory Planning Group (APG)—consisting of 16 representatives from all geographic areas and interest groups. This group met 11 times with project staff at College Park City Hall. A list of members and the organizations they represent is included in the beginning of this plan document. Members were selected based on nominations from the university and the city, and from written/verbal requests submitted at the February 7, 2000, public forum. The final list of members was approved by the Prince George's County (District) Council. The first APG meeting was held on March 23, 2000. APG met with project staff to offer advice in formulating solutions to the issues originally identified in the March 2000 document *Preliminary Goals, Concepts & Guidelines and Public Participation Program for the College Park US 1 Corridor Sector Plan*. Meetings were open to the public and project staff facilitated each meeting and provided administrative and technical assistance. Development of the preliminary plan and APG's discussion of it was



Stakeholders work together to find common solutions.

assisted by the two community-based workshops noted below. Also, the plan-

ning team met several times with the business community regarding the plan process and its relationship to the SHA improvement study.

- The Community—Community involvement began with the February 7, 2000, public forum. Later, two community workshops were held at the College Park Community Center to review ideas generated by APG. At the May 20, 2000 workshop, participants were briefed on the planning context, issues and land use alternatives, and participated in a breakout session to outline the desired character of development within the corridor. On June 17, 2000, previous findings were reviewed. A group

**Figure 1
Generalized Plan Development Process**

PUBLIC FORUM	PLAN PREPARATION	JOINT PUBLIC HEARING(S)	PLAN ADOPTION (Planning Board) AND APPROVAL (District Council)
<ul style="list-style-type: none"> • Issue identification • Participation program • Public comments 	<ul style="list-style-type: none"> • Community • City • University • Planning staff 	<ul style="list-style-type: none"> • Planning Board • District Council 	

**Figure 2
Public Participation Program**

PARTICIPANTS	COMPOSITION	SCHEDULE
The Community	Interested Persons	Meetings & Workshops
Advisory Planning Group (approximately 10-20 members)	City of College Park (up to 3 members) The University of Maryland (up to 3 members) Civic Associations (1 member each) Business/property owners (up to 3 members)	11 Advisory Planning Group meetings (March 2000-February 2001) Community workshops held May 20 and June 17, 2000 Developers Roundtable Forum held December 11, 2000

exercise allowed participants to consider potential forms of development for several areas along the corridor. Potential implementation techniques in zoning and traffic mitigation issues were also discussed.

Following several APG meetings, a proposal was made to enlarge the sector plan boundary. A second public forum was held on June 1, 2000, to receive public comment on the proposal. The District Council approved the boundary enlargement on June 27, 2000.

Both APG and the community-at-large are encouraged to take an active role in monitoring the implementation of the plan in the years ahead.

APPLICABILITY

This sector plan map and text amends the 1989 *Approved Master Plan for Langley Park, College Park and Greenbelt* (Planning Areas 65, 66 and 67) for the portion of Planning Area 66 containing the sector area only. The sectional map amendment contained herein supersedes the 1990 sectional map amendment for the portion of Planning Area 66 containing the sector area. This plan also amends the 1982 *Master Plan of Transportation* and the *Countywide Trails Plan*. The plan is consistent with the recommendations of the November 2000 Interim General Plan.

Note that the use of the word “shall” in the sector plan text indicates that the action proposed reflects an ordinance (i.e., clearly mandated by either state or county law) or reflects county policy regarding land development or other planning considerations. The use of the word “should,” while not necessarily legally binding, indicates that the guideline or proposal is to be followed in all instances where there are no extraordinary circumstances which would mitigate against it.

There are four categories of recommendations contained in this sector plan:

- The first category is the land use, urban design, transportation, environmental, and public facility recommendations which establish the policy criteria used to evaluate development proposals for conformance with the plan



Residents, elected officials, property and business owners participate in a community workshop discussing ideas to transform the US 1 corridor.

- The second comprises an economic development strategy that is intended to guide actions aimed at retaining, expanding and attracting new commercial and residential development
- The third comprises the sectional map amendment (SMA) which comprehensively rezones property to conform with and implement the plan’s land use policies and establishes the Development District Overlay Zone (DDOZ)
- The fourth category comprises the Development District Standards and table of permitted/prohibited land uses that will be used to evaluate development proposals, regulate new development, and ensure conformance with the plan’s land use and urban design recommendations

PLANNING BACKGROUND

RELATIONSHIP TO OTHER PLANS AND POLICIES

Several earlier plans have continuing significance for the College Park US 1 Corridor. They are subsumed within the context of this sector plan. The following summarizes how these previous planning policies relate to this sector plan.

1982 GENERAL PLAN AND MASTER PLAN OF TRANSPORTATION

This sector plan is consistent with the November 2000 Interim General Plan. The 2000 Interim General Plan reflects the fundamental recommendations of the county's Commission 2000 *Biennial Growth Policy Plan*, which supersedes the 1982 *General Plan*. The recommendations represent a comprehensive smart growth initiative that utilizes a system of growth tiers, corridors and centers to guide future land use and development in the county. The sector plan area is within and subject to development and revitalization policies recommended for Corridors and Developed Tier within the 2000 Interim General Plan.

The 1982 *Master Plan of Transportation* reflects the status of Baltimore Avenue and University Boulevard/Greenbelt Road as proposed arterial highways with 4-6 lanes and rights-of-way of 100-120 feet and 120-200 feet, respectively. However, US 1, within the sector plan boundary, is downgraded to a four-lane divided major collector as discussed in the Transportation and Circulation chapter.

THE 1989 LANGLEY PARK-COLLEGE PARK-GREENBELT MASTER PLAN

This master plan sets forth land use, public facilities, environmental and zoning recommendations for Planning Areas 65, 66 and 67. The sector plan is in Planning Area 66 and amends the portion of the area containing the sector area. The 1989 master plan evaluates the problems and issues along the US 1 corridor and emphasizes land assembly, protection of residential areas, limited

commercial expansion, development controls imposed through an overlay zone, reduction of access from side streets and individual properties, improved parking and sidewalks, increased landscaping, better signage, and removal of obsolete buildings. The recommended overlay zone was intended to establish development and design criteria for guiding future development, at such time as the county adopts an overlay concept. The 1990 SMA brought the zoning throughout the area into conformance with the master plan.

THE 1995 CITY OF COLLEGE PARK COMPREHENSIVE PLAN

In 1995, the City of College Park adopted a Comprehensive Plan, completing a three-year planning process which included an extensive visioning effort. The city's plan recommends implementation strategies related to citywide issues, including those pertinent to US 1 and the town center. The comprehensive plan affirms the city's intent to implement the then-seven state visions contained in the 1992 Maryland Economic Growth, Resource Protection and Planning Act.

The city's plan acknowledges that formal authority for planning and zoning matters in the city rests with M-NCPPC and the Prince George's County (District) Council. Therefore it is the 1989 Langley Park-College Park-Greenbelt master plan and amendments thereto, including the 1990 SMA, rather than the 1995 *City of College Park Comprehensive*



The City of College Park's vision for the US 1 corridor provided the framework for the sector plan's development.

Plan, which is recognized under county and state law as the legal document guiding land use and development within the sector plan area. The information brochure that was prepared in conjunction with the opening public forum held on February 7, 2000, drew extensively on the proposals and recommendations of the city's comprehensive plan.

THE 1992 MARYLAND ECONOMIC GROWTH, RESOURCE PROTECTION AND PLANNING ACT

This legislation was enacted to encourage economic growth, limit sprawl development, and protect the state's natural resources. It establishes consistent general land use policies to be locally implemented throughout Maryland. These policies are stated in the form of seven visions, which follow. During the 2000 Session of the Maryland General Assembly, Vision 8 was added.

1. Development is concentrated in suitable areas.
2. Sensitive areas are protected.
3. In rural areas, growth is directed to existing population centers and resource areas are protected.
4. Stewardship of the Chesapeake Bay and the land is a universal ethic.
5. Conservation of resources, including a reduction in resource consumption, is practiced.
6. To assure the achievement of 1 through 5 above, economic growth is encouraged and regulatory mechanisms are streamlined.
7. Adequate public facilities and infrastructure under the control of the county or municipal corporation are available or planned in areas where growth is to occur.
8. Funding mechanisms are addressed to achieve these visions.

The eight visions are a set of guiding principles that describe how and where growth and development should occur. They establish a land and water stewardship ethic to guide state, county, municipal and private sector decisions regarding growth and development. The act acknowledges that the comprehensive plans prepared by counties and municipalities are the best mechanism to establish priorities for growth and resource conservation. Once priorities are established, it is the state's responsibility to support them.

All of these visions already form the background of the November 2000 Interim General Plan and are addressed in the city's 1995 plan. This sector plan continues to recognize and support those that are relevant. The 1992 act requires consistency between local master plans and regulations, including zoning. Thus a major objective of this sector plan is to develop a streamlined regulatory framework which minimizes discrepancies between the plan and other county development regulations.

THE 1997 SMART GROWTH AND NEIGHBORHOOD CONSERVATION ACT

The act builds on the foundation of the eight visions adopted in the 1992 Act, as amended. The act is nationally recognized as an effective means of evaluating and implementing state-wide programs to guide growth and development.

Local governments are required to periodically update comprehensive plans in order to reflect the visions of the act. The act influences economic growth and development by directing state expenditures to priority funding areas, such as the City of College Park.

Two subsequent bills (collectively referred to as Smart Codes) were enacted by the State General Assembly in 2000 to further the Smart Growth concept. The bills provide model development regulations and offer incentives to local governments that adopt the model or similar legislation. The Maryland Building Rehabilitation Code (Senate Bill 207) promotes the rehabilitation of existing buildings by integrating existing codes, separating requirements for rehabilitation from

those for new construction, and introducing a sliding scale so that smaller rehabilitation projects have fewer code requirements with which to comply. The second (House Bill 285) requires the Maryland Department of Planning to develop model infill development and compact mixed-use tools to (1) support and enhance the development of land in existing communities and (2) promote compact new development in priority funding areas.

As a result of these bills, the Maryland Department of Planning has developed the Compact Mixed-Use (CMU) Overlay Zone, a model code for potential adaptation and local use in identified priority funding areas, such as the City of College Park. The zone requires a range of residential dwelling types in a vertical and horizontal mix of supportive commercial uses. Minimum residential density is seven units per acre, with one unit per acre resulting from use of a Transfer of Development Rights (TDR) technique. The model contains standards for open space, landscaping, public facilities, parking, etc.

This sector plan encourages residential and mixed-use infill development along US 1 in both vertical and compact building forms for the purpose of concentrating development. The Mixed-Use-Infill (M-U-I) Zone is used in this sector plan to implement mixed-use residential and commercial development and is based on the provisions of the state's model CMU Overlay Zone code. Also, the county's November 2000 Interim General Plan recommends consideration of an investigation of options for establishing a TDR program for use in Developed Tier communities such as College Park.

COMMISSION 2000 RECOMMENDATIONS

Between 1995 and 1997 the Prince George's County Council undertook an extensive study of growth management, adequate public facilities ordinances, and the existing development pipeline in the county. Building on these initiatives the Council created Commission 2000, a 53-member, broad-based advisory panel to develop and recommend a growth management plan and a strategy to achieve

it. Commission 2000 published its final report in July 2000, entitled *Commission 2000, Final Report, Biennial Growth Policy Plan*. The County Council adopted the recommendations, with amendments, as the Biennial Growth Policy Plan in November 2000, which became the November 2000 Interim General Plan. Key recommendations endorsed by this sector plan include the following:

- Recognizing the College Park US 1 Corridor as part of the Developed Tier and as part of one of six designated corridors. Recommended county priorities for these areas generally include:
 - Strengthening neighborhoods and encouraging compatible infill.
 - Encouraging redevelopment and revitalization that can be supported by existing or planned transportation and school capacity.
 - Encouraging a mix of residential and nonresidential development.
 - Encouraging transit- and pedestrian-oriented development.
 - Targeting public investment that will promote private investment in these areas with tools such as financial incentives, simplified development review processes, redevelopment assistance, preferential fee structuring, rezoning of vacant or underutilized land to achieve planned densities, and permitting a ten percent increase in density as a transferable development rights (TDR) receiving area. As noted above, the county does not have a TDR program at present.
- Recognizing the sector plan area as part of the proposed Revitalization Overlay policy. This policy encourages reinvestment in and redevelopment of older commercial areas and communities through public investments, incentives and code revisions. The sector plan utilizes the recently enacted Development District Overlay Zone (DDOZ) legislation to establish design standards and provide flexible development review procedures. Also, the new

Mixed-Use-Infill (M-U-I) Zone used in this sector plan will permit mixed residential and commercial infill development within the area under specific conditions.

- Recognizing the sector plan as part of the Environmental Resources Overlay (ERO), which encourages establishment of a network of open space, parklands, woodlands, waterways, stream valleys, and wetlands that promote natural habitats and species retention. Within the sector plan, Paint Branch Stream is recommended to become a component of the ERO.
- Recognizing the priority recommendations of the November 2000 Interim General Plan: quality schools (Priority 1); quality economic development (Priority 2); compatible development and redevelopment in built-up areas (Priority 3); adequate public facilities in areas targeted for infill and redevelopment (Priority 4); environmental protection (Priority 5); transit support (Priority 6); and high quality housing (Priority 8). Priority 7 is not pertinent as its focus is on farmland preservation.

COLLEGE PARK US 1 CORRIDOR CHARACTERISTICS

AREA PROFILE

The following descriptive data provides an overview of the US 1 corridor in College Park as it existed in March 2002. An inventory of land use is provided in Table 1 and shown on Map 3. Table 2 provides an inventory of zoning prior to approval of this sector plan and sectional map amendment, as shown on Map 4. There were no changes to either inventory between March 2000 and plan approval.

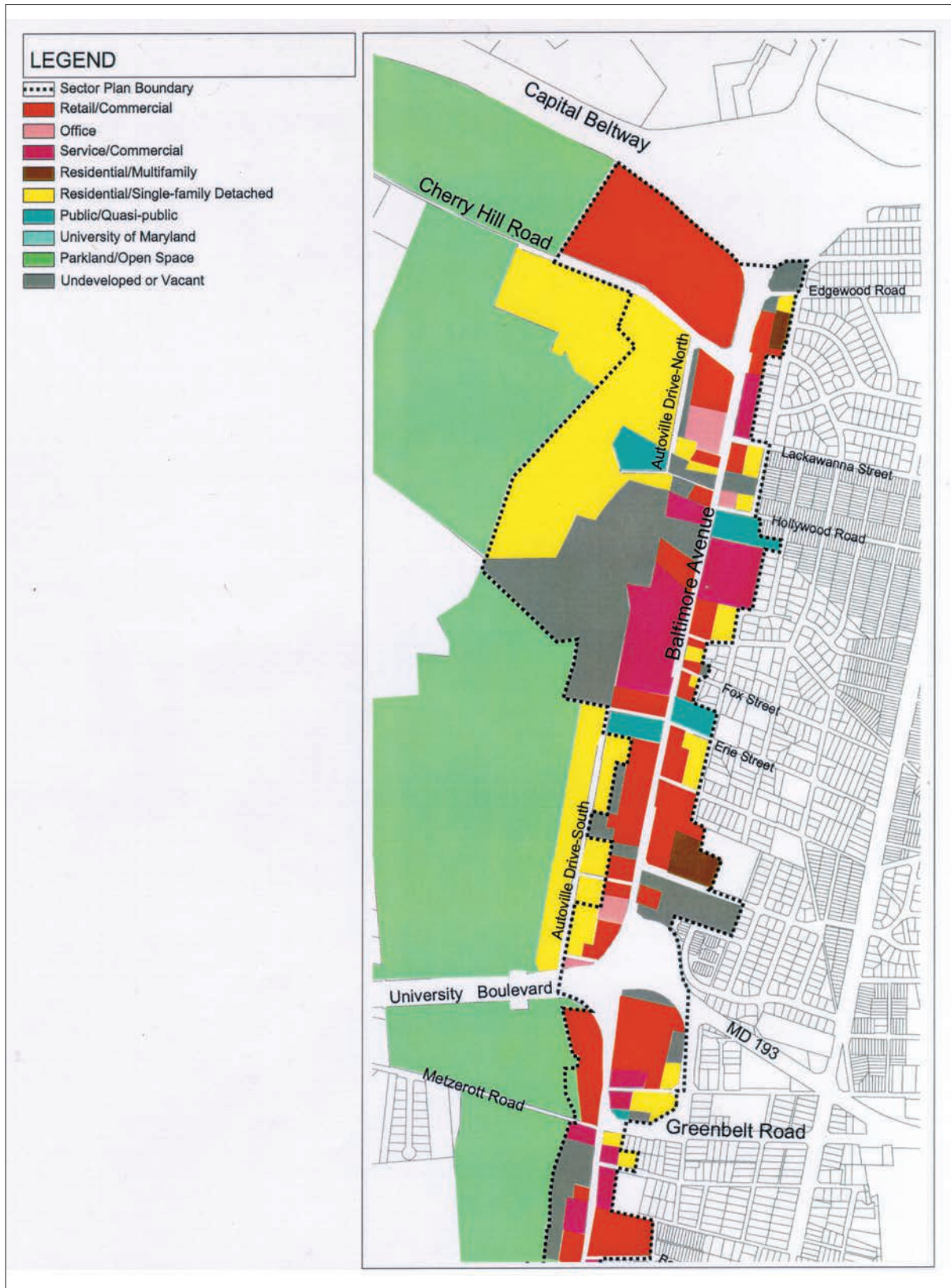
The sector plan includes 442 acres of which 279 acres are developed (63 percent). There are 132 acres (30 percent) in commercial use, 62 acres (14 percent) in residential use, and 8 acres in public/quasi-public use. The University of Maryland owns 78 acres.

- Approximately 300 business establishments comprise nearly 1,625,000 square feet of commercial retail, office, service, and personal service space.
- Commercial development is predominately in a strip development form along US 1, north of Paint Branch. Downtown College Park is developed in a more traditional compact and main street form.
- The predominant zoning category in the corridor prior to plan and sectional map amendment (SMA) approval was the Commercial Shopping Center (C-S-C) Zone (118 acres), with substantially smaller amounts of Commercial Office (C-O) and General Commercial (C-2) zoning. The

**Table 1
Land Use Prior to Plan Approval
College Park US 1 Corridor**

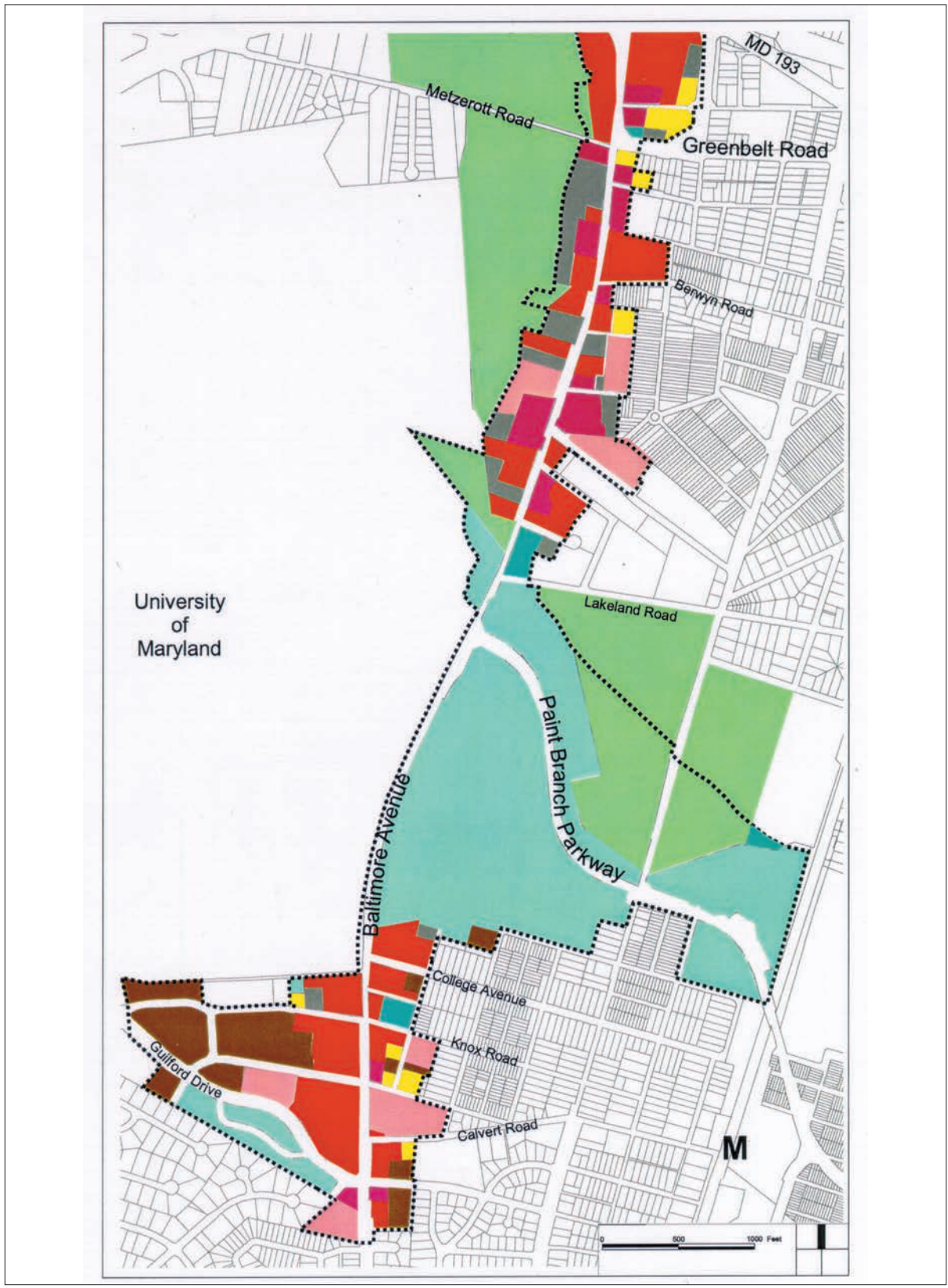
Land Use	Total Acres	Percent of Total
Commercial	132.1	29.9
• Retail	85.8	19.4
• Service	27.6	6.3
• Office	18.7	4.2
Residential	61.8	14.0
• Single-family detached	41.5	9.4
• Multifamily	20.3	4.6
Public/Quasi-Public	7.9	1.8
The University of Maryland	77.6	17.6
Subtotal: Developed Area	279.4	63.3
Parkland	40.0	9.0
Rights-of-Way	78.6	17.8
Undeveloped Area	43.8	9.9
Total	441.8	100.0

Source: M-NCPPC, Field Survey, March 2002.
Note: All acreage is approximate.



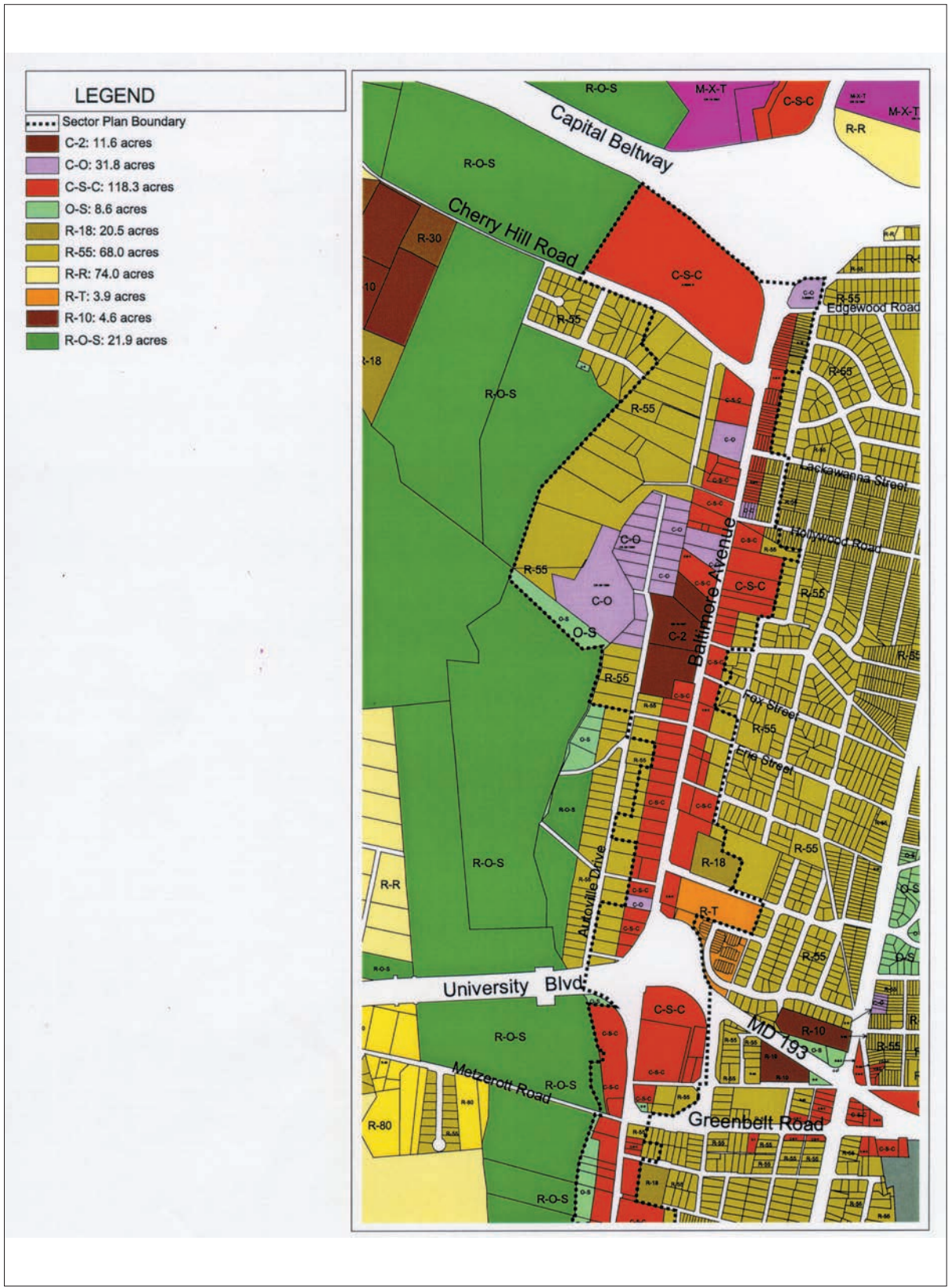
Existing Land Use-North (as of March 2000)

Map 3a



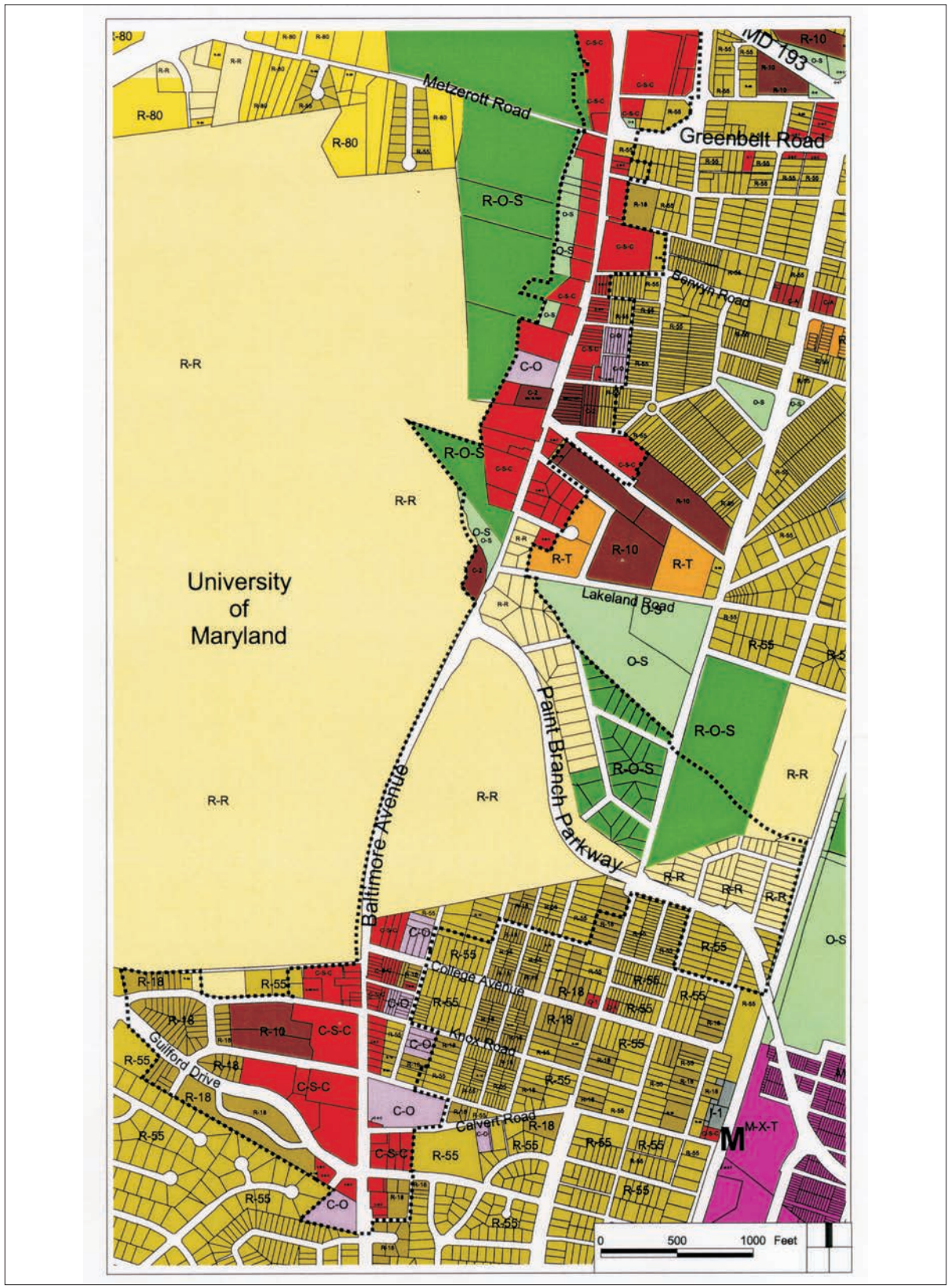
Existing Land Use—South (as of March 2000)

Map 3b



Zoning Prior to 2002 SMA Approval-North

Map 4a



Zoning Prior to 2002 SMA Approval-South

Map 4b

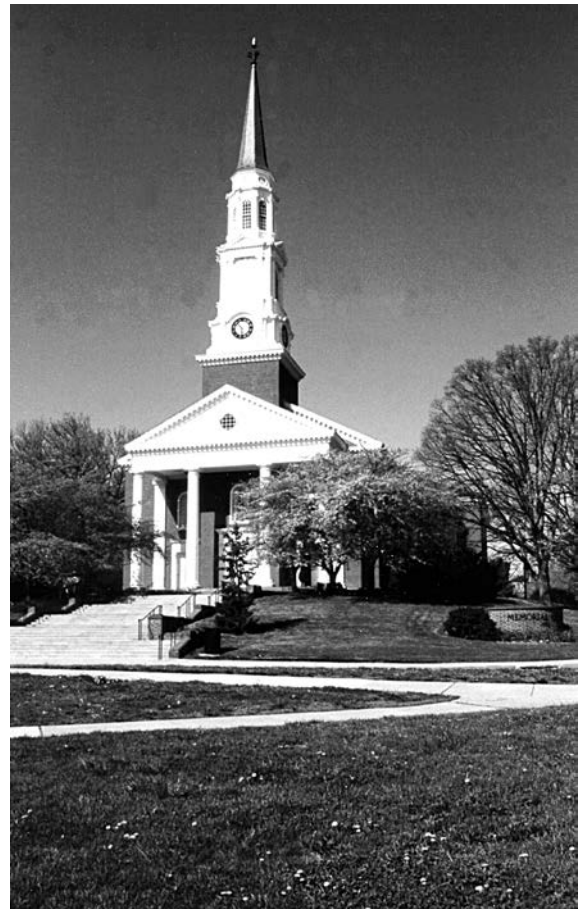
Table 2 Zoning Prior to 2002 SMA Approval College Park US 1 Corridor	
Zone	Acres
C-S-C	118.3
C-2	11.6
C-O	31.8
R-R	74.0
R-55	68.0
R-T	3.9
R-10	4.6
R-18	20.5
O-S	8.6
R-O-S	21.9
Subtotal	363.2
Rights-of-Way	78.6
Total	441.8

*Source: M-NCPPC, March 2000.
Note: All acreage is approximate.*

entire road frontage in downtown College Park is also zoned commercial. Zoning approved as part of the 2002 SMA is shown in Table 10 in the Sectional Map Amendment chapter of this plan.

- Residential development consists of approximately 40 single-family detached dwellings and 57 townhouse units. There are approximately 450 multifamily apartments, with approximately 350 located west of downtown College Park in the vicinity of Knox Road and Guilford Drive, which also includes the “Knox Boxes” apartment area. Approximately 1,125 students live in university housing in the East Campus area which is east of Ritchie Coliseum and includes Fraternity Row.
- Further north and west of US 1 commercial frontage properties are two small established single-family residential neighborhoods along Autoville Drive; one located between University Boulevard and Erie Street, the other south of Cherry Hill Road.

- In 2001, The University of Maryland had 24,600 full-time, 8,500 graduate, and 7,650 part-time students. There were approximately 7,000 full-time faculty and staff. The university’s East Campus area is within the sector plan area and includes various university maintenance and facility support functions on 78 acres.
- In 2001, The University of Maryland Shuttle served over 5,000 daily riders (1,000-2,000 at night). The nearby College Park-University of Maryland Metro Station, which is adjacent to the sector plan area, generated approximately 7,000 rail riders per day.
- The overall character of development along US 1 was established in the post-World War II era of suburbanization. The majority of properties fronting US 1 were subdivided during the 1930s and 1940s for both residential and commercial development.



The University of Maryland campus is a significant component in the development of the sector plan.

Over the years commercial rezonings increased and residential structures were often converted to commercial use. The commercial strip character emerged in earnest during the 1960s, prior to a master plan for the area.

AREA CHALLENGES

The following vision statement, problems and issues are excerpts taken from the city's 1995 comprehensive plan. They are intended to illustrate the range of issues and problems that challenge the realization of opportunities to revitalize US 1 and the creation of flexible planning and implementation strategies.



Vacant and underutilized properties provide opportunities for infill and redevelopment.

1995 CITY OF COLLEGE PARK COMPREHENSIVE PLAN

The following vision statement from the city's plan speaks to the opportunities that this sector plan is addressing:

"Route 1 is ... a boulevard with an inspiring visual image that emphasizes the pedestrian quality of the street. The corridor has been revitalized to provide an appropriate mix of shopping, institutional, service, office, residential and open space uses which serve residents, the University population, and visitors. The width of pavement has not increased and new development has been permitted to locate near the road, creating a corridor that is comfortable for pedestrians and bicycles. Alternative traffic routes for commuter and 'through' traffic

have been developed and provide other access points to the University."

The problems, issues and challenges that stand in the way of achieving this vision can be summarized by the following excerpts taken from the city's plan:

- Pedestrian safety:
 - "Inadequate provisions for pedestrian movement and safety;
 - "Signs and utility poles . . . located in the middle of sidewalks, blocking pedestrian's passage;
 - "Crosswalks and safety islands are generally lacking;
 - "Sidewalks lie right against traffic, with no planting strip to buffer or safeguard pedestrians."



Pedestrian amenities such as continuous sidewalks, bus shelters, bike lanes and street trees are generally lacking throughout the corridor.



Vehicular access is haphazard with uncontrolled left-turn lanes.

■ Traffic and circulation:

- “Continuous uncontrolled left-turn lanes (suicide lanes);
- “Difficult to access cross-streets; an increasing traffic accident rate; inadequate alternatives to the automobile.”



US 1 contains low-profile sprawling commercial strip development.

■ Commercial development:

- “Businesses typically located on small lots with little or no congruity in terms of setback, landscaping, signage or parking lot lay-out. Commercial uses on the east side of Route 1 are characterized by shallow lot depths of approximately 100 feet;
- “Building parcels are characterized by individual curb cuts, sometimes several on a single property, limited parking, and narrow or missing sidewalks.”

■ Appearance and amenities:

- “The image of Route 1 is that of a sprawling commercial strip of low-profile buildings on a sea of pavement and minimal landscaping;
- “Conflicts between Route 1 commercial and adjacent residential uses include noise, visual blight, spill-over traffic and parking;
- “Amenities such as benches, bus shelters, trash receptacles, and street directories are lacking.”



The pattern of development is in small, narrow lots fronting US 1, making land assembly difficult.

Based on these descriptive conditions, the city’s plan identifies the need for an up-graded highway, streetscape and pedestrian improvements, and for redevelopment. The plan acknowledges that:

“... existing land use controls are not adequate to regulate development along the corridor. Design guidelines and site plan review are needed to insure better quality development and aesthetics. Sign controls and screening of parking need to be enforced. Existing parcels need to be combined to provide larger building sites.”

The Land Use and Urban Design chapter provides some additional background information and discusses the concepts by which this sector plan addresses these challenges.



Plan Elements



LAND USE AND URBAN DESIGN

This sector plan supports the vision contained in the City of College Park's Comprehensive Plan to transform the strip development character of US 1 within the city into a revitalized gateway boulevard. The gateway boulevard concept integrates the implementation of the state's planned highway and pedestrian-friendly streetscape improvements with opportunities for changing the character of properties adjacent to the corridor through compact infill development. A range of new mixed-use development, including residential uses, are encouraged. Implementation of the gateway boulevard concept will be guided by new standards of urban design that will help define a greatly enhanced entryway to College Park and Maryland's flagship university.

GOALS

- Create an attractive and vibrant gateway to the City of College Park and The University of Maryland.
- Provide for concentrations of vertical, mixed-use development, including retail, service, office and residential, in appropriate locations to expand opportunities for living, working, shopping and studying within the corridor, while reducing use of the automobile.
- Balance the need for land assembly and redevelopment with sensitivity to impacts upon existing businesses and adjacent neighborhoods.
- Support public sector reinvestment in reconstruction of the corridor to complement new land use regulations and new development.
- Identify and stimulate appropriate redevelopment that can respond to changing market forces.
- Encourage quality development by utilizing new mixed-use infill zoning and urban design concepts, streamlining the development review process, and suggesting market-oriented incentives and partnerships.

BACKGROUND AND ISSUES

BACKGROUND

The sector plan's land use and urban design concepts and recommendations are guided by the problems, issues and opportunities identified in previous plans, planning studies,

and the January 2000 *Public Forum Information Brochure*. The Introduction chapter in this plan gives an overview of these prior efforts and provides a description of the area's planning challenges.

Two striking observations of the area's problems and opportunities are excerpted from the city's 1998 *Smart Growth Quality Community Survey*:

"US 1's deplorable condition is experienced daily by multitudes of drivers and by the University of Maryland students and faculty. Of particular concern for the University is the constant stream of young and impressionable minds which have to experience this as part of their formal education. This area begs for redevelopment as a symbol of hope and to encourage young people to find joy and delight in urban places—not the depression and dysfunctionality of a grunge environment."

It continues, however, by stating:

"Baltimore Avenue could become one of the great examples of urban transformation, from the current, generally negative strip commercial auto-dominated sense of place, into a livable community embodying the best principles of Smart Growth and Smart Transit."

To achieve this, the survey recommends the following:

- Reengineer and redevelop the highway as a boulevard and main street in specific locations, with appropriate streetscape improvements

- Implement architectural and design standards for every lot and building along US 1
- Establish mixed-use zoning to allow residential over ground floor commercial

ISSUES

The issues and opportunities repeatedly mentioned in all of the previous studies and plans can be summarized as follows:

- New development needs to be concentrated in clusters of compact and vertical mixed-use development that complement rather than compete with each other
- Opportunities to reduce use of the automobile need to be explored and the pedestrian quality of the street improved
- New planning and zoning tools are needed to allow the mixing of land uses and to establish high standards of urban design for new development
- Downtown College Park needs a defined town center that functions as a center of community activity with a complementary mix of land uses
- Student housing needs to be improved and located on or near the campus
- Single-family detached neighborhoods adjacent to US 1 need to be preserved and protected from incompatible uses
- Redevelopment opportunities are constrained by the need for land assembly and public/private partnerships are needed to assist implementation

CONCEPTS

LAND USE CONCEPT

The land use concept is comprised of several elements described below:

1. The corridor is divided into six areas for the purpose of examining issues and opportunities and formulating recommendations. Each area has been further divided into subareas for the purpose of defining the desired land use types, mixes, and character of development (see Maps 5 and 6).
2. Highway improvements are linked with land use improvements to achieve a gateway boulevard. The plan calls for reconstruction of US 1 with a landscaped center median, landscaped strips along the roadway, new sidewalks, bike lanes, as well as a pedestrian-oriented, mixed-use development character.
3. The land use concept provides flexibility so that future development can be determined by market demand within the context of the development character recommendations for each area and subarea. A full range of commercial uses is permitted and higher density residential uses are encouraged. These land uses may be mixed on a single site or combined within a single building. New legislation, the Mixed-Use-Infill (M-U-I) Zone, is utilized to allow a mix of residential and commercial development within the sector plan boundary. This will increase opportunities for new multifamily and student housing to be built which, in turn, will enable more people to live near work and school, thereby reducing the number of vehicle miles traveled.
4. The Development District Overlay Zone (DDOZ) is used to implement site plan review by the Planning Board to ensure that development is in compliance with the land use types and development character recommended in the plan.

URBAN DESIGN CONCEPT

To achieve the visions for the corridor and town center area, specific urban design principles are needed to guide the physical form of new development. These principles need to:

- Promote compact, pedestrian-friendly neighborhoods
- Delineate a clearly defined town center

LEGEND

. Sector Plan Boundary

Area 1:

- 1a. West of Downtown
- 1b. West side of US 1
- 1c. East side of US 1
- 1d. South of Paint Branch Parkway
- 1e. Metro-related area

Area 2:

- 2a. West side of US 1
- 2b. North of Paint Branch Parkway

Area 3:

- 3a. West side of US 1
- 3b. East side of US 1

Area 4:

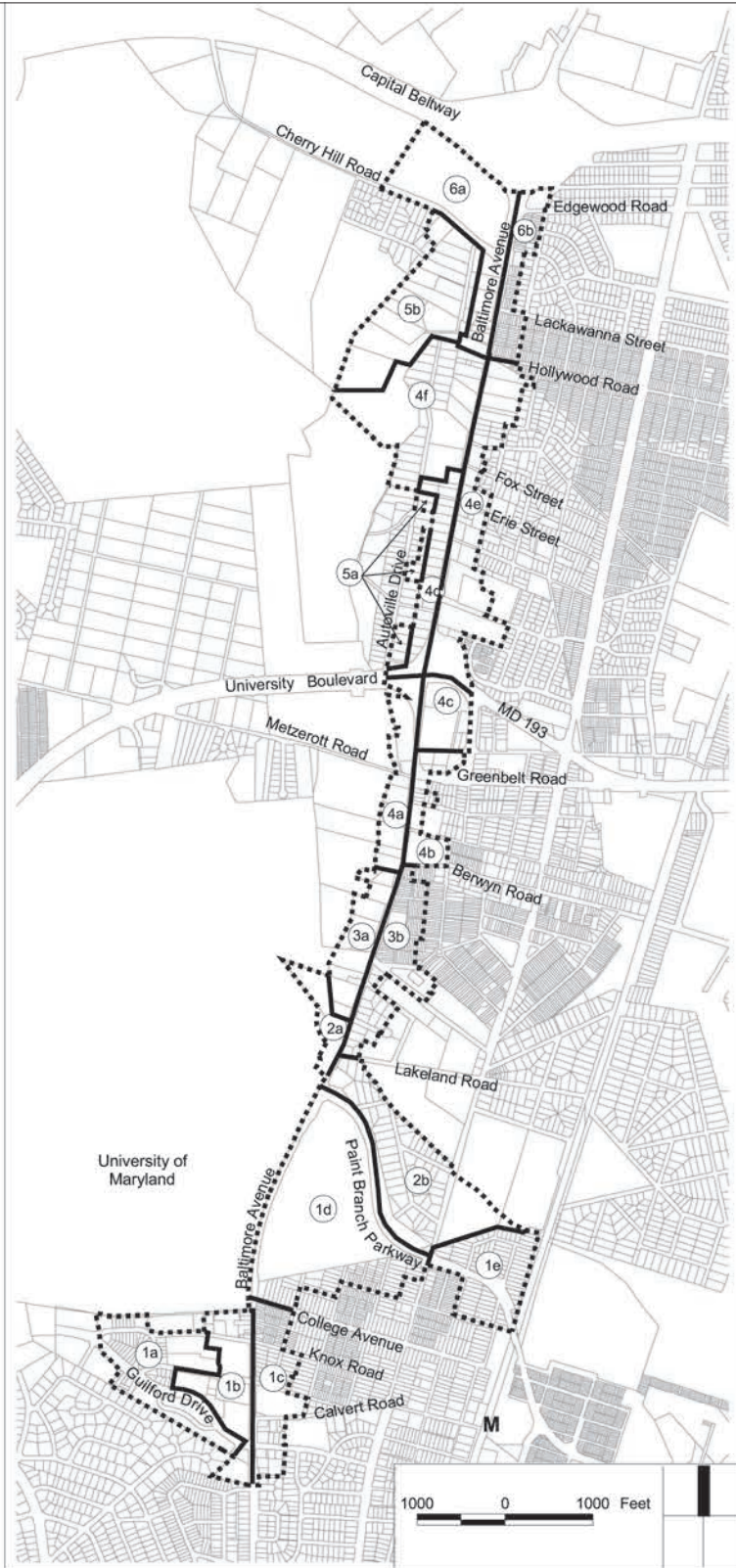
- 4a. West side of US 1, south of MD 193
- 4b. East side of US 1, south of Branchville Road
- 4c. SE corner of US 1/MD 193
- 4d. West side of US 1, north of MD 193
- 4e. East side of US 1, north of MD 193
- 4f. West side, between Fox Street & Hollywood Road

Area 5:

- 5a. Autoville Drive - South
- 5b. Autoville Drive - North

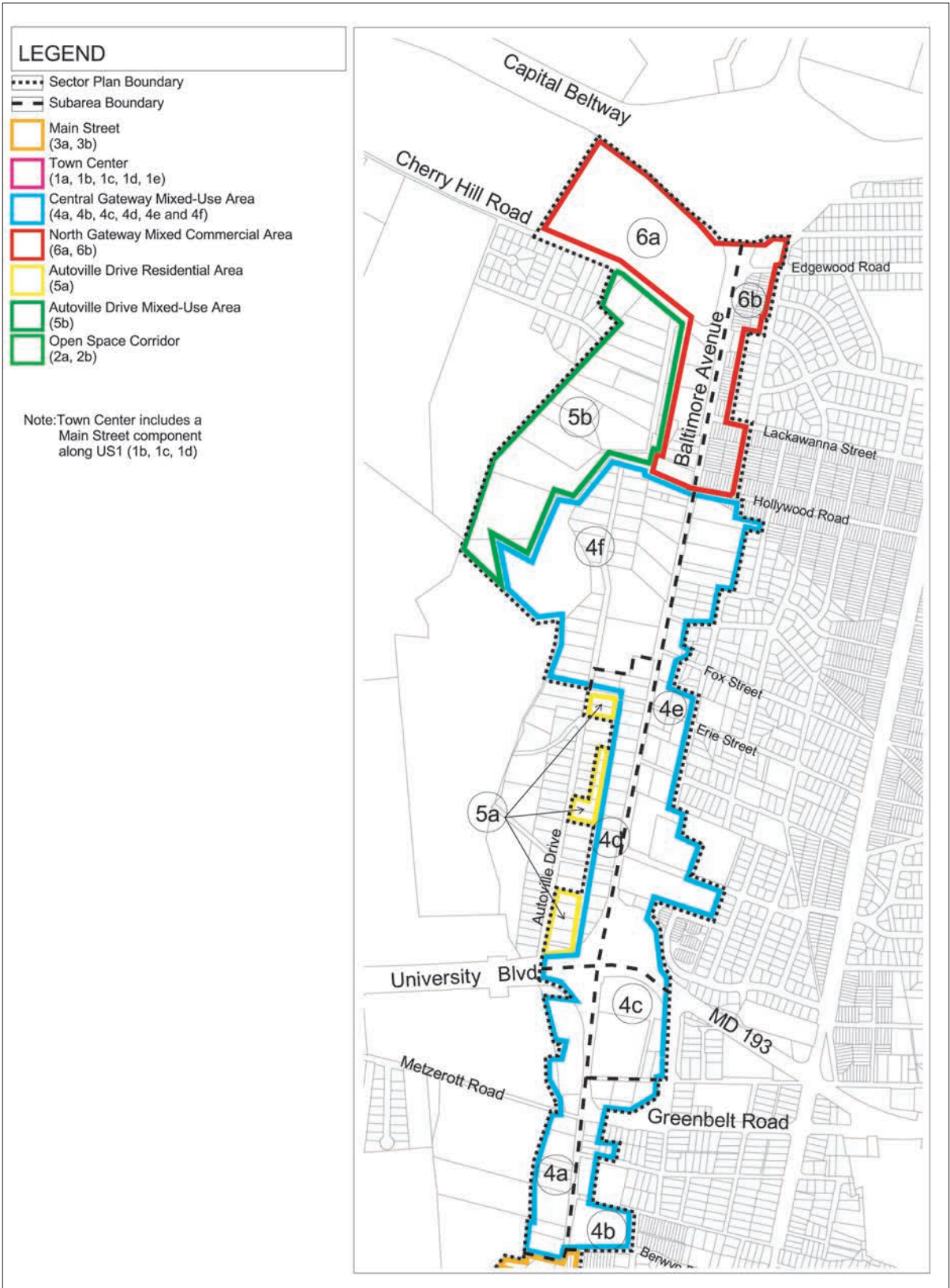
Area 6:

- 6a. West side of US 1
- 6b. East side of US 1



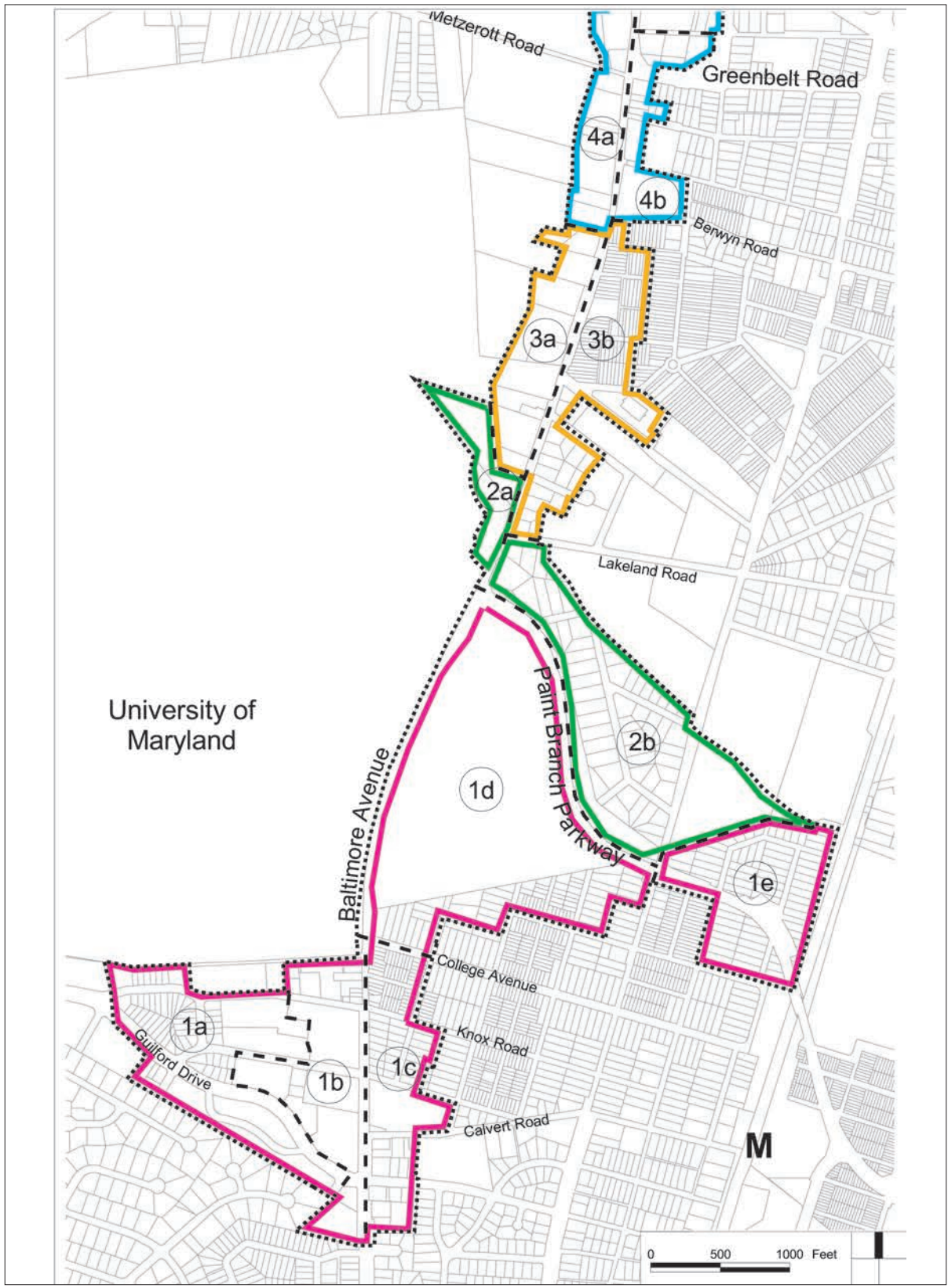
Areas and Subareas

Map 5



Development Character-North

Map 6a



Development Character-South

Map 6b



Civic uses are an important component of a town center.



Public amenities are an integral component of mixed-use developments.



Public spaces serve as the focus of activity for a town center.

- Integrate public spaces with new buildings
- Provide convenient access to transit and other nonvehicular modes of transportation
- Emphasize the spatial relationship of buildings to the street

The urban design concept is composed of the following elements:

1. Reaffirmation of the city’s vision for a town center. According to the city vision statement:

“A true ‘town center’ is created in the downtown, featuring an attractive, pedestrian-oriented environment with diverse commercial, residential and public uses, which has adequate parking and easy access by foot, bicycle, mass transit, and automobile Commercial growth has focused on revitalizing existing commercial areas, with new mixed-use land development.”

The town center concept includes the following elements:

- A pedestrian-oriented street environment and vertical mixed-use buildings
- Buildings located close to the street separated by wide sidewalks
- Convenient pedestrian access provided between uses
- Concentrations of civic, institutional, commercial, entertainment, lodging and conference facilities, residential, and various commercial retail and office uses, allowing visitors to accomplish multiple tasks with one trip
- Unobtrusive parking structures and shared parking facilities that are compatible with the design and scale of buildings

2. The promotion of compact and mixed-use development as defined below:

- *Compact development* is generally defined as buildings located close to the street frontage and close to each other. Parking is available on the street or is located to the rear of buildings. Individual buildings can be either single or mixed-use and may contain several floors. The overall impression created for the passing

pedestrian, bicyclist or motorist is immediate and interesting, and encourages individuals to enter buildings from the street. Downtown College Park, between College Avenue and Knox Road, is a good example of compact development.

- *Vertical mixed-use development* is generally defined as a combination of land uses within a single building. The typical arrangement includes retail on the ground floor, providing direct access to patrons entering buildings from adjoining streets or sidewalks. The remainder of the building is usually occupied by a single use, such as residential apartments or offices. Vertical mixed-use buildings help reduce individual travel distances and the use of automobiles.

3. Reaffirmation of a main street character for US 1 from downtown College Park to Berwyn Road. A main street character has:

- Compact development along the street edges
- A strong pedestrian environment with buildings located close to the street, separated by wide sidewalks, and with amenities such as street furniture and plazas
- A primarily retail environment with a mix of community-related shopping, institutional, service, office, residential and open space uses
- Conveniently located shared and structured parking facilities

4. Utilization of the Development District Overlay Zone (DDOZ) to regulate the design and layout of new development. Use of the DDOZ will:

- Implement new design standards through site plan review
- Provide the opportunity to review each new development for confor-



Compact development encourages pedestrian activity along the street.



Vertical mixed-use development promotes a walkable town center.

mance with the sector plan and its Development District Standards.

- Eliminate the need for the separate filing of zoning applications, special exceptions, departures from parking and loading design standards, and variances, thereby streamlining the development review process and encouraging economic reinvestment. Such issues are resolved during review of a site plan by the Planning Board, as the request must conform



The main street emphasizes retail activity in a pedestrian-friendly environment.

with the land use recommendations of the sector plan and the design standards.

- Exempt existing businesses from Development District Standards and site plan review until such time as they are enlarged or redeveloped. This is explained in more detail in the applicability section of the Development District Standards chapter.
5. Use of an illustrative concept drawing to suggest a possible redevelopment scenario along with illustrating urban design principles and relationships. Maps 8-1 through 8-6, at the end of this chapter, depict the Illustrative Concept. The maps depict an integrated land use concept with arrangements of buildings, public spaces, amenities, access roads, and parking locations.

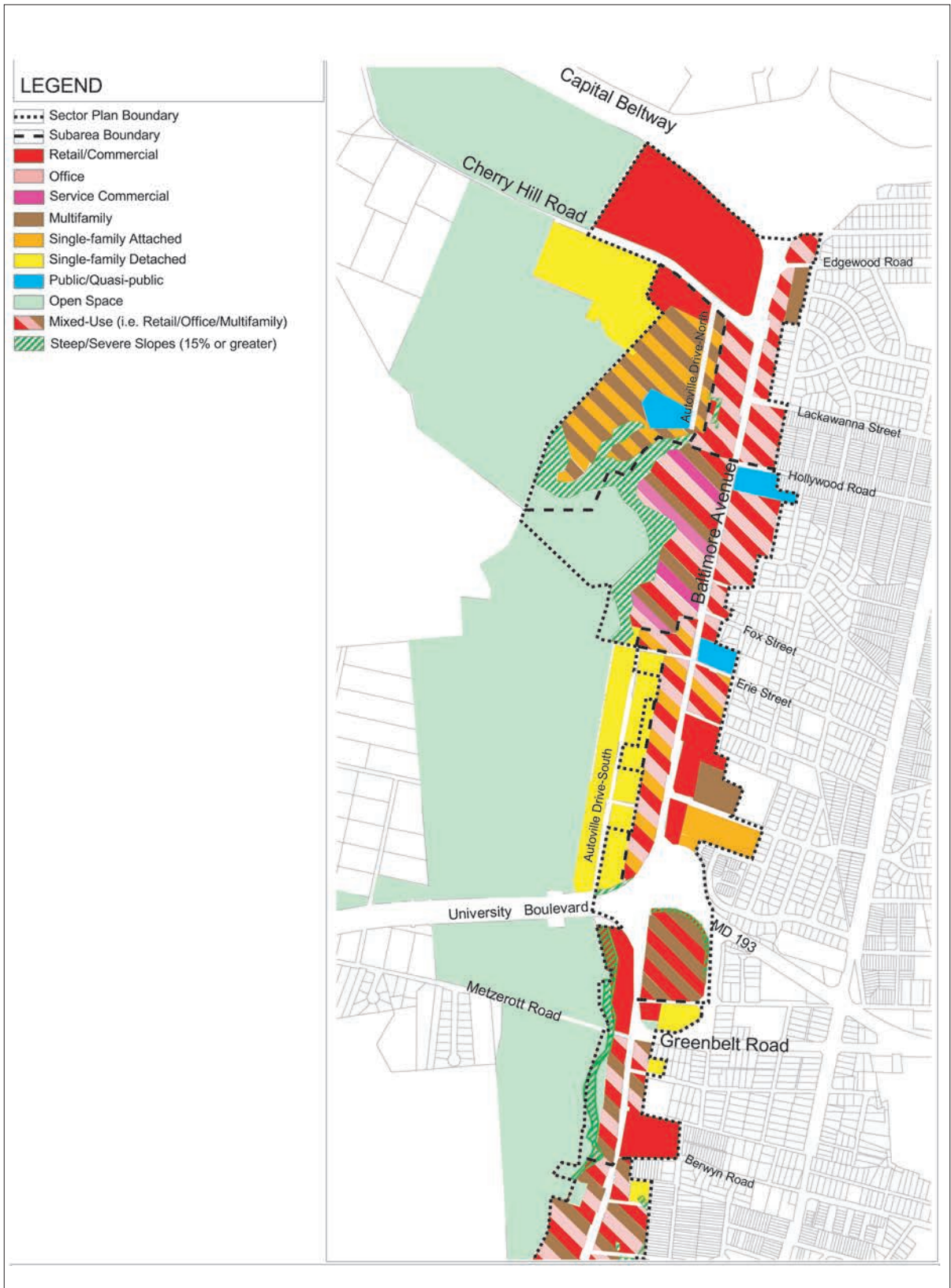
RECOMMENDATIONS

Based on identified land use goals, issues, and the above concepts, the plan makes eight corridor-wide recommendations regarding land use and zoning, and three corridor-wide recommendations regarding urban design. In developing these recommendations, the plan has minimized impacts upon existing businesses. The approved land use plan is shown on Map 7.

CORRIDOR-WIDE LAND USE AND ZONING RECOMMENDATIONS

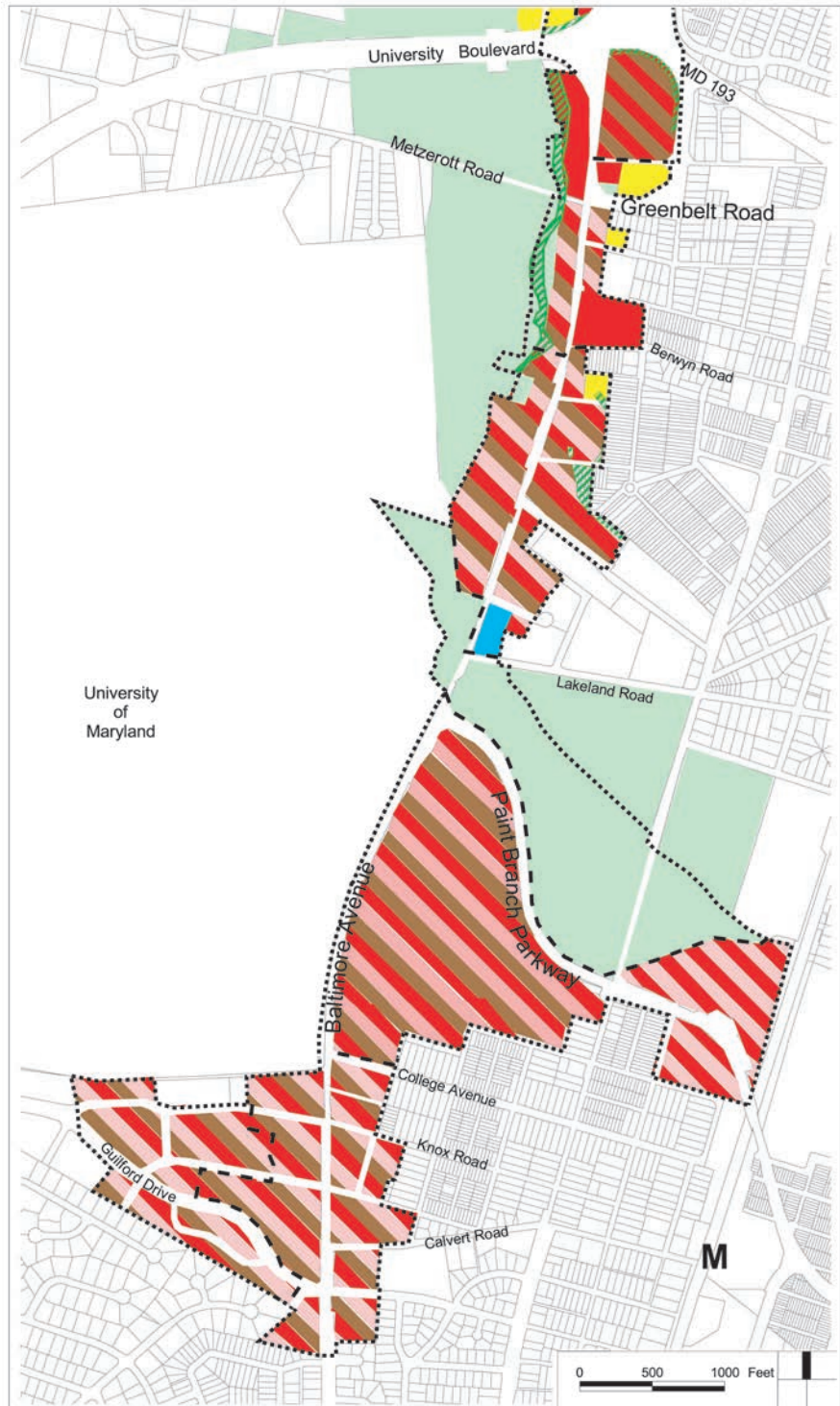
1. *Reconstruct US 1 as a gateway boulevard to improve both the function and the character of the roadway*—As US 1 is reconstructed in accordance with the final plans approved by SHA, it will include the various vehicular, pedestrian, bicycle, transit, and aesthetic enhancements ultimately agreed to by the community.
 - The plan supports the four-lane divided reconstruction alternative proposed by SHA for the US 1 corridor within the sector plan area (see Transportation and Circulation chapter for further discussion).
 - The plan recommends that every attempt to consolidate and manage access points be pursued during detailed site plan review. The same applies to the review of parking with the aim to encourage shared and/or structured parking.
2. *Encourage existing businesses to be part of the redevelopment process*—The plan encourages the relocation of existing freestanding businesses into new, mixed-use buildings or onto redeveloped property.
 - During land assembly negotiations, every effort should be made to accommodate existing businesses within the context of redevelopment.
 - The applicability section of the Development District Standards chapter provides specific exemptions to existing

- uses from the standards and site plan review.
3. *Establish a specific character for each area and subarea*—A specific development character intended to guide the type of use and form of development is recommended for each area and subarea within the sector plan, as discussed in the Area and Subarea Recommendations section of this chapter.
 4. *Establish a Development District Overlay Zone (DDOZ) coterminous with the sector plan area*—The DDOZ establishes development standards to implement the sector plan’s land use and urban design recommendations. The development standards will regulate the form and type of new development (see the Development District Standards chapter for additional discussion).
 5. *Change existing zoning to reflect the sector plan’s land use recommendations*—The plan utilizes a new mixed-use zone that allows residential, commercial or mixed residential and commercial infill development. The Mixed-Use-Infill (M-U-I) Zone is intended to provide development flexibility in responding to market needs by allowing residential and/or commercial use in appropriate locations. It can also be applied upon individual application and approval of a rezoning request under provisions of the DDO Zone after SMA adoption. The SMA applies the M-U-I Zone in instances where the Commercial Shopping Center (C-S-C) Zone, Commercial Office (C-O) and General Commercial (C-2) Zone exist. It is also applied where residentially zoned properties are owned by a public entity and land assembly is not an issue. Existing automobile dealerships, formerly located in the C-2 Zone, are recognized as permitted uses within the M-U-I Zone in the DDOZ’s Table of Permitted Uses.
 6. *Retain existing zoning in certain instances*—Throughout the sector plan area, private residential property is in small, individually owned lots where land assembly is needed before redevelopment can occur. The SMA has not rezoned private residential properties because no redevelopment proposal was pending at the time of SMA approval, except where land assembly had occurred in Subarea 5b. These properties could be rezoned under provisions of the DDOZ at such time as sufficient land assembly has occurred to support approval of a development proposal found to be in conformance with both the sector plan’s land use and the DDOZ’s development district standards.
 7. *Retail, office, hotel and multifamily uses are recommended as the most intensive uses within the sector plan area.* The intent of the plan is to prohibit industrial uses, including consolidated storage and warehouses, and to limit new auto-related sales and services to specific subareas. Also, uses with drive-in or drive-through windows are prohibited in Subareas 1 and 3, where compact, pedestrian-oriented development should be located. The exception is for existing uses where drive-in or drive-through windows must be removed because of government actions. In such cases, they shall be considered permitted uses in Subarea 3 only. In addition, the Table of Permitted Uses (Tables 17 and 18) further restricts other uses deemed undesirable within the context of achieving the goals of this sector plan and the city’s visions for the US 1 Corridor and town center.
 8. *Allow expansion of the College Park DDOZ boundary only under specific conditions*—The College Park DDOZ is coterminous with the sector plan boundary. However, the DDOZ regulations allow amendments to the DDOZ boundary, underlying zoning and list of



Approved Land Use Plan-North

Map 7a



Approved Land Use Plan-South

Map 7b

permitted uses by the District Council, or upon petition of a property owner whose land is either in, adjoining, or separated by a right-of-way from the development district.

- The sector plan recognizes the possibility of future requests to expand commercial zoning beyond the plan boundary. This could include the rear portion of blocks along the east side of US 1 in subareas 3b, 4b, 4c, 4e and 6b. This potential to extend zoning and commercial land use into these split block areas should only relate to possible right-of-way expansion along the east side of US 1 and the impact such expansion may have upon existing businesses fronting US 1 in these narrow portions of the sector plan area.

Also, it is recognized that owners of properties outside of but adjoining an area in the DDOZ could request a change in the DDOZ boundary and a change in the property’s underlying zone if the property is added to the DDOZ. In all cases, the plan recommends the following be evaluated in any request to expand the sector plan boundary:

- (1) Any expansion of the boundary beyond the zoning boundaries granted as part of this plan’s sectional map amendment should occur only to accommodate existing business uses that are impacted by right-of-way expansion along US 1
- (2) Lots acquired for purposes of expansion should abut the commercial property to be expanded and comprise contiguous lots in the expansion area
- (3) All existing residential structures should be removed from the expansion area
- (4) Access to the expanded DDOZ area should not be from residential streets parallel to US 1

(5) All use of the property and new buildings shall conform to the DDOZ design standards

(6) In no event should expansion occur beyond the first block east of US 1.

(7) In no event should expansion of the DDOZ boundary occur west of Subarea 5a to accommodate commercial use.

9. *Proximity to College Park Airport* – Because portions of the sector plan area are proximate to the College Park Airport, strategies to develop these areas in a manner compatible with airport operations should be part of the development planning process.

CORRIDOR-WIDE URBAN DESIGN RECOMMENDATIONS

The following three general urban design recommendations provide a framework for the gateway boulevard, town center and main street concepts:

1. *Establish a bicycle- and pedestrian-friendly gateway boulevard along the entire US 1 corridor with priority given to implementing the following:*
 - Compatible street furnishings, ornamental light fixtures and banners as design elements to functionally and visually tie the three-mile corridor together
 - Sidewalks that connect to all areas within the corridor and are wide enough to accommodate various users, while linking with a convenient and visible pedestrian and bicycle off-road trail network
 - Trees to provide shade and screening lining the edges of Baltimore Avenue and Paint Branch Parkway
 - Crosswalks in contrasting materials and pedestrian crossing signals along US 1 and Paint Branch Parkway to create highly visible and safe crossing opportunities



Crosswalks in contrasting paving materials will create safe crossing opportunities for pedestrians along the entire US 1 corridor.

- Pedestrian bridge crossings over Paint Branch to connect the university and business establishments along the corridor
- A consistent informational system of welcome, directional and identification signs throughout the corridor to assist visitors



A wayfinding sign program of welcome, directional and identification signs will visually tie together the three-mile-long corridor and provide needed information for visitors.

- Information kiosks in highly visible locations
- Overhead utility lines relocated underground along US 1
- Appropriate management systems and partnerships between the city, county, university, business community and others to implement many of the above elements

2. *Treat the street environment as an important public space for establishing the character of the corridor*—The typical suburban development pattern of buildings with large parking lots set back from the street should be changed to promote pedestrian activity instead of the auto-dominated environment which currently exists along the corridor.

- Buildings should be placed directly facing the street, separated by wide



Buildings with inviting street edge treatments will enhance the character of the town center.

sidewalks, with parking provided in lots located to the sides or rear of properties

- The primary building entrance should be provided from the street to facilitate convenient pedestrian connections
- Buildings should incorporate pedestrian-scaled features such as awnings, arcades, display windows and balconies to respond to the human activity at street level



Outdoor dining will animate the street environment in the town center.

3. *Promote property maintenance with active code enforcement*—Commercial encroachment into residential areas and poorly maintained properties are detrimental to the community as a whole and foster negative perceptions by residents, commuters and visitors. Physical evidence of neglect should be eliminated through the timely removal of dilapidated structures, graffiti and litter. Proactive steps should be taken by the city and county to encourage the continued maintenance and upkeep of commercial properties through strict and prompt code enforcement.

AREA AND SUBAREA RECOMMENDATIONS

The following land use and urban design recommendations establish the preferred mix, type and form of development desired in the six areas and their subareas. (See the Development

Character Map 6; Approved Land Use Plan Map 7; and the Illustrative Concept Maps 8-1 through 8-6.)

During creation of the subarea land use recommendations it was determined that each subarea should convey a certain development character. Once the development character was defined, the plan's Table of Permitted Uses (Tables 17 and 18) was developed to assist in creating the desired character for each subarea by permitting or prohibiting certain uses in specific subareas as indicated by footnotes 37, 38 and 39 in Table 17. The footnotes indicate prohibited uses in specific subareas where such uses are considered inconsistent with the plan's recommended development character and the city's visions for the corridor.

Area 1: Town Center—The town center is located at the southern end of the gateway boulevard between Paint Branch Parkway on the north and Guilford Drive on the south. It encompasses the existing commercial area known as downtown College Park (1b and 1c), the multifamily residential areas to the west of downtown (1a), and The University of Maryland East Campus area (1d and 1e), located east of US 1.

Recommended Characteristics—The vision for this area is to have an expanded town center that is a focus of community activity. The main street shopping area along Baltimore Avenue should be expanded and enhanced to include additional vertical mixed-use buildings with ground floor commercial uses. Existing student housing areas should be redeveloped as new student neighborhoods with appropriate commercial and recreational facilities. Other compact, mixed-use development should be encouraged to include additional office, hotel or conference space, entertainment uses, public buildings, plazas and urban open space. Both public and private parking structures should be incorporated into plans for new development. Where this is not possible, shared use parking is encouraged.



Opportunities exist to create a new university-related residential neighborhood in the “Knox Box” area.

Subarea Recommendations:

1a. West of Downtown—This area west of downtown and south of The University of Maryland campus is currently developed with a mix of student housing, including the high-rise College Park Towers, the Graduate Garden apartments and the four-plex units known as the “Knox Boxes,” among other uses. Recommendations for this area include:

- Land assembly and comprehensive development of the “Knox Boxes” area with a variety of attached and multifamily housing suitable for faculty, graduate and undergraduate students
- A mix of service-oriented retail and office uses that minimize the need for vehicular trips
- Compact and vertical mixed-use development
- Possible reorientation of internal roadways and enhanced pedestrian connections and open space
- The use of structured and/or shared parking
- Rezoning as necessary to provide a mix of uses after land is assembled and a development proposal prepared

1b. West side of US 1—This downtown main street commercial area has frontage

along the west side of Baltimore Avenue and is located between the south gate of the university and Pine Way Road in University Park. It has a mix of retail, office and hotel uses, including the College Park Shopping Center and the Quality Inn, among other uses. Recommendations for this area include:

- Development with vertical mixed-use buildings to enhance the main street character and to serve both students and local neighborhoods
- Compact infill development
- Entertainment uses that are not incompatible with the community
- The use of structured and/or shared parking
- The introduction of plazas, public open space areas, improved pedestrian connections, street furniture, and public art
- Rezoning as necessary to provide a mix of uses



Additional vertical mixed-use buildings should be located throughout downtown College Park.

1c. East side of US 1—This downtown main street commercial area has frontage along the east side of Baltimore Avenue and is located between College Avenue on the north and Fordham Drive on the south. It is adjacent to the residential neighborhoods of Old Town and Calvert Hills. Recommendations for this area include:

- Development with vertical mixed-use buildings to enhance the main street character and serve both students and local neighborhoods
- Compact infill development
- Adequate screening, buffering and tapering of building heights adjacent to single-family neighborhoods
- Structured and/or shared parking
- The introduction of plazas, public open space areas, improved pedestrian connections, street furniture, and public art



Ritchie Coliseum contributes to the academic setting of buildings facing Baltimore Avenue.

1d. South of Paint Branch Parkway—This large area is known as the East Campus district of The University of Maryland and contains Fraternity Row, Ritchie Coliseum, student dormitories, office space, greenhouses, power and physical plant buildings, and vehicle maintenance and repair services, among other uses. Recommendations for this area include:

- Development with a vertical and horizontal mix of uses serving both the university and the community, if existing university facilities can be relocated. Types of uses recommended are hotel/conference center, high-tech, general and flex-office space, multifamily and student housing, retail, restaurant, and entertainment such as movie theaters

- Development should be oriented around a significant public gathering space
- The street frontage along US 1 should reflect the main street character of subareas 1b and 1c



The existing main street character should be enhanced with compact development forms.

- Retention of Fraternity Row, Ritchie Coliseum, and the power plant
- Consideration of a bandstand, sculpture garden or other public uses in the open space area of Fraternity Row
- Improvements to transform Paint Branch Parkway into a boulevard including a landscaped median and a sidewalk on the north side of the road
- Consideration of direct vehicular and pedestrian access to Paint Branch Parkway at appropriate locations
- Pedestrian and bicycle connections to the campus area west of US 1, to the College Park-University of Maryland Metro Station, and to the Old Town neighborhood
- Screening, buffering and tapering of building heights adjacent to the Old Town neighborhood
- Structured and/or shared parking

1e. Metro-Related Area—This area is adjacent to the College Park-University of Maryland Metro Station and is owned by the university. The properties are

improved with the Maryland Institute for Fire Research and Engineering north of Paint Branch Parkway and a warehouse facility on the south side. Recommendations for this area include:

- Development for office and high-tech uses, with some convenience retail
- Compact development
- Pedestrian-friendly linkages to the College Park Metro Station
- Transitioning of building heights adjacent to existing residential areas
- No intrusion into the environmentally sensitive areas of the Paint Branch Creek

AREA 2: Open Space Corridor—This area comprises a portion of the environmentally sensitive Paint Branch Stream Valley Park and associated streams, floodplain, woodlands and wetlands along the north side of Paint Branch Parkway and a small area west of US 1. It is currently undeveloped open space.

Recommended Characteristics: This area is part of an open space corridor or greenway and is a valuable amenity that should be retained. It also has the potential to enhance the pedestrian circulation between the university, US 1 businesses, and the Metro station. The entire area should be designated as part of the environmental overlay tier as recommended in the final report of Commission 2000. Such an area may be improved with gateway park components, including trails, boardwalks, stream crossing bridges, rest areas, and passive recreational space.

Subarea Recommendations:

- 2a. West side of US 1—This undeveloped area lies between the main campus and US 1 businesses. It is owned by the university and M-NCPPC. Recommendations for this area include:
- Development of a formal gateway park with linkages to the university and US 1 via a pedestrian bridge and trails

2b. North of Paint Branch Parkway—This undeveloped area lies between Paint Branch Parkway on the south and Lakeland Park to the north. Recommendations for this area include:

- Protection of important natural features
- Passive recreational uses that provide opportunities for access into this natural area

AREA 3: Main Street—This area encompasses both sides of US 1 north of Lakeland Road and south of Berwyn Road. It is characterized by a mix of commercial uses including the Campus Village Shopping Center, several fast-food restaurants, and the Koon’s Ford new and used car dealerships, among other uses.

Recommended Characteristics—The vision for this area is that of a neighborhood main street district featuring a compact mix of retail shopping, restaurants and offices. There are opportunities for retail infill development to meet the demand for office and high-tech uses in close proximity to the research and engineering facilities of the university.

Subarea Recommendations:

- 3a. West side of US 1—This area has frontage along the west side of US 1 and is adjacent to the Paint Branch Stream Valley Park and the Engineering/Sciences district of the university. Recommendations for this area include:
- Compact development with offices located above ground floor retail to take advantage of technology linkages to the university
 - Vertical, mixed-use development where feasible outside of the floodplain
 - Compliance with Prince George’s County floodplain regulations in the portion of the subarea impacted by floodplain



Pedestrian bridges over Paint Branch will provide convenient connections between The University of Maryland campus and the US 1 corridor.

- A rear service road to improve access and circulation as part of a comprehensive redevelopment effort
 - Introduction of a one-way service lane adjacent to US 1 with on-street parking between Berwyn Road and Berwyn House Road
 - Shared and/or structured parking
 - Pedestrian bridges across Paint Branch Creek to connect with the campus over a system of trails and boardwalk
 - No building balconies for housing facing directly onto US 1
- 3b. East side of US 1—This area has frontage on the east side of US 1 and is adjacent to the Lakeland and Berwyn neighborhoods. Recommendations for this area include:
- Vertical, mixed-use development with ground floor retail
 - Compact infill development
 - Screening, buffering and tapering of building heights adjacent to residential areas
 - Compliance with Prince George’s County Floodplain Regulations for that portion of the subarea in the floodplain
 - Shared and/or structured parking

- No building balconies for student housing facing directly onto US 1

AREA 4: Central Gateway Mixed-Use Area—This area extends from Berwyn Road north to Hollywood Road. It is characterized by many hotel/ motels and freestanding retail and service businesses. It also has several vacant and blighted structures, including the former Starlight Inn, among other uses. The neighborhoods of Autoville/Cherry Hill, Hollywood, Daniels Park/Branchville and Berwyn adjoin this area of US 1.

Recommended Characteristics: The vision for this area is a mix of uses with an emphasis on new residential development if adequate land can be assembled to provide amenities necessary for high-quality development. Comprehensive redevelopment of certain areas is encouraged along with the protection of adjacent single-family areas.

Subarea Recommendations:

4a. West side of US 1, south of MD 193/University Boulevard—This area adjoins the Paint Branch Stream Valley Park and is a mix of auto-related uses including a car wash, hotel, Jiffy Lube and 7-Eleven convenience store, among other uses. Recommendations for this area include:

- Compact, mixed-use development
- Maximize the views of parkland
- Maintenance of appropriate stream buffers
- Shared parking

4b. East side of US 1, south of Branchville Road—This area lies between Branchville Road to the north and Berwyn Road to the south. It adjoins the Berwyn neighborhood to the east. It contains the Best Western Motel, the Veteran’s Memorial, and auto sales and service establishments, among other uses. Recommendations for this area include:



The Veteran's Memorial Park is recommended for expansion.

- Compact infill development including single-family attached, multifamily residential buildings and commercial uses
 - Screening, buffering and tapering of building heights adjacent to single-family residential uses
 - Expansion and improvement of the Veteran's Memorial Park
 - No building balconies for housing facing directly onto US 1
- 4c. Southeast corner of US 1/MD 193— This area between MD 193 and Branchville Road is improved with a shopping center, bowling alley, car repair, Pizza Hut, and a single-family home, among other uses. Commercial zoning extends to 48th Avenue, giving it much more depth than other US 1 loca-

tions. Recommendations for this area include:

- Land assembly with compact and vertical mixed-use development for condominiums, multifamily apartments with ground floor retail, office or hotel
 - Shared and/or structured parking
- 4d. West side of US 1, north of MD 193— This area lies between MD 193 and Fox Street and only includes the shallow, commercially zoned lots between US 1 and the single-family neighborhood along Autoville Drive. It is improved with a mix of freestanding businesses with large parking lots, including restaurants, motels, auto repair and service stations, among other uses. It is also the site of the American Legion. Recommendations for this area include:
- No expansion of commercial uses into the existing single-family residential area
 - Development with attached single-family residential uses
 - Compact, infill development with retail or office uses
 - Screening, buffering and tapering of building heights adjacent to single-family neighborhoods
 - Primary access should be from US 1
- 4e. East side of US 1, north of MD 193— This area lies between MD 193 and Hollywood Road and is adjacent to the Daniels Park/Branchville neighborhood. It contains several motels, retail uses, camping vehicle establishments, the Compassion Center, College Park Post Office, and vacant property zoned for townhouses, among other uses. Recommendations for this area include:
- Compact, infill development with attached and limited detached single-family residential, retail and/or office uses

- Screening, buffering and tapering of building heights adjacent to residential areas
 - Possible redevelopment as an auto sales and service park in conjunction with subarea 4f
- 4f. West side of US 1, between Fox Street and Hollywood Road—This area along US 1 is dominated by auto-related uses, including large car dealerships, among other uses. Land to the west of the unimproved Autoville Drive is vacant and wooded with steep slopes. Recommendations for this area include:
- Redevelopment for an auto sales and service park. Other auto-related uses on the corridor could be consolidated at this location
 - Redevelopment with a mix of commercial uses is an alternative, including retail and office along US 1, and multifamily residential uses to the rear, oriented to take advantage of views into the stream valley park
 - A new road to provide alternative access to the subarea by extending Autoville Drive (as proposed Autoville Drive North) to the south with a connection to an extended Hollywood Road and Fox Street, with traffic signals at US 1 (see Transportation and Circulation chapter for details)
 - No connection of proposed Autoville Drive North with Autoville Drive South

AREA 5: Autoville Drive Residential Area—This area represents existing single-family detached development in two locations off of US 1. Subarea 5a is located between Erie Street and University Boulevard and is improved with single-family homes on small lots on both sides of Autoville Drive South. Subarea 5b is south of Cherry Hill Road and west of existing Autoville Drive North. It is improved with single-family homes on large wooded parcels. Autoville Drive is discontinu-

ous and these two neighborhoods are separated by subarea 4f.

Recommended Characteristics: The vision for Subarea 5a is to retain a residential character. Subarea 5a is almost fully developed with single-family detached housing and should be protected from commercial encroachment. Subarea 5b is sparsely developed and offers the opportunity for more dense mixed-use development following traditional neighborhood design principles. Although planned for ultimate mixed-use development, the central portion of Subarea 5b was retained in the R-55 Zone to accommodate existing residents.

Subarea Recommendations:

- 5a. Autoville Drive South—Recommendations for this area include:
- Protect the integrity of the existing neighborhood by prohibiting further commercial encroachment and providing screening and buffering adjacent to existing single-family residences
 - Infill development with single-family detached residential along the east side of Autoville Drive
 - No connection of Autoville Drive South with Autoville Drive North



The character of the existing Autoville Drive south neighborhood should be retained and protected from commercial encroachment.

- 5b. Autoville Drive North—Recommendations for this area include:

- Comprehensively planned redevelopment with a mix of attached single-family and multifamily housing types. Some existing single-family detached housing could be retained
- Limited retail development facing Cherry Hill Road at such time as Autoville Drive North is relocated
- Relocation of Autoville Drive North to the west to align with the existing traffic signal
- Upgrading and reconfiguring Autoville Drive (as Autoville Drive North) to accommodate new development (see Transportation and Circulation chapter for details)
- Rezoning the central portion to the M-U-I Zone after land is assembled and a development proposal is prepared
- Protection of sensitive environmental features and provision of site amenities
- Provision of a trail connection to link new development with the Paint Branch Stream Valley Park



Opportunities exist to create a mixed residential neighborhood adjacent to the Paint Branch Stream Valley Park.

AREA 6: North Gateway Mixed Commercial Area—This area extends from the Capital Beltway to Hollywood Road on the east side of US 1 and its proposed extension on the west side. It is a mix of mostly older commercial development on the east side adjacent to the Hollywood neighborhood and new develop-

ment on the west side adjoining the Autoville neighborhood

Recommended Characteristics: This area is recommended to retain a variety of commercial development without the introduction of residential uses. If the intersection of US 1 and Cherry Hill Road is reconstructed with a grade-separated interchange as proposed by SHA, there would be little or no opportunity for redevelopment north of Hollywood Road.



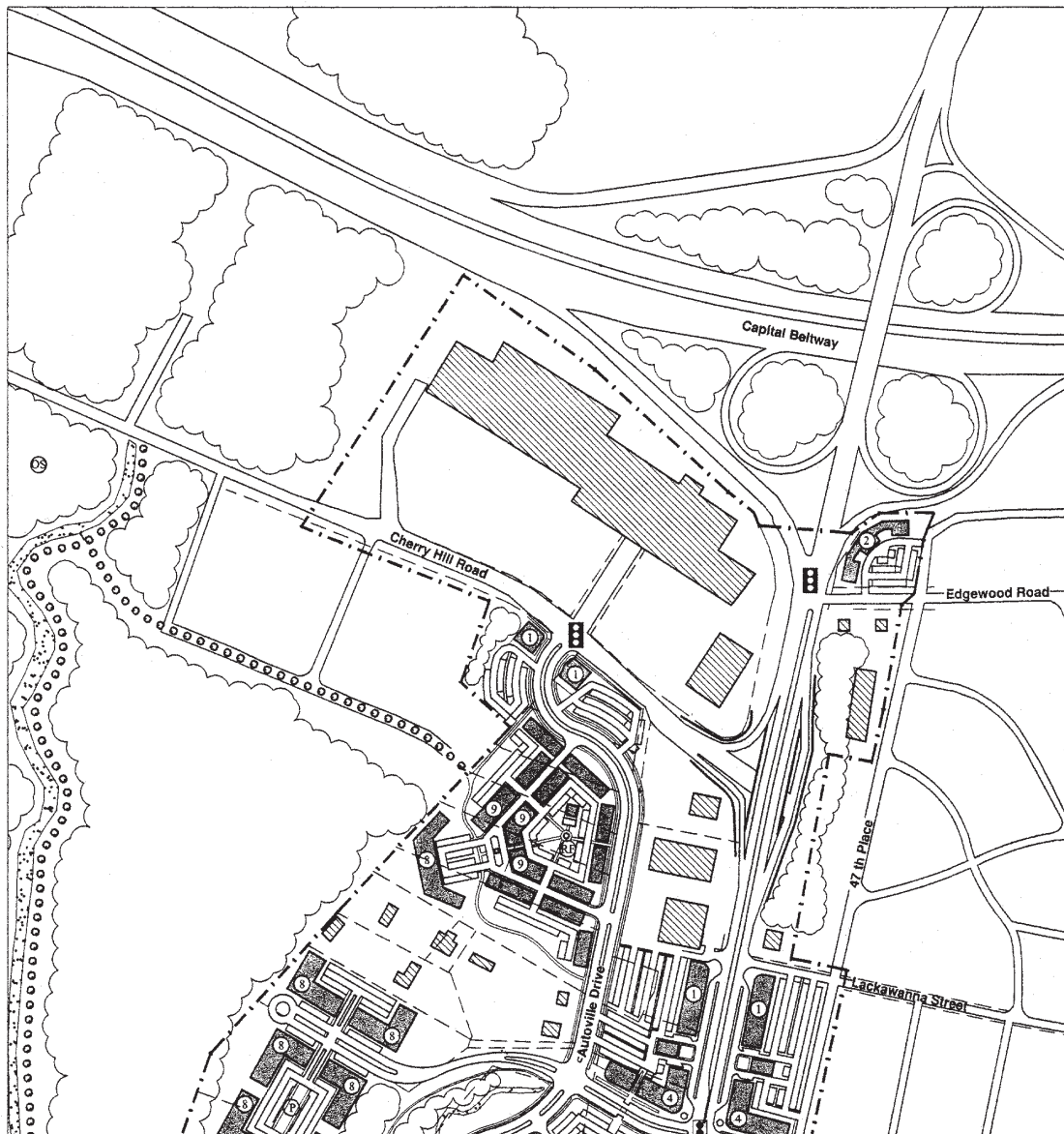
The College Park Marketplace shopping center provides retail services for the surrounding community.

Subarea Recommendations:

- 6a. West side of US 1—This area extends to the city’s northern and western boundaries and includes the new College Park Marketplace Shopping Center, IHOP and Hampton Inn. Older buildings with auto-related uses are in the southern part of this subarea. Recommendations for this area include:
 - Compact infill development of older properties with a mix of commercial uses
 - Screening, buffering and tapering of heights adjacent to single-family residential areas
- 6b. East side of US 1—This area from Hollywood Road to the Capital Beltway is characterized by a mix of retail, service and office uses, including the Remax Building, McCormick Paints, Precision Small Engines, Dunkin Donuts, and other commercial uses. Most development is located very close to the street

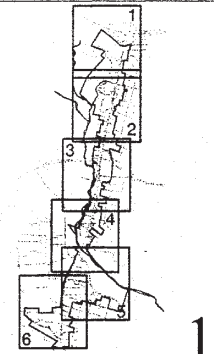
within the proposed right-of-way for US 1. Businesses north of Lackawanna Street may be displaced by construction of a new interchange at US 1 and Cherry Hill Road. Recommendations for this area include:

- Development with a mix of retail, office and service-commercial uses
- Compact infill development
- Screening, buffering and tapering of building heights adjacent to single-family residential areas



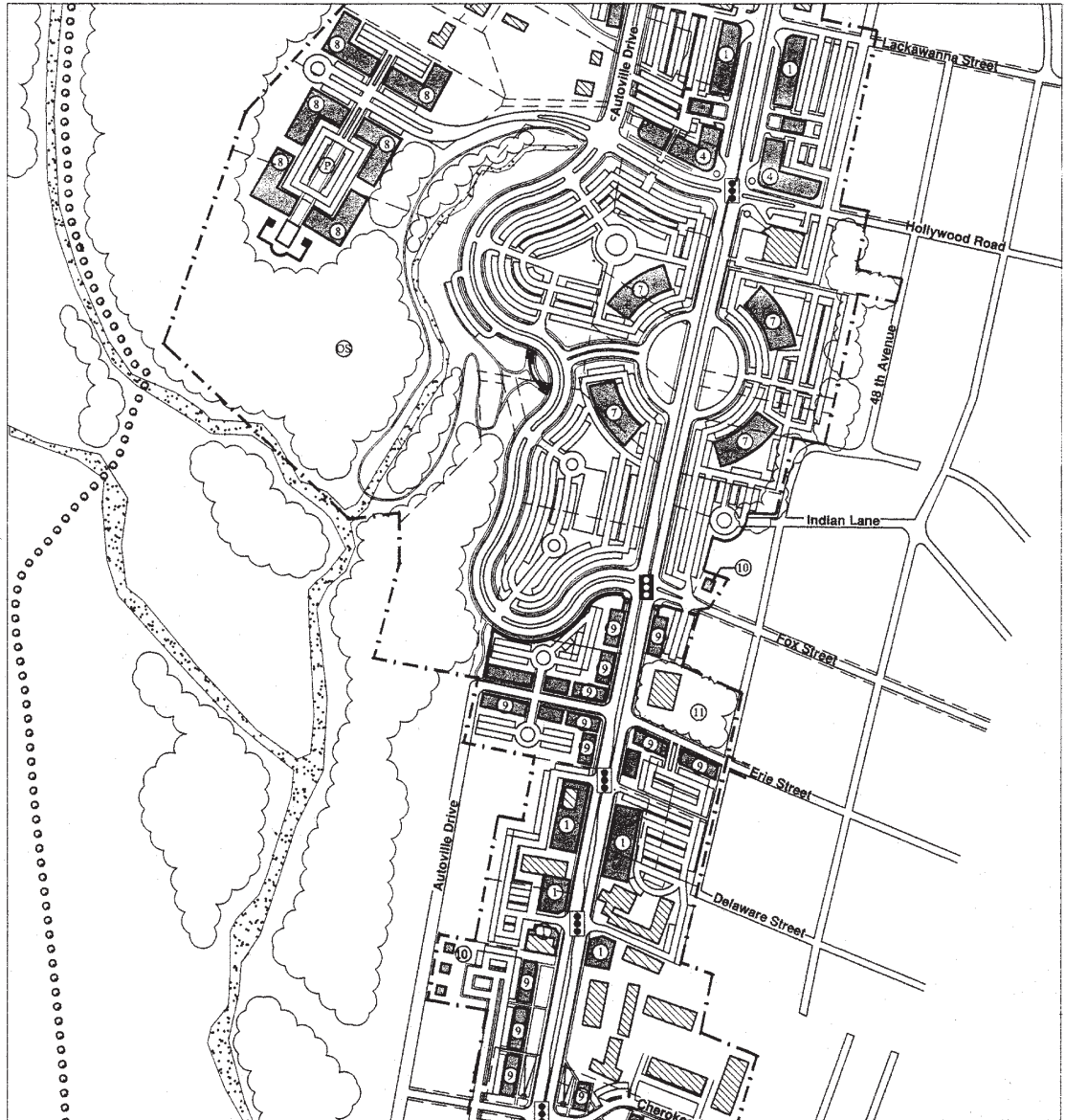
LEGEND-

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|--|---|--------------------------------|--------------------------------------|
| ① Retail / Commercial | ⑧ Residential / Multifamily | P Parking Structure | Trails |
| ② Office | ⑨ Residential / Single-Family Attached | ▭ Sector Plan Boundary | Woodlands |
| ③ Hotel / Conference Center | ⑩ Residential / Single-Family Detached | ▨ Existing Buildings To Remain | Ⓜ Existing Traffic Signals To Remain |
| ④ Mixed-use Ground Floor Retail / Office | ⑪ Institutional / Public / Quasi-Public | ▩ Potential Building Locations | Ⓜ Potential Traffic Signal Locations |
| ⑤ Mixed-use Ground Floor Retail / Multifamily | Ⓜ Urban Park | ▬ Paint Branch Stream | ▭ Property Line |
| ⑥ Mixed-use Ground Floor Retail / Office / Multifamily | Ⓜ Recreational Facilities (Private) | | |
| ⑦ Service Commercial / Auto Related | Ⓜ Open Space | | |



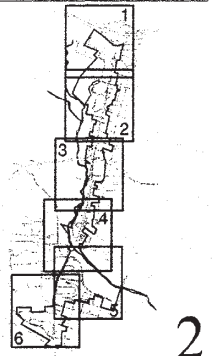
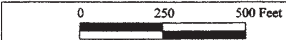
Illustrative Concept
Source: Rhodeside & Harwell, Inc.

Map 8-1



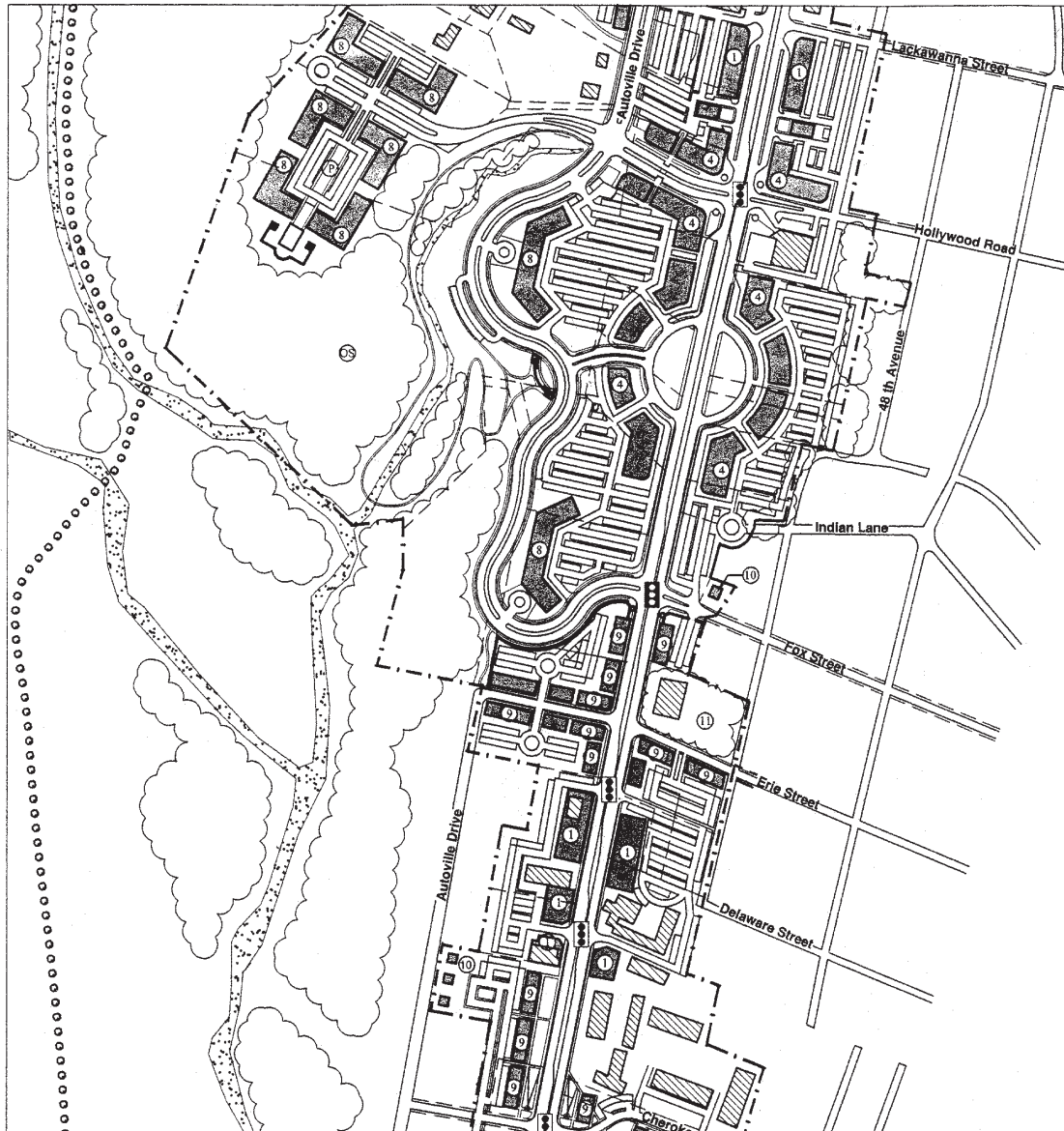
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| ① Retail / Commercial | ⑧ Residential / Multifamily | P Parking Structure | Trails |
| ② Office | ⑨ Residential / Single-Family Attached | ▭ Sector Plan Boundary | Woodlands |
| ③ Hotel Conference Center | ⑩ Residential / Single-Family Detached | ▨ Existing Buildings To Remain | Ⓜ Existing Traffic Signals To Remain |
| ④ Mixed-use Ground Floor Retail / Office | ⑪ Institutional / Public Quasi-Public | ▩ Potential Building Locations | Ⓜ Potential Traffic Signal Locations |
| ⑤ Mixed-use Ground Floor Retail / Multifamily | ⑫ Urban Park | ▬ Paint Branch Stream | ▭ Property Line |
| ⑥ Mixed-use Ground Floor Retail / Office / Multifamily | Ⓜ Recreational Facilities (Private) | | |
| ⑦ Service Commercial / Auto Related | Ⓝ Open Space | | |



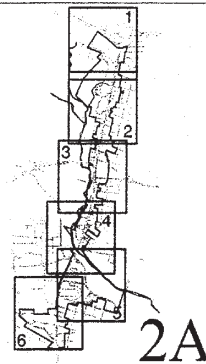
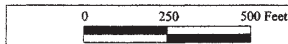
Illustrative Concept
Source: Rhodeside & Harwell, Inc.

Map 8-2

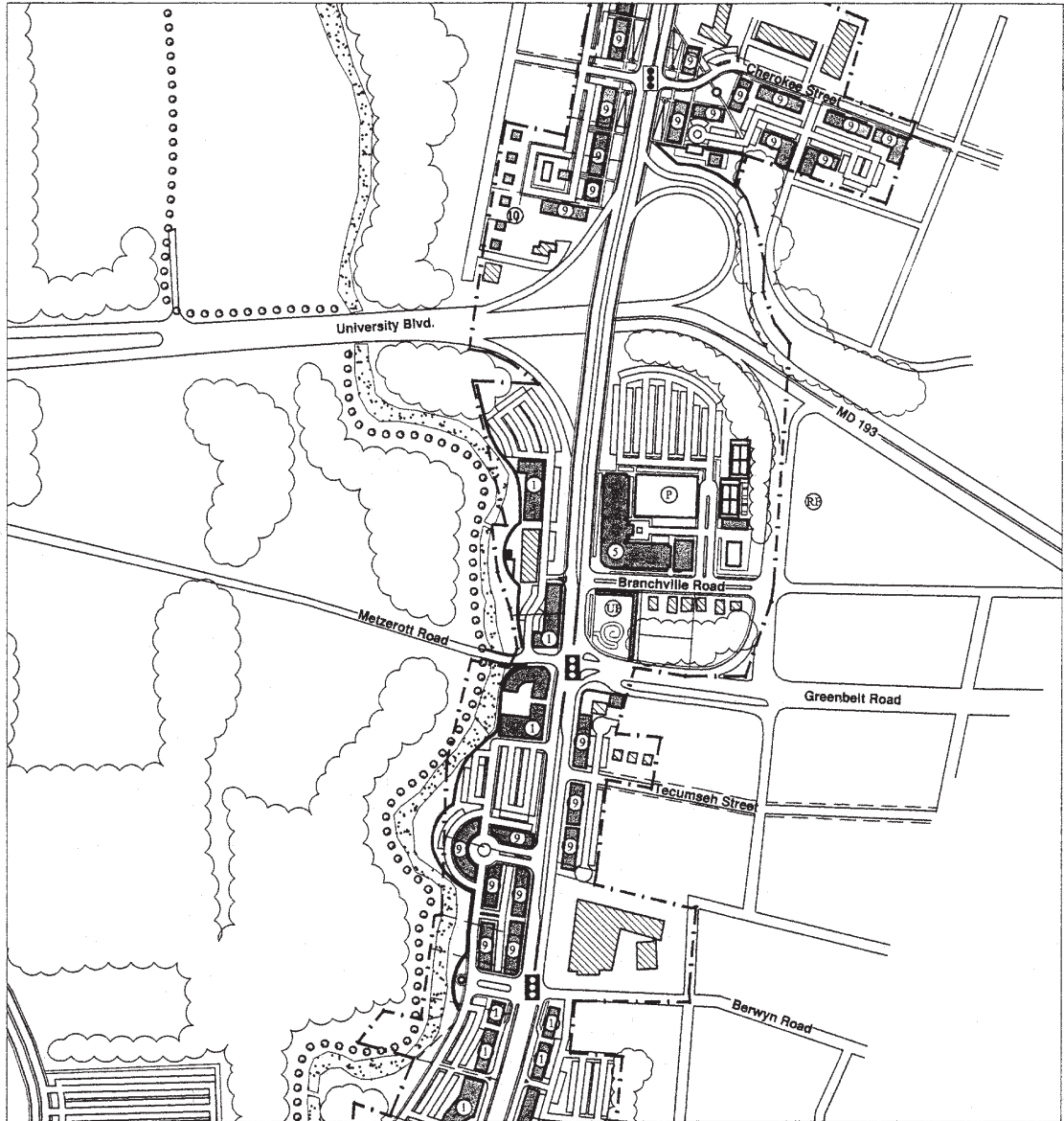


LEGEND-

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| ① Retail / Commercial | ⑧ Residential / Multifamily | Ⓧ Parking Structure | ⋯ Trails |
| ② Office | ⑨ Residential / Single-Family Attached | ▭ Sector Plan Boundary | ☁ Woodlands |
| ③ Hotel / Conference Center | ⑩ Residential / Single-Family Detached | ▨ Existing Buildings To Remain | Ⓜ Existing Traffic Signals To Remain |
| ④ Mixed-use Ground Floor Retail / Office | ⑪ Institutional / Public / Quasi-Public | ▩ Potential Building Locations | Ⓡ Potential Traffic Signal Locations |
| ⑤ Mixed-use Ground Floor Retail / Multifamily | Ⓧ Urban Park | ▬ Paint Branch Stream | ▭ Property Line |
| ⑥ Mixed-use Ground Floor Retail / Office / Multifamily | Ⓡ Recreational Facilities (Private) | | |
| ⑦ Service Commercial / Auto Related | Ⓢ Open Space | | |

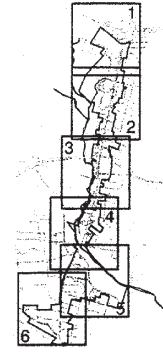
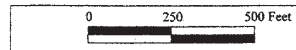


Illustrative Concept (Alternative for Subareas 4f & 6b) Map 8-2a
Source: Rhodeside & Harwell, Inc.



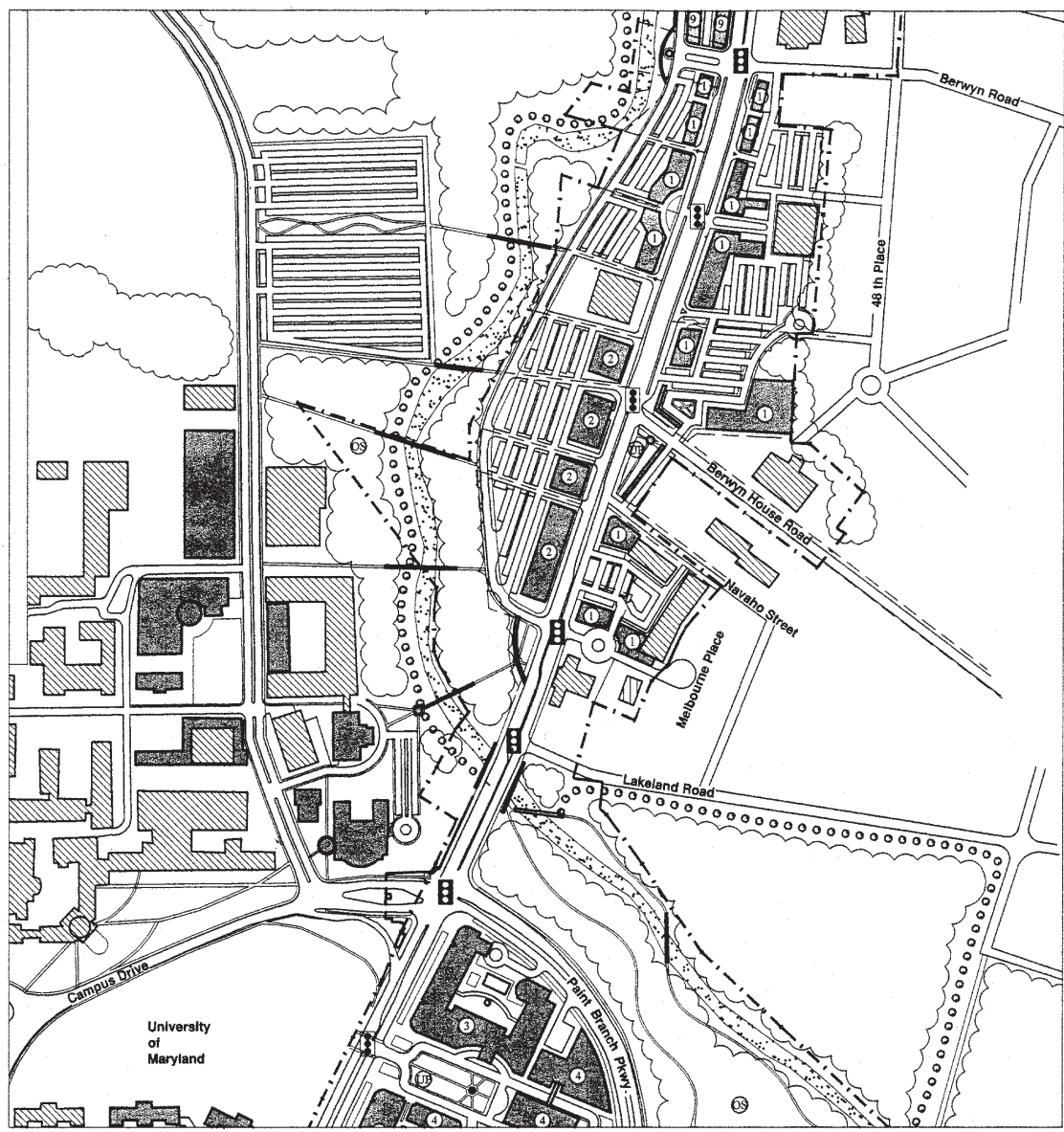
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| ① Retail / Commercial | ⑧ Residential / Multifamily | Ⓟ Parking Structure | ⋯ Trails |
| ② Office | ⑨ Residential / Single-Family Attached | ▬ Sector Plan Boundary | ☁ Woodlands |
| ③ Hotel / Conference Center | ⑩ Residential / Single-Family Detached | ▨ Existing Buildings To Remain | Ⓡ Existing Traffic Signals To Remain |
| ④ Mixed-use Ground Floor Retail / Office | ⑪ Institutional / Public / Quasi-Public | ▩ Potential Building Locations | Ⓢ Potential Traffic Signal Locations |
| ⑤ Mixed-use Ground Floor Retail / Multifamily | ⑬ Urban Park | ▬ Paint Branch Stream | ▬ Property Line |
| ⑥ Mixed-use Ground Floor Retail / Office / Multifamily | ⓇⓇ Recreational Facilities (Private) | | |
| ⑦ Service Commercial / Auto Related | ⓇⓈ Open Space | | |



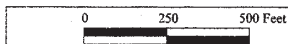
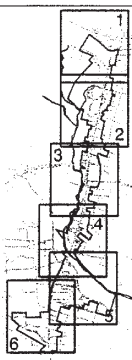
Illustrative Concept
Source: Rhodeside & Harwell, Inc.

Map 8-3



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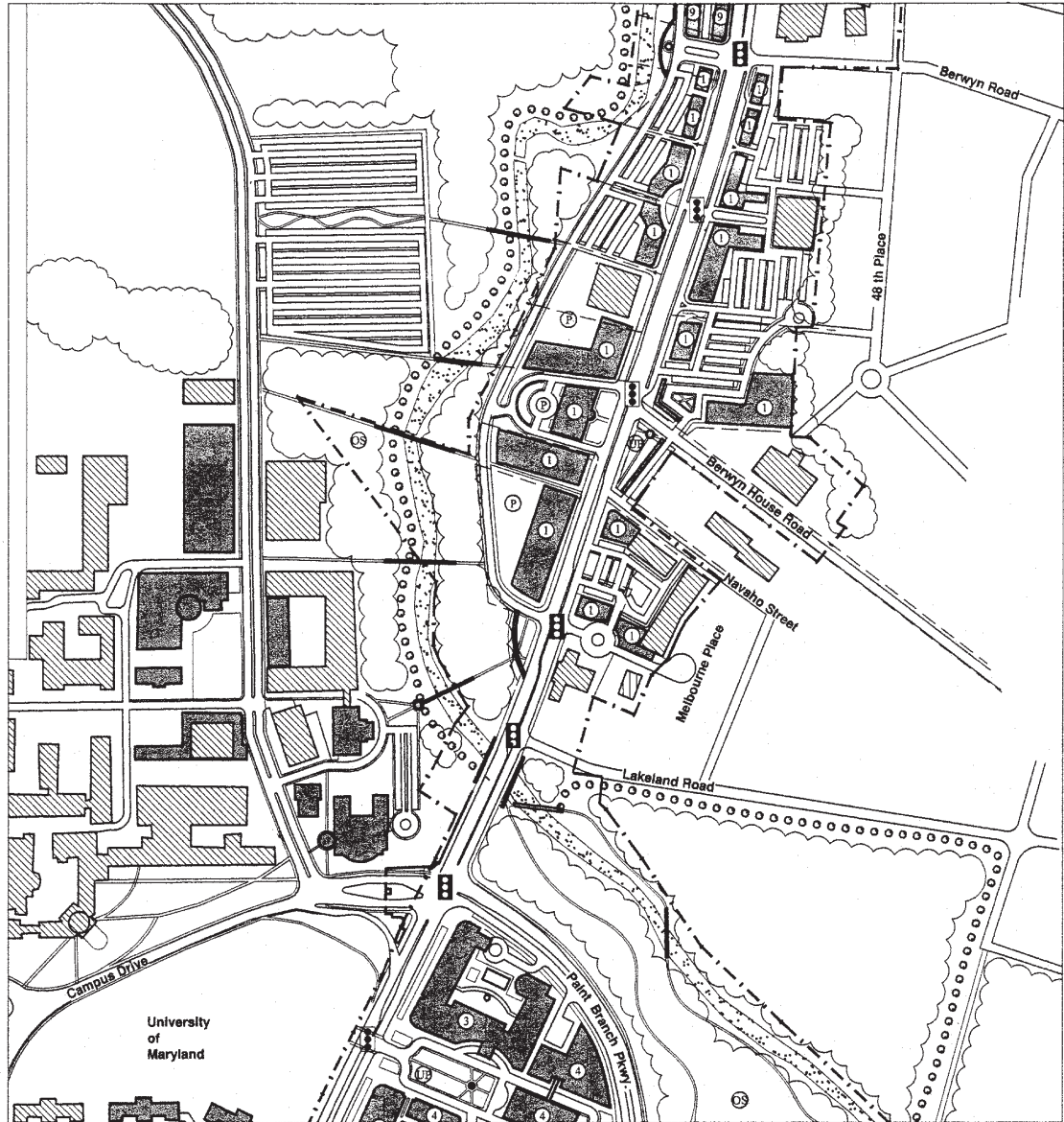
① Retail / Commercial	⑧ Residential / Multifamily	Ⓟ Parking Structure	⋯ Trails
② Office	⑨ Residential / Single-Family Attached	▭ Sector Plan Boundary	⊞ Woodlands
③ Hotel / Conference Center	⑩ Residential / Single-Family Detached	▨ Existing Buildings To Remain	Ⓜ Existing Traffic Signals To Remain
④ Mixed-use Ground Floor Retail / Office	⑪ Institutional / Public / Quasi-Public	▩ Potential Building Locations	Ⓛ Potential Traffic Signal Locations
⑤ Mixed-use Ground Floor Retail / Multifamily	⑫ Urban Park	Ⓜ Paint Branch Stream	Ⓛ Potential Traffic Signals Location
⑥ Mixed-use Ground Floor Retail / Office / Multifamily	Ⓜ Recreational Facilities (Private)		
⑦ Service Commercial / Auto Related	Ⓞ Open Space		

4

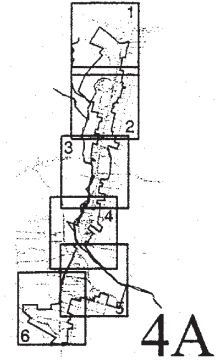
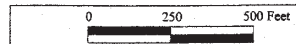
Illustrative Concept
Source: Rhodeside & Harwell, Inc.

Map 8-4



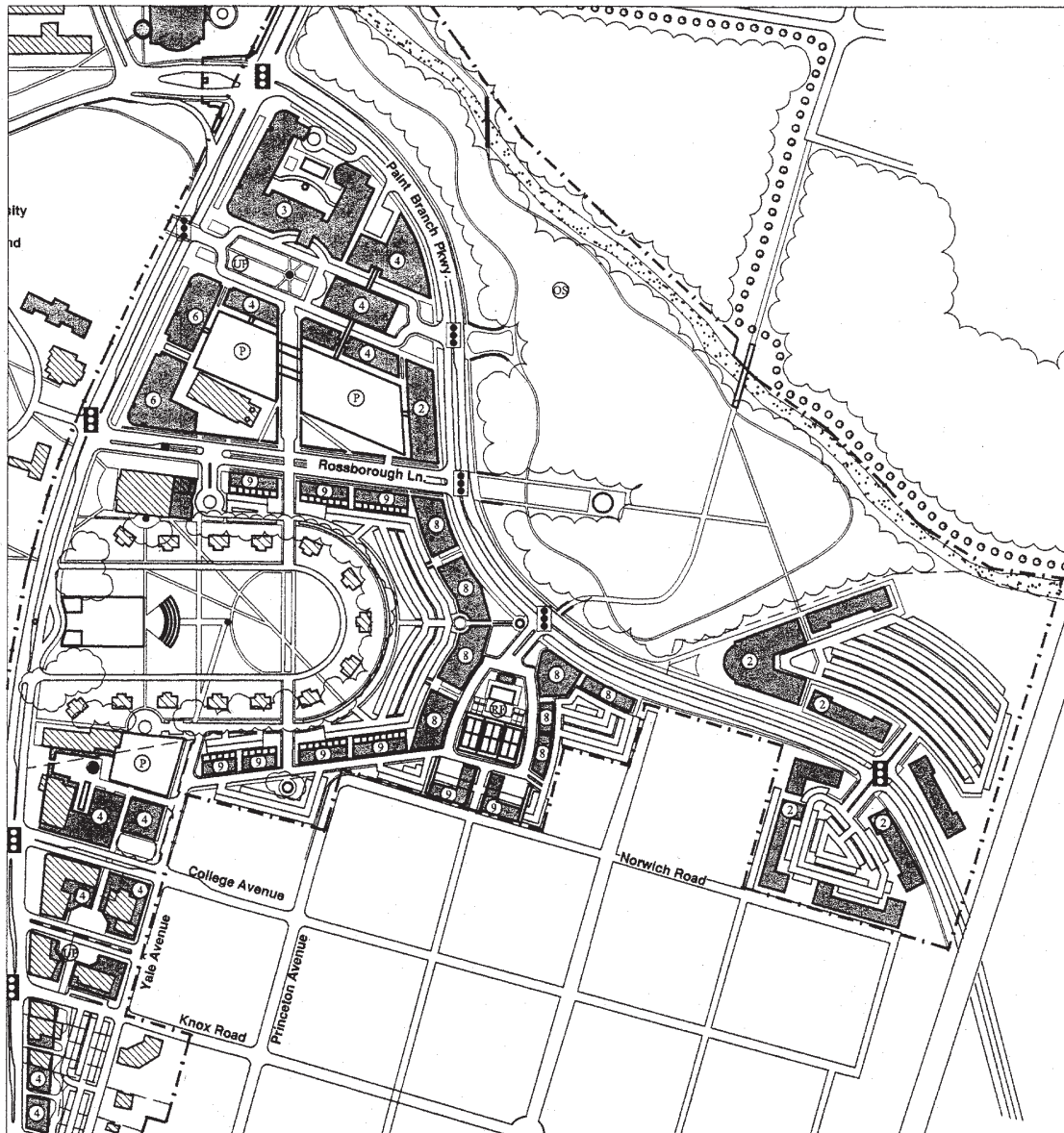
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| ① Retail / Commercial | ⑧ Residential / Multifamily | Ⓟ Parking Structure | ⋯ Trails |
| ② Office | ⑨ Residential / Single-Family Attached | ▭ Sector Plan Boundary | ▭ Woodlands |
| ③ Hotel / Conference Center | ⑩ Residential / Single-Family Detached | ▨ Existing Buildings To Remain | Ⓜ Existing Traffic Signals To Remain |
| ④ Mixed-use Ground Floor Retail / Office | ⑪ Institutional / Public / Quasi-Public | ▨ Potential Building Locations | Ⓜ Potential Traffic Signal Locations |
| ⑤ Mixed-use Ground Floor Retail / Multifamily | ⑫ Urban Park | ▭ Paint Branch Stream | ▭ Property Line |
| ⑥ Mixed-use Ground Floor Retail / Office / Multifamily | ⓂⓅ Recreational Facilities (Private) | | |
| ⑦ Service Commercial / Auto Related | Ⓞ Open Space | | |



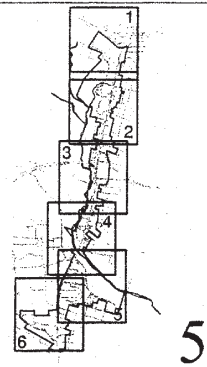
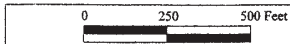
Illustrative Concept (Alternative for Subarea 3a)
Source: Rhodeside & Harwell, Inc.

Map 8-4a



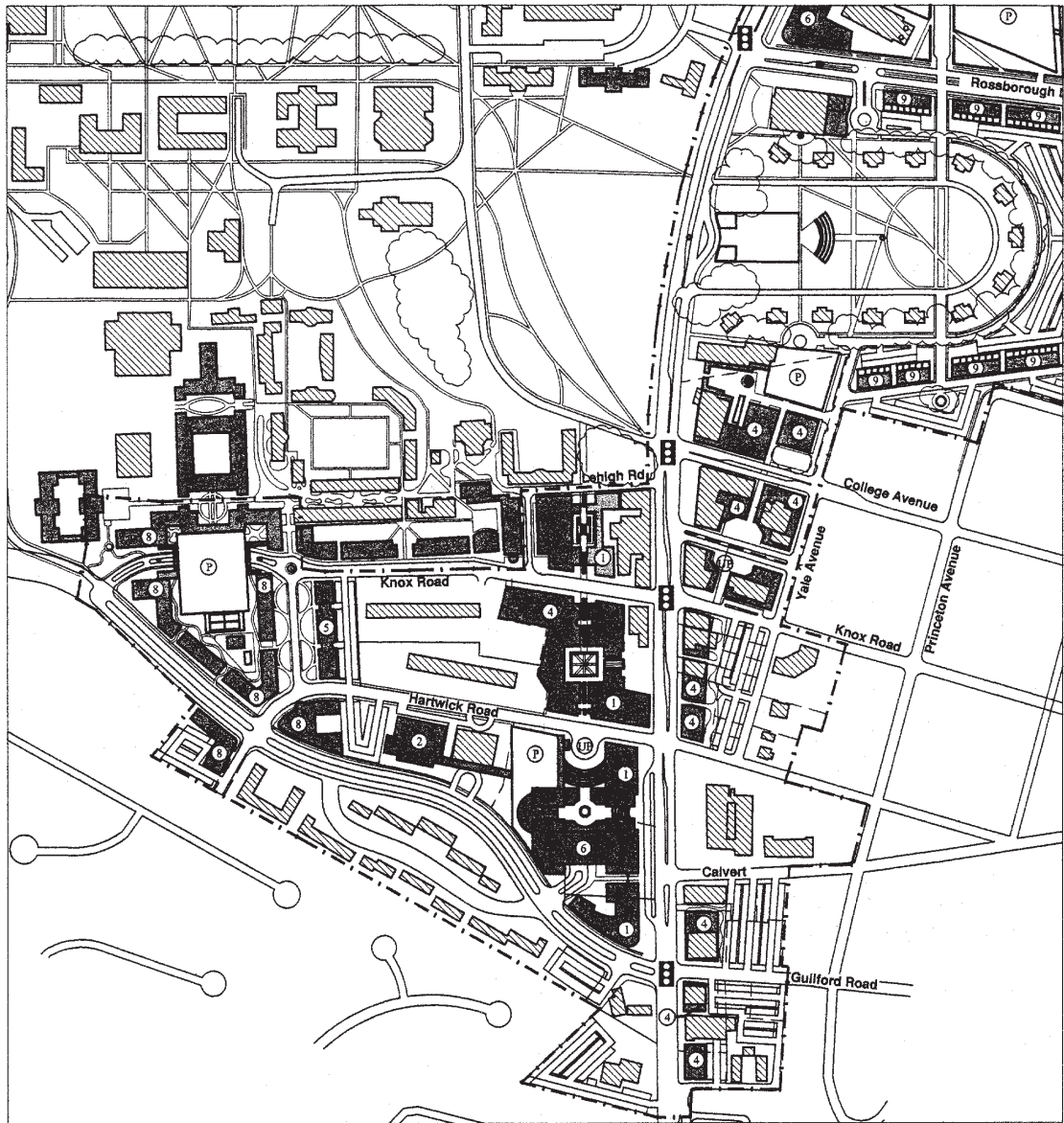
LEGEND-

Retail / Commercial	Residential / Multifamily	Parking Structure	Trails
Office	Residential / Single-Family Attached	Sector Plan Boundary	Woodlands
Hotel / Conference Center	Residential / Single-Family Detached	Existing Buildings To Remain	Existing Traffic Signal Locations
Mixed-use Ground Floor Retail / Office	Institutional / Public / Quasi-Public	Potential Building Locations	Potential Traffic Signal Locations
Mixed-use Ground Floor Retail / Multifamily	Urban Park	Paint Branch Stream	Property Line
Mixed-use Ground Floor Retail / Office / Multifamily	Recreational Facilities (Private)		
Service Commercial / Auto Related	Open Space		



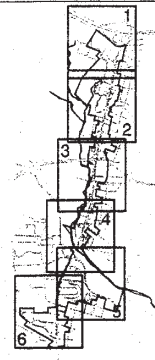
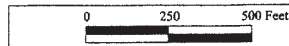
Illustrative Concept
Source: Rhodeside & Harwell, Inc.

Map 8-5



LEGEND-

Retail / Commercial	Residential / Multifamily	Parking Structure	Trails
Office	Residential / Single-Family Attached	Sector Plan Boundary	Woodlands
Hotel / Conference Center	Residential / Single-Family Detached	Existing Buildings To Remain	Existing Traffic Signals To Remain
Mixed-use Ground Floor Retail / Office	Institutional / Public / Quasi-Public	Potential Building Locations	Potential Traffic Signal Locations
Mixed-use Ground Floor Retail / Multifamily	Urban Park	Paint Branch Stream	Potential Traffic Signals Location
Mixed-use Ground Floor Retail / Office / Multifamily	Recreational Facilities (Private)		
Service Commercial / Auto Related	Open Space		



Illustrative Concept
Source: Rhodeside & Harwell, Inc.

Map 8-6

TRANSPORTATION AND CIRCULATION

The plan reinforces the City of College Park's vision for US 1 becoming a revitalized gateway boulevard. Emphasis has been placed on the pedestrian quality of a new streetscape with increased mixed-use, transit-oriented development providing an inspiring gateway into the city and Maryland's flagship university. The plan's emphasis on Smart Growth initiatives attempts to balance land use and transportation capacity, while managing congestion.

GOALS

- Provide an accessible, safe, aesthetically pleasing and convenient circulation network for pedestrians, bicyclists, motor vehicles, and public transit while improving traffic flow along US 1.
- Integrate the US 1 improvement proposals from the State Highway Administration into the land use and circulation network.
- Develop parking strategies that include incentives and provide parking flexibility to encourage mixed-use development.

BACKGROUND AND ISSUES

US 1 Existing Character—US 1 is a four-lane, undivided highway with a center turn lane. Vehicles and pedestrians move in an auto-dominated environment with over 57,000 vehicles per day. Heavy traffic congestion occurs during the morning and evening peak hours. Turning movements across traffic create frequent delays and safety hazards as long queues of through traffic create poor sight distance for turning vehicles. This situation is complicated by trucks and autos entering and leaving the many businesses, most of which are on narrow frontage lots without alternative access. With over 177 closely spaced business entrances (curb cuts) in the corridor, frequent traffic conflicts and low speeds are common. Trucks often load and unload while parked partially or totally in a travel lane. Parking for business patrons is variable in amount and placement, often independent of demand.

Pedestrian and bicycle use is substantial around The University of Maryland. Approximately 24,600 full-time undergraduate and 8,500 graduate students attend the university. Many travel on foot and by bike within

the corridor. There are no significant buffers along the highway to separate this nonvehicular traffic from the heavy motor vehicle traffic. Sidewalks, where they exist, are adjacent to travel lanes and are extremely narrow. Vehicle and pedestrian movements are slowed by 25 intersecting streets, 10 of which are signalized. The situation is compounded by deficient nighttime lighting, numerous utility poles and distracting business signage. There is limited street furniture for pedestrian use. Also, the lack of bus



Individual curb cuts and vehicular access controls are lacking along the majority of US 1.



Numerous poles and inadequate pedestrian facilities project a less-than-inviting environment.

pull-off areas is another impediment to smooth traffic movement.

Accident rates on this section of US 1 are more than double the statewide average for similar roadways. The level of fatal accidents has been more than three times the statewide average. Increasing traffic volumes and the high number of conflict and access points are likely to increase this trend even further if no improvements are made. According to the State Highway Administration (SHA) the level of service along this part of US 1 is generally poor, with conditions in many sections at near-failing or failing levels, or E and F, respectively. These conditions require several signal cycles to clear traffic through a single intersection.

Paint Branch Parkway—This is a four-lane collector roadway that extends from US 1 to Kenilworth Avenue (MD 201) and provides access to the College Park-University of Maryland Metro Station and the surrounding employment and mixed-use transit district. The weekday 24-hour volume is about 11,000 vehicles. Along the south side of the road, there is an eight-foot hiker/biker trail that narrows to four feet in the tunnel under the railroad and Metro tracks. No sidewalk exists on the north side. This road is identified in the *College Park-Riverdale Transit District Development Plan* (CP-R TDDP) as a gateway into the transit district. It is recommended for improvement between MD 201 and River Road, as a four-lane divided roadway with a 16-foot wide landscaped median, new streetscape amenities, and enhanced pedestrian and bicycle circulation facilities.

OTHER PLANS AND STUDIES

The county and city have long recognized the congested character of the US 1 corridor and the need to improve the flow and safety of all forms of traffic while enhancing the appearance of the highway. In facing this challenge, the county approved the 1989 *Langley Park-College Park-Greenbelt Master Plan* which proposed upgrading US 1 to a six-lane arterial while adding mass transit options. It also documented the city's request to conduct a feasibility study of a potential US 1 bypass

to reduce traffic volumes on US 1. However, the ongoing SHA alternatives study is not proposing any significant widening beyond the existing four- to five-lane section.

- A continuing need exists for a new US 1 bypass road. The 1994 *Approved Master Plan for Planning Area 68* (south of College Park) proposed a main street concept for the corridor, citing the recommendations of the 1993 *US 1 Bypass - Transportation Study* to downgrade US 1 from an arterial to a four-lane collector between the District of Columbia and University Boulevard (MD 193). This action amended the 1989 master plan, but did not fully adopt other recommendations of the US 1 bypass study, including the recommendation to build a new road with limited access between the university and the Capital Beltway to relieve congestion along US 1. While the need for a new road is frequently voiced by area residents, officials and businesses, further analysis of such a roadway is beyond the scope of the current SHA's US 1 project planning study and this sector plan.
- There is a need to agree upon a comprehensive roadway and streetscape upgrading and a development character for the corridor. The 1995 *City of College Park Comprehensive Plan* also recommends downgrading US 1 to a divided four-lane road south of the Beltway. The need for increased mass transit choices was again emphasized. A major recommendation in the city's 1998 Smart Growth Quality Community Survey was to reengineer and redevelop parts of the highway as a boulevard and main street, with appropriate streetscape improvements.
- In 1998, at the request of the city, SHA initiated a project planning study to address safety and manage congestion along US 1 within the sector plan area. The goals of the US 1 project are:
 - Driver safety and vehicle flow—To improve the ability of the roadway to safely manage current and predicted

traffic volumes at a more appropriate level of service.

- Aesthetics—To stimulate the emergence of a “sense of place” along the US 1 corridor by developing streetscape plans that, through the use of landscaping, utility relocation and other unifying elements, present users with consistent and coherent visual themes.
- Pedestrians—To identify methods for reshaping US 1 as a pedestrian-friendly facility that will improve pedestrian safety and circulation.
- Bicyclists—To identify safe and practical measures to improve facilities for those who choose to bicycle within the project area.
- Transit—To investigate opportunities for improving transit in coordination with the City of College Park, The University of Maryland, Prince George’s County, the Mass Transit Administration, and Washington Metropolitan Area Transit Administration (WMATA).

The SHA study seeks to comprehensively reconstruct the highway following anticipated location/design approvals in 2002 by the Federal Highway Administration (FHWA). In addition to the no-build option, four design alternatives are under consideration:

1. No-build—The no-build alternative consists of routine maintenance and safety improvements only. This alternative is under consideration as a baseline to which all other build alternatives will be compared.
2. Five-lane undivided reconstruction—US 1 would be reconstructed from Sunnyside Avenue to College Avenue as a new 5-lane undivided roadway, with two 11-foot inside travel lanes and two 16-foot outside travel lanes (in each direction) separated by a 13-foot center turn lane.

3. Four-lane divided reconstruction—US 1 would be reconstructed from Sunnyside Avenue to College Avenue as a new 4-lane divided roadway, with two 11-foot inside travel lanes and two 16-foot outside travel lanes (in each direction) separated by a 16-foot raised grass median. The median would narrow at selected intersections to accommodate left and U-turning vehicles.

4. Cherry Hill Road interchange option—Although intersection improvements at Cherry Hill Road and US 1 are included in the build alternatives, the intersection will continue to operate at a poor level of service in the future. A grade-separated interchange option has been developed to address this poor level of service. If either the aforementioned four- or five-lane sections is selected, this interchange option could be incorporated into either build alternative. The interchange concept is based on carrying Cherry Hill Road under the existing grade of US 1 and creating ramps leading from and to the northbound lanes of US 1. This intersection would accommodate all of the turning movements between Cherry Hill Road and US 1. All of the vehicles that want to pass through the intersection on US 1 will continue and not be interrupted by a traffic signal. Ramps from southbound US 1 to Cherry Hill Road and ramps to southbound US 1 from Cherry Hill Road will be included to facilitate these movements.

Strategies for Transportation Systems Management (TSM) and Transportation Demand Management (TDM) have been incorporated into both the four-lane divided and five-lane undivided alternative. Some of these strategies include bus pull-off lanes, access driveway consolidation, and intersection improvements at US 1/MD 430 (Greenbelt Road section) and US 1/Cherry Hill Road, Edgewood and Hollywood Roads.

Further information regarding the study can be found in the SHA's website www.sha.state.md.us/plansproj.htm.

OTHER ISSUES

- SHA indicates that with the reconstruction of US 1, the road network will continue to operate at or near capacity based on projected growth in regional and local traffic. Therefore, a key issue is how to encourage new development without worsening traffic congestion.
- Another current study having potential implications for this sector plan is the *Capital Beltway Corridor Transportation Study* (also known as the Purple Line study). In the early 1990s, the Maryland Department of Transportation (MDOT) studied the feasibility of introducing high occupancy vehicle (HOV) lanes to improve traffic conditions on Maryland's section of the Beltway. In 1994, MDOT reinitiated the study to consider a wider range of possible transportation strategies (including both highway and transit improvements) to address current traffic problems and future traffic growth, and to improve safety. The study is in the preliminary phase of alternatives evaluation, examining costs, projected benefits, and potential environmental impacts. This regional study also involves evaluating Virginia's portion of the Beltway for highway, multimodal, and transit enhancements. The transit alternatives include both rail transit and express bus service.

The Purple Line is the original rail transit alternative developed for the Capital Beltway which consisted of a single circumferential corridor around the Capital Beltway. As the study progressed, additional alternatives were added. At present, there are six transit corridors being considered. These six corridors form circumferential routes intended to connect key residential and employment areas as well as radial transportation facilities such as the existing Metro. Three alternatives consisting of heavy or light rail transitways are outside the Capital Beltway. These

corridors head west from the Greenbelt Metro Station connecting to the I-95 park-and-ride lot before turning northwest connecting to White Oak and the Wheaton Metro Station. One heavy rail transit alternative connects the Greenbelt Metro Station along the Capital Beltway to the Forest Glen Metro Station. Two alternatives (P2 and P6), consisting of a heavy rail and light rail transitway, respectively, connect the New Carrollton Metro Station to the College Park-University of Maryland Metro Station; continuing west on University Boulevard before turning toward and connecting to the Silver Spring Metro Station. Implications for the sector plan relate to the potential role the College Park-University of Maryland and Greenbelt Metro Stations may play in the proposed transit alternatives and the possibilities for reducing traffic congestion along US 1. Further information regarding the study can be found at the SHA website, www.sha.state.md.us/planproj.htm.

- No changes are proposed for the Capital Beltway by this sector plan. However, current traffic conditions on the Beltway will continue to worsen as traffic volumes increase with expanded household and job growth in the region. The plan recognizes that the *Capital Beltway Corridor Transportation Study* may include recommendations which could affect the sector plan area in the future.

CONCEPT

The transportation concept supports improving the transportation system inside the Beltway. The plan calls for US 1 to be reconstructed as a gateway boulevard with landscaped medians, bike lanes, sidewalks/crosswalks, and enhanced streetscape planting, furnishings and lighting. This multimodal system of local streets, bus routes, bikeways, trails and sidewalks is integrated with the plan's land use recommendations to encourage a user-friendly system for bicyclists, pedestrians and motorists. The intent is to implement the previously stated goals of the SHA alternatives study.

The concept establishes the following road classifications (with abbreviations in parentheses) as the basis for the transportation recommendations below.

Freeway (F): A divided highway for through traffic, with full access control by grade separations at intersections, intended solely to carry large volumes of traffic over medium to long distances. Rights-of-way range from 300 to 600 feet. Example: Capital Beltway (I-95/I-495).

Arterial (A): A divided highway with intersections at grade and geometric designs and traffic controls intended to expedite the movement of through traffic. Direct access to abutting properties may be permitted but may also be controlled. Rights-of-way are generally a minimum of 120 feet. Example: Greenbelt/University Boulevard (MD 193).

Major Collector (MC): A four-lane roadway with turning lanes at intersections and some control of access, which provides movement primarily for local traffic along with some elements of through traffic. Rights-of-way are generally a minimum of 90 feet. None exist in the sector plan area. However, US 1 is recommended for reconstruction as a major collector within the sector boundary.

Collector (C): A multilane or two-lane highway designed to carry medium speed traffic between arterial and internal local streets, to provide access to major traffic generators and to connect residential neighborhoods to major highway systems. Access to abutting properties is usually permitted. Rights-of-way are generally a minimum of 70 feet. Example: Rhode Island Avenue.

Industrial (I) , Primary (P) and Secondary (S) Residential Roadways: These two-lane roadways provide access to, from and through developed areas. On primary roadways, the street space is probably used as much for nonvehicular movement and storage as it is for vehicular movement. Rights-of-way for Industrial, Primary and Secondary residential roadways are generally a minimum of 70 feet, 60 feet, and 50 feet, respectively.

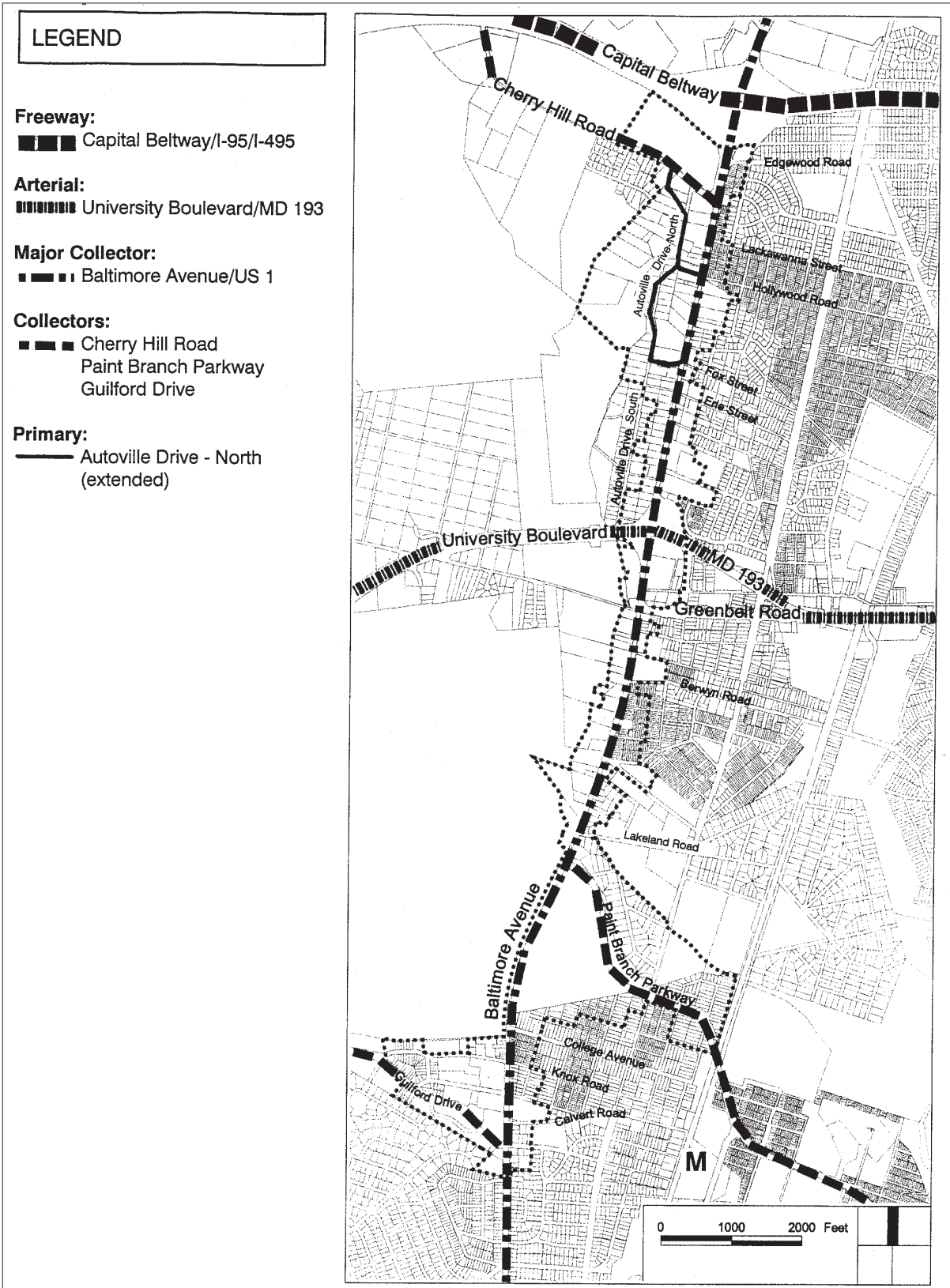
RECOMMENDATIONS

The recommendations below are intended to improve circulation, access and parking while minimizing traffic congestion within the sector plan area. All new development, with minor exceptions, will be subject to site plan review to ensure compliance with the goals and recommendations of the sector plan and Development District Standards for parking and access.

VEHICULAR ACCESS AND CIRCULATION RECOMMENDATIONS

Typical highway cross-section drawings are shown in Section P1 (Road Network) in the Development District Standards chapter. The proposed vehicular circulation system is shown on Map 9 and is described in Table 3. The individual elements are as follows:

1. *Baltimore Avenue (US 1) is recommended as a major collector within a four-lane divided reconstruction alternative, unless determined otherwise by the State Highway Administration based on safety and design considerations. The intent is to provide a gateway boulevard appearance within a right-of-way of 90 to 110 feet.* The plan supports the SHA alternatives study which will result in a reconstructed gateway boulevard, upon selection of an alternative and funding of construction. A clearly recognized streetscape treatment along and within the corridor areas (such as the town center and main street areas) will alert motorists that they are entering a quality area, where pedestrians are separated from traffic. Implementation of the gateway boulevard, with the streetscape elements being planned by SHA, will be a major public investment catalyst. Infrastructure reinvestment of this magnitude should help leverage private sector land development and related changes in each segment of the corridor. The major collector classification is considered most consistent with the current function and the proposed alternatives in the SHA study.



Vehicular Circulation System

Map 9

Functional Classification/ID	Name	Route Number	Proposed Right-of-Way	Proposed Number of Lanes
F-5	Capital Beltway	I-95/I-495	300 feet	8-12
A-16	Greenbelt Road	MD 193	120-200 feet	4-6
MC-200	Baltimore Avenue	US 1	90-110 feet	4
C-201	Cherry Hill Road	County	80 feet	4
C-202	Paint Branch Parkway	County	80 - 100 feet	4
C-203	Guilford Drive	City	80 - 100 feet	4
P-200	Autoville Drive North (relocated)	Proposed	60 feet	2

In addition, the plan amends the *Master Plan of Transportation* to downgrade US 1 from an arterial highway, between the Beltway and MD 193, to a four-lane divided major collector. From MD 193 to Guilford Drive, this sector plan amends the *Master Plan of Transportation* to upgrade US 1 from a collector to a four-lane divided major collector classification (see Figure 3 and the bottom sketch in Figure 4, which illustrate the four-lane section).

2. *The plan recommends the SHA's Cherry Hill Road interchange option be incorporated into the US 1 selected build alternative. The grade-separated inter-*

change option, as proposed by SHA, will carry Cherry Hill Road under the existing grade of US 1, creating an intersection between Cherry Hill Road and ramps to be constructed leading from and to the northbound lanes of US 1. The main north-south movements on US 1 would no longer be interrupted by a traffic signal. Ramps from southbound US 1 to Cherry Hill Road and from Cherry Hill Road to southbound US 1 will be included to facilitate these movements. While this improvement will be the single-most expensive element in the reconstruction of US 1 within College Park, most of the people who attended the SHA's Alternatives Public

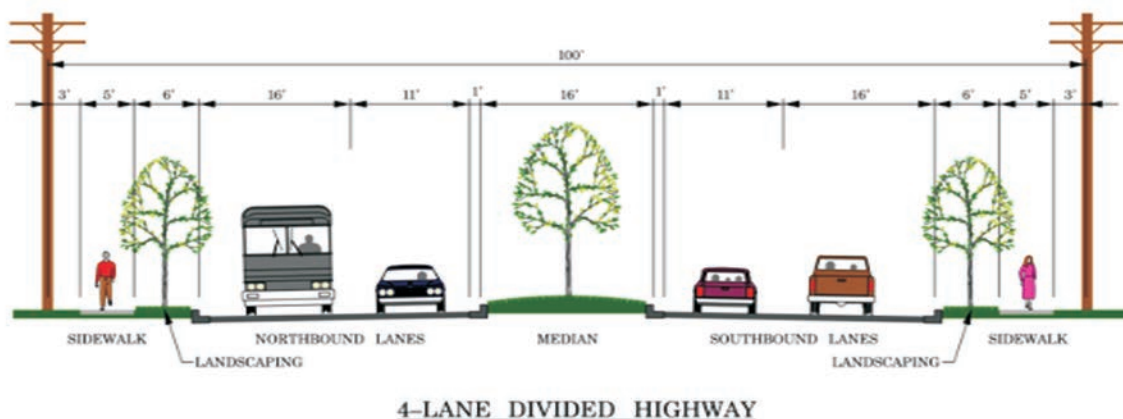


Figure 3. Four-lane divided reconstruction section.

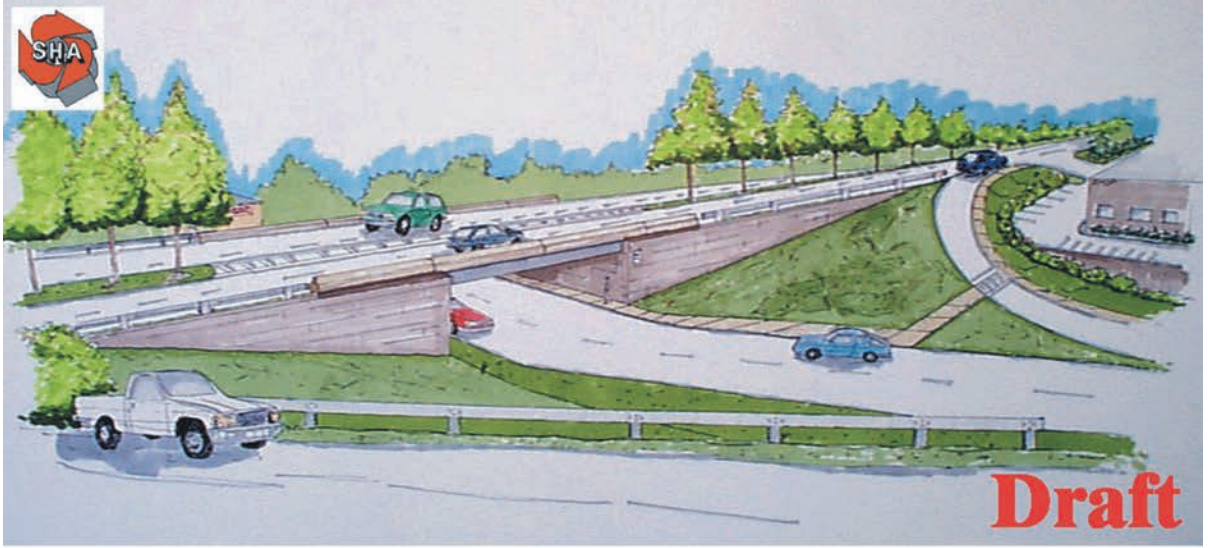


Figure 4. Cherry Hill Road Interchange (top) and Four-Lane Divided Reconstruction (bottom).

Workshop and commented on this issue preferred the interchange option. It is important that any planned improvement to the intersection be designed to accommodate bicyclists and pedestrians and provide safe separation from vehicular movements (see the top sketch in Figure 4, which illustrates a conceptual design for the interchange option).

3. *The intersection of Autoville Drive North with Cherry Hill Road is recommended for relocation to the west.* The existing intersection at Autoville Drive and Cherry Hill Road is proposed to be moved further west, away from the congested Cherry Hill Road/US 1 intersection. This will create a four-way intersection at the existing exit and traffic signal from the College Park Market Place shopping center onto Cherry Hill Road. The existing traffic signal at that point should be modified to include the additional traffic movements. Autoville Drive North is planned to curve from the new intersection point and rejoin its present alignment approximately 300 feet south of the present Cherry Hill Road/Autoville Drive intersection. This improvement must be coordinated with the Prince George's County Department of Public Works and Transportation.
4. *Autoville Drive North is also recommended for extension to the south in a curvilinear alignment to join with US 1 opposite Fox Street.* This loop road will provide access to developable land in the interior of subareas 4f and 5b by linking additional privately constructed local streets. This road should be built within a right-of-way of 60 feet and utilize traffic calming techniques to discourage traffic cut-through from US 1. In addition, the plan recommends access to this road from commercial properties fronting on US 1 as a means of minimizing turning movements on US 1.
5. *Paint Branch Parkway is recommended to remain a collector road with a right-of-way of 80 to 90 feet.* It is recommended this roadway be improved to a four-lane divided roadway with a 16-foot-wide landscaped median, new streetscape amenities, and enhanced pedestrian and bicycle circulation facilities for the segment between US 1 and the existing traffic signal just to the west of the CSX underpass. The College Park-Riverdale Transit District Development Plan suggests similar enhancement for the segment between Kenilworth Avenue and River Road. Several new points of entry are planned from proposed local roads serving future development in subarea 1d (East Campus) and within subarea 1e. It is recommended that these new intersections be signalized, when warranted by the county Department of Public Works and Transportation (DPW&T).
6. *Guilford Drive is recommended to remain a collector road with varying rights-of-way.* As the area develops, Guilford Drive will become a major southern gateway to The University of Maryland and the redeveloped residential portions of the town center. It is recommended that this roadway be improved to a four-lane divided roadway with new streetscape amenities, enhanced pedestrian and bicycle circulation facilities, and retention of the open drainage area as a median between the dualized travel lanes.
7. *The plan recommends initiation of a concept-level planning investigation of providing a new alternate road from the Capital Beltway to The University of Maryland.* The investigation should include engineering, right-of-way and environmental detail, and continued public involvement. Based on recommendations in the US 1 Bypass-Transportation Study and discussions in advisory planning group and community meetings, The University of

Maryland, SHA, and city officials are encouraged to explore provision of an alternate route to the university from the Beltway. This continues to be suggested by existing businesses, the city, and the university as a way of reducing reliance on US 1 to move traffic. However, such a study is beyond the scope of both the sector plan and the SHA US 1 alternatives study.

Typical highway cross sections illustrating the recommended roadway design for Paint Branch Parkway, Guilford Drive and the proposed Autoville Drive North are shown in the Development District Standards chapter. All roads should be designed in accordance with current American Association of State Highway and Transportation Officials *Guidelines for the Development of Bicycle Facilities*.

PUBLIC TRANSPORTATION RECOMMENDATIONS

The sector plan and surrounding area are currently accessible by the Metrorail Green Line and the College Park-University of Maryland and Greenbelt Metrorail Stations, the College Park MARC Station, as well as the Washington Metropolitan Area Transit Authority (WMATA) Metrobus routes 81, 83, 86, and C2, and The University of Maryland Shuttle/UM bus service.

The College Park Metrorail and MARC Stations provide a commuter parking lot for buses, carpools and vanpools, and includes bicycle racks and lockers. This joint facility provides excellent opportunities for diverting vehicle trips, particularly peak-period, single-occupant vehicle trips destined to The University of Maryland.

Usage strategies that promote public transportation are essential in reducing traffic congestion and in achieving the development and land uses envisioned in this sector plan. As of October 1, 2000, transit use experienced an upswing as a result of federal employers' increased involvement in transit subsidization. As of that date, all federal employers in the Baltimore-Washington area offered \$60 per month to cover an employee's

transit costs. The subsidy is provided through WMATA's *Metrochek* program and MTA's *TransitPlus* 2000 program. WMATA expects 20,000 new riders on Metrorail alone by January 2001. The subsidy will expand to \$100 per month by mid-2002. Such programs are effective in reducing vehicle trips.

To emphasize the importance of transit, the plan recommends the following which build upon the recommendations made by the 1993 *US Route 1 Bypass-Transportation Study*, the 1994 *Master Plan and Sectional Map Amendment for Planning Area 68*, and the 1997 *Transit District Development Plan for the College Park-Riverdale Transit District Overlay Zone*:

8. *Direct shuttle bus service is recommended along US 1.* This shuttle should connect the I-95/I-495 park-and-ride commuter lot, Metro and MARC stations, and downtown College Park. Such a facility will better utilize the park-and-ride lot and lead to reduction of university-related automobile trips on US 1. Bus pull-off lanes being incorporated into the SHA's US 1 study are recommended.
9. *Subsidy programs such as WMATA's Metrochek and MARC's TransitPlus initiatives are recommended.* The University of Maryland is a major contributor to the volume of commuter traffic on US 1. The university and other area employers are encouraged to follow the federal government's lead and participate in these subsidy programs, at least at the minimum level of providing a pre-tax payroll deduction for transit costs and possible tuition rebate programs. The university provides pre-tax payroll deductions for employee parking fees.

NONVEHICULAR ACCESS AND CIRCULATION RECOMMENDATIONS

Nonvehicular movement consists of cycling on bikeways, and pedestrian movement principally along off-street trails and sidewalks. The plan recommends the following:



Bus pull-offs are needed throughout the US 1 corridor.

Bikeway Trails and Sidewalk Network




10. *Enhance the network of existing and proposed trails.* Trails, sidewalks and bikeways are a valuable asset in promoting nonvehicular movement. They serve a range of users, including pedestrians, hikers, bikers and equestrians, and should link key locations in the community such as the town center and the university campus. The network will ultimately include the existing Paint Branch Stream Valley Trail, bike routes or bike lanes as shown on Map 10, and a continuous system of sidewalks. Sidewalks are recommended on both sides of all roads. All trails should be designed in accordance with current *AASHTO Guidelines for the Development of Bicycle Facilities*. The goal is to provide opportunities for traveling to destinations by bicycle or by walking.

This will reduce dependency on the automobile, as well as safely accommodate those who do not have a car or choose not to use one for all trips. Accommodating a large number of bicycle and pedestrian trips made to The University of Maryland, various locations along US 1, and the nearby College Park-University of Maryland Metro Station is a top priority. The sidewalks that exist along US 1 are extremely narrow in width and disjointed. For the SHA's US 1 study, both the four-lane and five-lane alternative include five-foot-wide sidewalks on both sides of US 1. In addition, in the 16-foot outside travel lane, 11 feet would be used for automobiles and 5 feet would be used to accommodate bicycles.

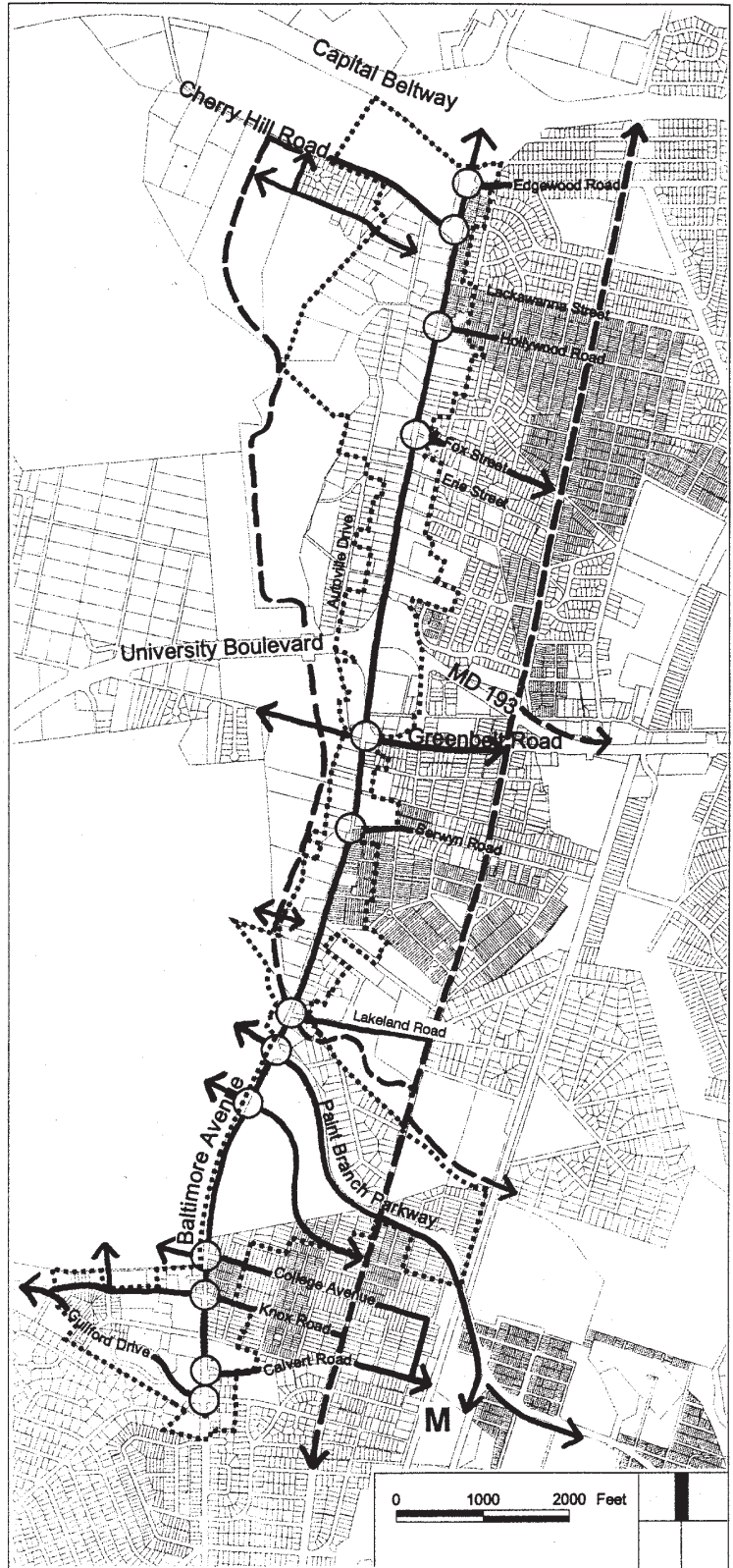


Trails provide valuable nonvehicular connections to the university, the Paint Branch Stream Valley Park, and workplace, residential and retail destinations within College Park.

LEGEND

-  Planned Bicycle Network (on-street)
-  Hiker/Biker Trails (off-street)
-  Crosswalks (primary)

Note: The type of bicycle facility for Paint Branch Parkway and Cherry Hill Road may be on-street, off-street or a combination of both.



Pedestrian Circulation System

Map 10

Additional connections to the Paint Branch Stream Valley trail from US 1 are recommended in all subareas west of US 1, and in subareas 1d, 1e and 2b, so that bicyclists and pedestrians can access the trail from various points. Trail connections to parts of subareas 4a, 4f and 5d may be infeasible due to severe slopes and grade changes. The planned local system also links to adjoining sections of the countywide trail system. A trail is also being planned by the City of College Park along the entire Rhode Island Avenue right-of-way, situated to the east of, and parallel to, the sector plan area.

Provision for Parking

As mentioned in the background section of this chapter, adequate parking is an essential component in encouraging mixed-use development. The plan has sought to relax parking standards in this urbanized area in order to encourage redevelopment and in recognition of US 1 being a transit corridor where alternative forms of transportation are readily available. Also, the plan exempts all existing uses through the use of “grandfather” provisions contained in the Development District Standards chapter.

The primary objectives for setting parking standards and ratios are:

- To increase accessibility and economic utility of land within the corridor
- To reduce the amount of land devoted to parking in order to lessen environmental and compatibility problems
- To prevent parking spillover into adjacent neighborhoods
- To improve traffic operations and circulation along the corridor and to encourage use of alternative modes of transportation
- To reduce the cost of constructing excessive parking spaces which are needed only to accommodate relatively infrequent peak demands during the year

Parking Standards and Ratios

11. *Minimum and maximum parking requirements are recommended and will be implemented through the Development District Standards.* Parking requirements typically are developed based on the requirements for peak parking demand at single-use developments. However, the use of standard, single-use parking formulas has often resulted in too many or too few parking spaces to meet actual demand. Recognizing this tendency, many jurisdictions have established minimum and maximum parking code requirements. Provision of minimum parking requirements ensures that there will be adequate spaces available, while the maximum requirement limits the number of spaces to promote community objectives, including reducing traffic congestion. They can assist in promoting the use of transit, and in enhancing development and redevelopment opportunities. They are intended to ensure that parking supply management becomes an integral part of plan implementation.

Shared Parking

12. *Parking credits are recommended to encourage shared parking facilities.* Excellent potential exists for maximizing efficient use of existing parking facilities by allowing two or more adjacent property uses to satisfy their parking



Creative solutions to parking should be explored, such as parking on the ground floor of a building.

requirements in a common parking facility. This concept is commonly known as shared parking. The Development District Standards provide the criteria and procedure for evaluating shared parking proposals.



Shared parking facilities reduce the amount of spaces needed between two or more uses.

Alternative Modes of Transportation

13. *Parking credits are recommended for using alternative modes of transportation.* Excellent potential exists for relieving traffic congestion and reducing vehicle trips, particularly single occupant vehicle (SOV) trips. The plan recommends allowing parking credits to the minimum parking standards. Parking credits should encourage optimum utilization of trip reduction measures to combine and/or divert as many SOV trips as possible to carpool, vanpool and transit. It also capitalizes on the regional transit system presence in the area. This concept is commonly known as Transportation Demand Management. The Development District Standards provide the criteria and procedure for seeking parking credits.



Amenities should be provided to encourage alternative modes of transportation other than single-occupant vehicle (SOV) trips.

Structured Multipurpose Parking Structures Within the Town Center and Main Street

14. *The use of public parking facilities is recommended.* The use of structure (multi-level garage) parking throughout the corridor, especially in the main street and town center areas, is strongly encouraged. It is recommended that public or private parking garage(s) be constructed for use by all property owners within the area. The funding, construction and management of such facilities will be in accordance with all guidelines and regulations established for such purposes. The Development District Standards provide the criteria and procedure for seeking parking credits for structure parking. Also, the plan's intent is not to count the number of underground (below grade) parking levels against the maximum number of stories recommended for each subarea.

Adequacy of Transportation Facilities

15. *Establishment of a traffic Level-of-Service E is recommended based on the average peak-period levels of service for all signalized intersections.* The transportation adequacy provisions contained in this plan reflect a number of factors:
 - The expected growth of through traffic and traffic generated or attracted by new

development within the sector plan area. Table 4 compares average daily traffic (ADT) volumes for key roadways and segments of US 1. The ADT estimates are based on buildout development potentials under existing zoning and sector plan proposed zoning.

- Opportunities for diverting some vehicle trips to transit which include Metrorail, Metrobus, the Prince George’s County THE BUS, and The University of Maryland Shuttle/UM Transit System.

- SHA alternatives to improve levels of service at selected intersections along US 1.
- Commission 2000’s recommendation to target new development toward existing transportation infrastructure, particularly along corridors and within centers, and the recommendation to revise the transportation adequacy requirements for areas within the Capital Beltway to Level-of-Service E during peak periods.

**Table 4
Comparison of Forecast Average Daily Traffic Volumes**

Street/Road	Existing ADT	Forecast ADT Baseline Buildout (Approved Master Plans) (1)	Forecast ADT Sector Plan Buildout and Approved Master Plans (2)	Forecast ADT Difference Between Base Buildout and Sector Plan Buildout (3)
US 1 (Baltimore Avenue) From Sunnyside Avenue to I-95/495	31,900*	82,095	82,200	+105 (+0.13%)
US 1 (Baltimore Avenue) from I-95/495 to Cherry Hill Road	42,480**	66,950	66,365	-1,585 (-2.40%)
US 1 (Baltimore Avenue) from Cherry Hill Road to MD 193 (Greenbelt Road)	54,650*	48,435	48,210	-225 (-0.46%)
US 1 (Baltimore Avenue) from MD 193 to Campus Drive/Paint Branch Parkway	46,950**	34,000	34,365	+365 (+1.07%)
US 1 (Baltimore Avenue) from Campus Drive/ Paint Branch Parkway to College Avenue	39,960**	31,790	32,320	+530 (+1.70%)

Street/Road	Existing ADT	Forecast ADT Baseline Buildout (Approved Master Plans) (1)	Forecast ADT Sector Plan Buildout and Approved Master Plans (2)	Forecast ADT Difference Between Base Buildout and Sector Plan Buildout (3)
US 1 (Baltimore Avenue) from College Avenue to Guilford Drive	26,520**	33,390	33,445	+55 (+0.60%)
Cherry Hill Road West of US 1 (Baltimore Avenue)	24,170***	24,760	24,905	+145 (+0.60%)
Autoville Drive North South of Cherry Hill Road	N/A	6,105	5,824	-281 (-4.60%)
Rhode Island Avenue North of MD 193 (Greenbelt Road)	13,290***	20,380	20,605	+225 (+1.70%)
MD 193 East of US 1	41,675*	42,880	43,160	+280 (+0.65%)
MD 193 West of US 1	41,275*	73,285	73,330	+45 (+0.06%)
MD 430 Greenbelt Road East of US 1	17,375*	18,870	18,785	-85 (-0.45%)
Paint Branch Parkway	10,690***	25,935	26,820	+885 (+3.40%)
Guilford Drive	N/A	12,215	13,020	805 (+6.60%)

NOTES:

ADT: Average Daily Traffic, Vehicles Per Day

(1) **Baseline Buildout**—Represents buildout of the existing land use and transportation recommendations for all properties within the sector plan and buildout of currently approved master plan recommendations for transportation and land use for the rest of the region.

(2) **Sector Plan Buildout**—Represents buildout of the preliminary sector plan recommendations for land use and transportation for all properties within the sector plan and buildout of currently approved master plan recommendations for transportation and land use for the rest of the region.

(3) **Forecast ADT Difference**—The reported difference for each roadway link represents an increase (+) or a decrease (-) between the forecast daily traffic volumes, resulting from the buildout of existing land use, and the sector plan's land use recommendations.

* Source: MD Department of Transportation, Maryland State Highway Administration, Prince George's County, 2000 Traffic Volume Map.

** Source: MD Department of Transportation, Maryland State Highway Administration, Environmental Assessment US 1 College Park.

*** Source: Prince George's County, Department of Public Works and Transportation, 2000 Traffic Count Book.

ENVIRONMENTAL FRAMEWORK

The sector plan emphasizes the preservation and enhancement of natural and scenic environmental resources and the creation of new green infrastructure. The intent is to achieve a healthy, pleasant, and revitalized gateway boulevard that contains shaded, tree-lined streets, well-protected green open areas, and landscaped plazas. The result will be an open space network whose components provide economic, environmental, educational and aesthetic benefits. As new development occurs, the sector plan provides guidance for integrating a variety of environmentally sensitive development concepts into a redeveloped gateway boulevard.

GOALS

- Improve the quality and aesthetics of the natural environment along US 1 and Paint Branch to improve both air and water quality.
- Enhance water quality and natural stream functions by incorporating appropriate low-impact development¹ techniques and bioengineering practices to manage stormwater run-off, control stormwater quality, and improve the quality of receiving streams.
- Enhance the natural environment by creating and providing access to new green infrastructure elements and providing public access to open space along the US 1 corridor as development occurs.
- Enhance the sustainability of the built environment by incorporating green building techniques, energy efficiency in buildings, and waste reduction and recycling programs into development proposals.

BACKGROUND AND ISSUES

The corridor is generally dominated by a hard-edged environment of large expanses of concrete and asphalt with very little green area other than The University of Maryland property in the vicinity of Paint Branch. These large areas of impervious surfaces create excessive stormwater run-off and pollution, create urban heat islands, and are devoid of

opportunities for recreation and wildlife habitat. Most of the corridor was developed before modern standards existed for landscaping, green space, forest conservation, and stormwater management. Also, little or no consideration was given to the aesthetic and quality of life value of designing developments around the stream valleys. For instance, within sub-area 3a, commercial development has occurred within the floodplain along the west side of US 1, north of Paint Branch, to the vicinity of Pontiac Street. Such development ignores the sensitivity of the area and creates numerous challenges to the consideration of environmentally-friendly redevelopment. Also, except for the Anacostia Tributary Trail System through portions of the Paint Branch, public access to and use of the area's natural resources is limited.

Most existing development along the corridor was constructed with excessive impervious surfaces and outmoded methods of stormwater management. Broadly defined, stormwater management is intended to improve the quality of stormwater that eventually leaves a site, maintain or reduce existing peak discharges of water from storm events, alleviate or prevent flooding problems, and preserve and protect the natural stream system. Low-impact development (LIDs) techniques seek to manage stormwater through reduced land consumption, generally less expensive installation costs, and reduced maintenance costs over traditional stormwater dry ponds. These LID concepts are applied in an integrated approach,

¹ Low Impact Development Design Strategies: An Integrated Design Approach. June 1999. Prince George's County Department of Environmental Resources, Programs and Planning Division.

utilizing existing site hydrology as a framework, and focus on controlling stormwater at the source, while integrating nonstructural methods to create a diverse landscape.

Low-impact development techniques are titled “integrated management practices” or IMPs. One of the best-known IMPs is rain gardens, also called bioretention areas, which are features that are relatively small in size. Their purpose is to collect, treat and, where infiltration is not possible, discharge rainwater on a small scale. Other low-impact techniques that may be appropriate in the plan area include dry wells, infiltration trenches, filter/buffer strips, vegetated swales, infiltration swales, rain barrels, and cisterns.

Bioengineering focuses on the use of natural materials for the construction and reconstruction of natural features. Instead of adherence to the strict use of concrete, rip-rap stone and other hardscape construction methods, bioengineering uses logs, stone, soil and vegetation to construct features that attempt to replace natural functions. Bioengineering is particularly useful in stream reconstruction and streambank stabilization projects that will most likely be needed in the study area.

The term “green infrastructure” refers to all environmental elements of the landscape, including wooded areas, stream valleys, landscaped buffers, landscaped islands, street trees, and even rooftop gardens. Built features such as buildings, streets, sidewalks and stormwater management ponds can and should contain green infrastructure elements.

Green building techniques range from the use of renewable resources and recycled materials in building construction to the use of renewable energy sources. Green building can also refer to the rehabilitation of existing structures that have had previous environmental problems. Another green building technique is the use of building designs that are energy efficient. In redevelopment areas, energy efficient building designs could be used as an economic draw for both business and residential development. Business and residential recycling programs reduce the waste stream and the need for disposal sites.

In redevelopment areas, waste reduction programs could result in fewer truck trips in and through the area and reduce the need for waste holding areas such as outdoor dumpsters. This reduction could lead to an overall reduction in paved surfaces as the need for dumpster locations is reduced.

CONCEPT

The plan’s intent is to concentrate development in compact and vertical mixed-use building forms, thereby reducing local and regional environmental impacts. Additionally, it is the purpose of the plan to correct existing environmental problems while preserving and enhancing what remains of the natural environment. Integration of new green infrastructure elements into existing and future development is also encouraged.

It is important that future development along the corridor integrate environmentally sensitive design elements and provide open space amenities for the benefit of residents and workers. Incorporating new stormwater management technologies in the reconstruction of US 1 and as part of all new development projects, along with enhancing streets with landscaping, will make the highway corridor more environmentally friendly. Enhancing landscaping and green areas around new development will contribute to creating environmental, aesthetic and economic benefits that make the area more economically competitive.

The concept also relies upon existing land use controls to implement the proposed open space and environmental network without excessive public expenditure or creating unreasonable demands on the private sector. The following land use regulations are currently used to achieve open space and protect environmental features that add value to communities.

1. Public Park Acquisition or Dedication: Land is acquired for active and passive recreation through acquisition by purchase, gift, or through mandatory dedication provisions of the subdivision regulations (Subtitle 24, Section 134, Prince George’s County Code).

2. **Floodplain Areas:** Land which is within the 100-year floodplain is generally restricted from development under the provisions of the floodplain regulations (Subtitle 4, Division 2, Prince George’s County Code).
3. **Stormwater Management:** Existing regulations require adequate control of stormwater runoff (Subtitle 4, Division 2, Prince George’s County Code).
4. **Use of Unsafe Land:** Land which is subject to flooding, erosive stream action, unstable soil conditions, or man-made unsafe conditions (unstable fills or slopes) is generally restricted from development by the Subdivision Regulations (Subtitle 24, Section 131, Prince George’s County Code).
5. **Protection of Wetlands:** Existing federal and state regulations require buffers adjacent to nontidal wetlands and generally restrict wetland areas from development. These are enforced locally through the county permit process (Subtitle 4, Division 2, Prince George’s County Code).
6. **Provision of Stream Buffers:** Areas within 50 feet of streams and adjacent wetlands, and floodplains and adjacent slopes, are generally restricted from development by the subdivision regulations (Subtitle 24, Section 130, Prince George’s County Code).
7. **Protection and Restoration of Woodlands:** The Woodland Conservation Ordinance requires the conservation of woodlands through preservation, reforestation and afforestation of woodland and specimen trees by meeting minimum woodland conservation thresholds (Subtitle 25, Prince George’s County Code).

RECOMMENDATIONS

Based on the above identified goals, issues and concept, the plan makes the following general environmental recommendations:

1. *Avoid and enhance environmentally sensitive areas.* Development shall be avoided in environmentally sensitive areas. All development should enhance the existing environmental features and replace lost environmental features to the fullest extent possible.
2. *Incorporate low-impact development design features and implement green building techniques.* New building designs and building rehabilitation and redevelopment projects should strive to incorporate the latest environmental technologies in project building and site design. Wherever possible, existing buildings within the plan area should be reused and retrofitted for new uses.
3. *Affirm county Commission 2000 and state Smart Growth initiatives.* New development and redevelopment should enhance existing green infrastructure elements such as wetlands, woodlands, open space, landscaped areas, street tree corridors, and sensitive species habitats. It should also establish open space linkages where they do not currently exist.
4. *Seek opportunities to create new, connected green infrastructure elements.* New or redevelopment project proposals should establish landscaped areas and open space connections between adjacent parcels and the stream valley.

The following recommendations relate to specific elements that comprise the environmental framework:

STORMWATER MANAGEMENT AND STREAM RESTORATION

1. Low-impact stormwater management methods should be used on new development or

redevelopment projects. Low-impact development techniques treat stormwater by removing pollutants and slowing water flow by providing filtration and/or infiltration into the soil on-site. In most instances the use of low-impact development techniques eliminates the need for traditional stormwater management ponds, thereby increasing the amount of developable land and eliminating pond maintenance costs.

2. *Retrofit existing stormwater quantity and control management practices.* Some sites that are proposed for redevelopment may have existing stormwater management facilities on-site and new development may use existing stormwater management facilities to meet their on-site requirements. These facilities should be retrofitted using low-impact development techniques. Some appropriate techniques may include sand filters, infiltration trenches or on-site retention facilities such as rain barrels or cisterns.
3. Preserve and reestablish woodland when constructing stormwater management ponds. Low-impact development is preferable to stormwater management ponds. However, in some areas a pond may be the only option. In this situation, the pond should incorporate all appropriate reforestation, revegetation and bioengineering techniques.
4. *Restore physically degraded streams through bioengineering techniques.* The streams in the area have been physically degraded by stormwater reaching streambanks untreated and unhindered. Carefully planned streambank restoration projects are part of the overall plan to restore green infrastructure along the area's existing streams. When planned, these projects should implement bioengineering techniques to replicate the preexisting natural condition of the streams. Bioengineering techniques include the use of logs, stone,

trees and plants implemented through a plan to restore the natural look and function of degraded streams.

FLOODPLAIN

5. *New buildings should be elevated out of the floodplain when redevelopment occurs.* The existing 100-year floodplain poses challenges to redevelopment (see Map 11). Map 12 illustrates the computed depth of floodwater in a 100-year storm. Because the upstream drainage area of Paint Branch and backflow from Indian Creek dominate flooding effects on the corridor, no significant local solutions are possible. However, during redevelopment of any property within the 100-year floodplain, the potential for damage from a 100-year storm will be carefully evaluated. All parking areas and roadways shall be no more than 18 inches below the computed 100-year floodplain elevation. The lowest point where floodwater might enter a building should be located at least 12 inches above the 100-year storm elevation.

WETLANDS

6. *New development should avoid impacting wetlands.* Existing nontidal wetlands are limited to the Paint Branch stream valley. New development should not only avoid impacts to existing wetlands but, where feasible, construct new wetlands with the intent of replicating the functions provided by natural wetlands, including pollutant interception, erosion control, and provision of wildlife habitat.

WOODLAND CONSERVATION

7. *Replanting trees should be a priority along the corridor.* With the large amount of impervious surfaces within the corridor, there is very little green area other than on The University of Maryland property and in the vicinity of Paint Branch. The redevelopment of most properties will not be subject to the Woodland



The Paint Branch Stream Valley is an important resource for the College Park area.

Conservation Ordinance because they contain less than 10,000 square feet of woodland. However, all development proposals should place a high priority on incorporating trees into site design.

8. *Establish street tree corridors along roadways and on individual properties.* Street trees provide multiple benefits in a built environment, including reductions in ambient air and surface temperatures, reductions in ground-level ozone, and improvements to air quality. Street tree corridors should be established on both public and private properties in planting strips designed to promote long-term health benefits.

NOISE POLLUTION

9. *Noise impact should be evaluated for proposed development.* There are two types



Street trees provide many benefits in an urban setting such as the US 1 corridor.

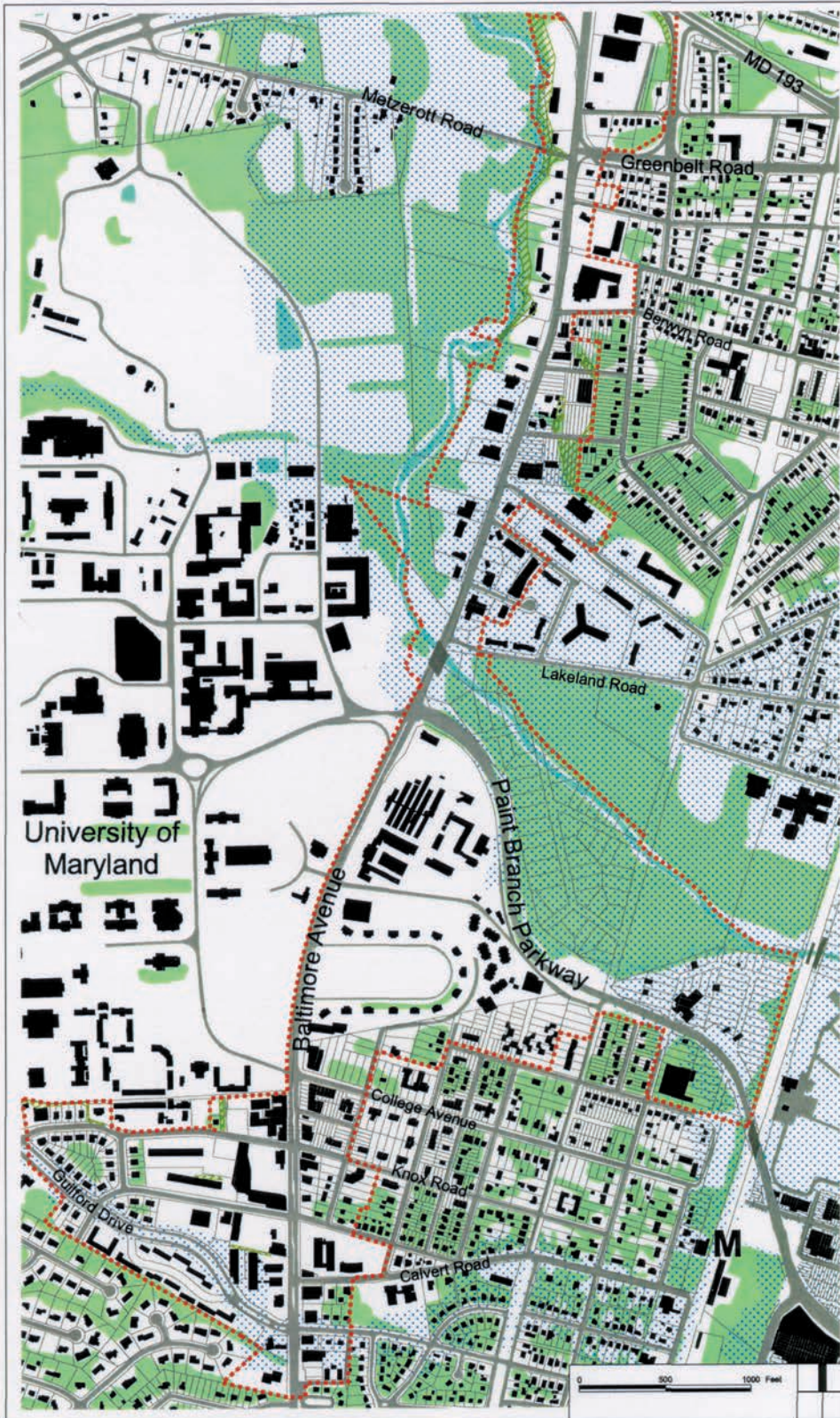
of noise that should be evaluated. One type is the noise generated by a proposed use and its potential impact on residential areas. Specific uses that generate noise adjacent to residential uses should be evaluated in conformance with state noise standards. Noise mitigation measures should be required where necessary to reduce noise levels to within the state noise standards.

The second type of noise that should be evaluated is transportation-generated, which is only a concern for residential uses. The maximum exterior noise level for residential uses is 65 decibels (dBA) during the day and 55 dBA at night (10 p.m. to 7 a.m.). The maximum interior noise level is 45 dBA Ldn. Ldn (level-day/night) is a measurement of noise over a 24-hour period. Noise mitigation measures should be required where necessary to reduce noise levels to within state noise standards.



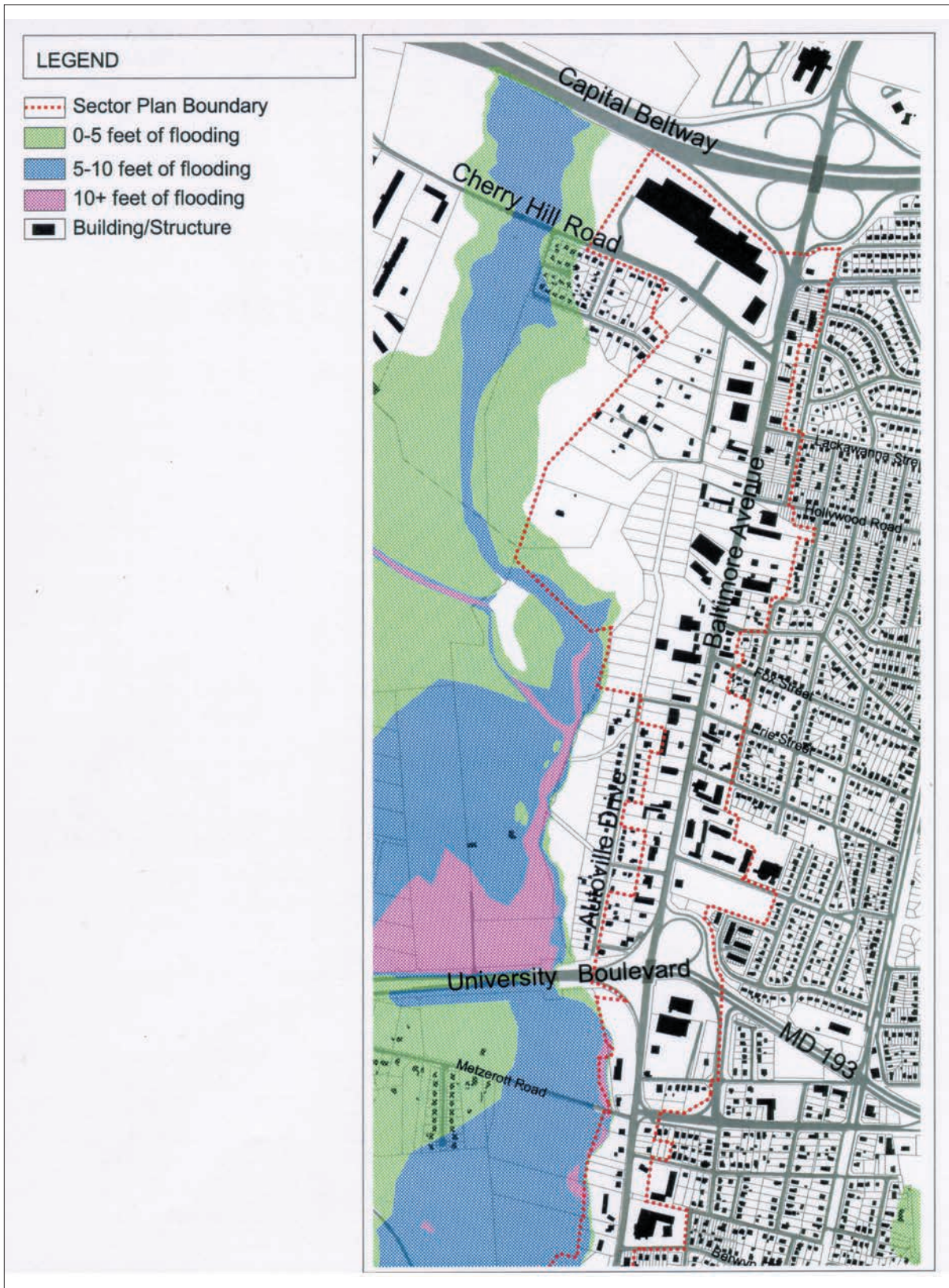
Environmental Features-North

Map I 1a



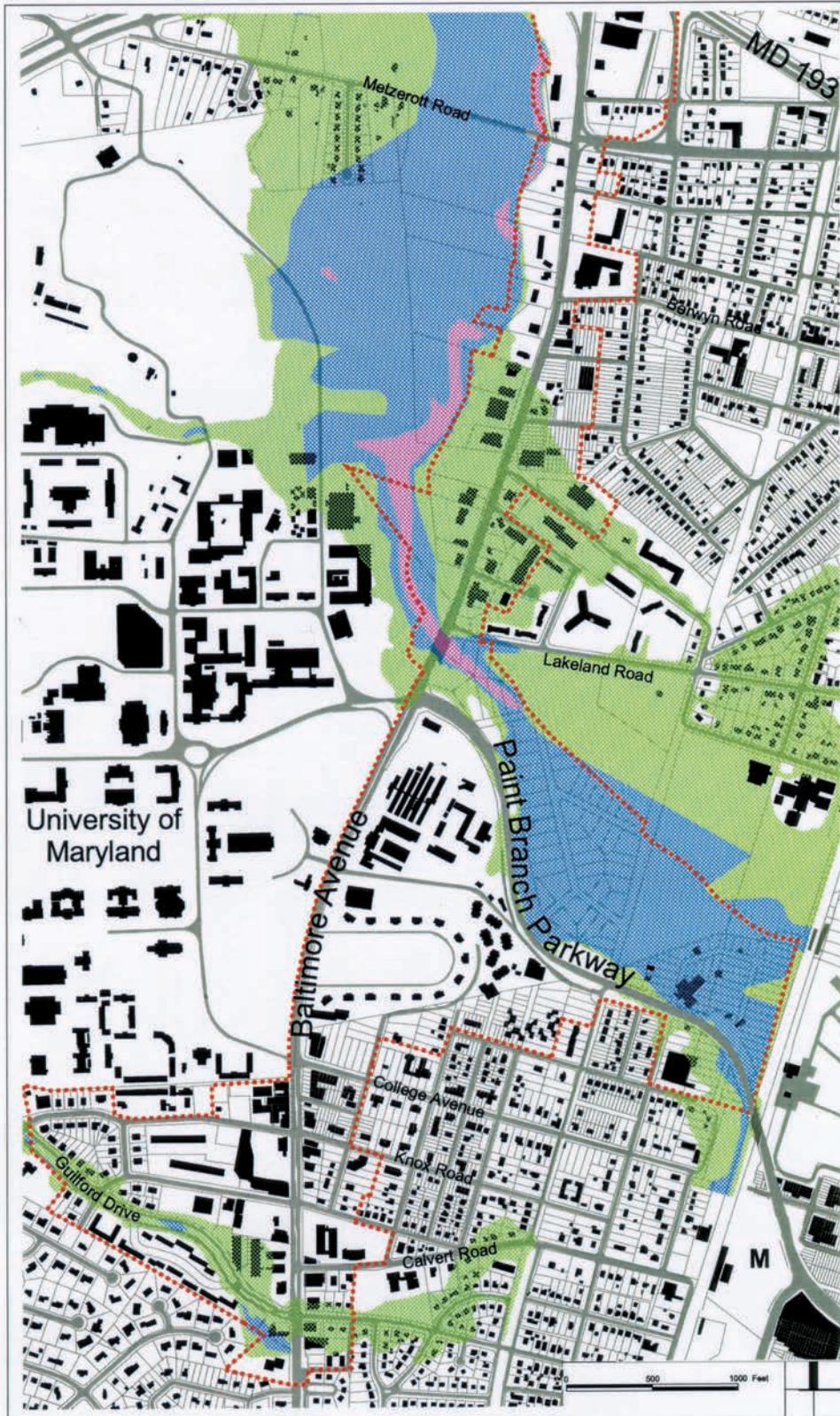
Environmental Features-South

Map I 1b



Flooding Depths-North

Map 12a



Flooding Depths-South

Map 12b

AIR QUALITY

10. *Continue efforts to aggressively curb air quality deterioration.* Planning for air quality is a regional responsibility of the Metropolitan Washington Council of Governments. In the near future, the state is expected to adopt and implement an air quality plan to bring Maryland in conformance with the 1990 Amendment to the Clean Air Act.

The plan's emphasis on Smart Growth and mixed-use development will reduce air pollution as reliance on vehicles is reduced. Further reductions are achieved by concentrating new development in compact and vertical mixed-use development forms. As development proposals are submitted, they should be reviewed for inclusion of efforts to promote nonautomobile use by residents, customers and employees. The use of available state and local incentives to help reduce the use of the automobile and in turn reduce air pollution is encouraged.

PUBLIC FACILITIES

The sector plan incorporates a range of facilities and services that meet the needs of residents, businesses and others. Existing public facilities and service systems have been evaluated to ensure that the land use proposals and the demands they will generate can be accommodated and meet approved standards within the capacity of each existing, proposed or planned system.

GOALS

- Provide adequate public facilities in a timely manner to meet the needs of ongoing and future land uses within the sector area.
- Support development which is economically advantageous to the city and the county by providing an adequate system of public facilities and services.
- Apply public services to support the recommendations of this sector plan, including elimination of the physical evidence of neglect and reduction in visual clutter.

BACKGROUND AND ISSUES

The sector plan area is served by a range of public facilities and services, including schools, parks, trails, libraries, recreation, public safety (police, fire and rescue), and health services, all of which are described be-

low. Additional service systems that are publicly provided in this area include transportation, water, sewerage and storm drainage, all of which are planned and provided at the regional level.

PUBLIC SCHOOLS

- The US 1 corridor area within the city is primarily served by three elementary, two middle, and two high schools, as listed in Table 5, which includes address, year built, building size, and capacity. The adopted FY 2001-2006 Capital Improvement Program proposes renovation of a former elementary school site now used by the Maryland Fire and Rescue Institute in Berwyn Heights (6200 Pontiac Street). It is programmed for reconversion to a 450-seat elementary school with a capacity for 450 students. The renovation is expected to cost \$5.4 million and is programmed for full

**Table 5
Schools Serving the College Park US 1 Corridor**

Elementary	School Address	Year Built	Building Size	1999 Capacity
Hollywood	9811 49 th Ave	1952	40,500 s.f.	354
Paint Branch	5105 Pierce Ave.	1972	59,021 s.f.	469
University Park	4315 Underwood St.	1978	50,479 s.f.	546
Middle				
Greenbelt	8950 Edmonston Rd.	1937	141,125 s.f.	802
Hyattsville	6001 42 nd Ave.	1938	119,597 s.f.	648
High				
Northwestern	7000 Adelphi Rd.	1951 (rebuilt and renovated 2000)	276,013 s.f.	2,064 (FY 2000)
Parkdale	6001 Good Luck Rd.	1968	254,965 s.f.	2,007

funding in FY 2003. The funding sources indicated \$2.59 million in county government obligation bonds and \$2.84 million in state funding. There are several other schools that have students assigned from the sector plan area because of special circumstances or programs. These include Hyattsville, Beltsville, Riverdale and Hollywood Elementary Schools, Buck Lodge Middle School, and High Point and Eleanor Roosevelt High Schools.

PARKS

- The western edge of the sector plan area between the Capital Beltway and the Paint Branch bridge under US 1 is adjoined by extensive public parkland, centered on the Paint Branch Stream Valley. (See Map 13.) From north to south, this includes the Cherry Hill Road Community Park, the Cherry Hill Neighborhood Park, Paint Branch Stream Valley Park (part), Paint Branch Golf Course, Acredale Community Park and Paint Branch Stream Valley Park (part). Continuing along the stream east of the bridge crossing, additional park areas include the Lakeland Community Park, College Park Community Center and Paint Branch Stream Valley Park (part), College Park Airport and the Lake Artemesia Conservation Area. The only areas of parkland within the sector plan area are along Paint Branch in the immediate vicinity of the US 1 bridge crossing. The city owns and operates seven neighborhood playgrounds in the area.



Parks and open space provide amenities and recreational opportunities a short distance from the US 1 commercial corridor.

As a condition of approval, new residential subdivisions in Prince George's County are required to either dedicate land for park and recreation use, provide a fee-in-lieu, or develop private recreational facilities on site, the contribution levels being defined in the Subdivision Regulations.

TRAILS

- US 1 in College Park is a vital bicycle and pedestrian corridor, heavily used by nonvehicular traffic connecting businesses, residences, The University of Maryland campus, and other locations. It is an important link in the county's planned bike and trails network and will connect to the existing Anacostia Tributaries Trails System, the partially designed Rhode Island Avenue Trail, and many other future bike routes and trails.

At this time the Paint Branch Trail is the only existing major facility, extending from Cherry Hill Road to Lakeland Avenue. Many cross streets that will provide future trail choices linking Rhode Island Avenue to US 1 and indirectly to the Paint Branch Trail include Edgewood, Hollywood, Fox, Greenbelt, Berwyn, Lakeland, Paint Branch, College, Knox and Calvert (see Map 10 in the Transportation and Circulation chapter).

LIBRARIES

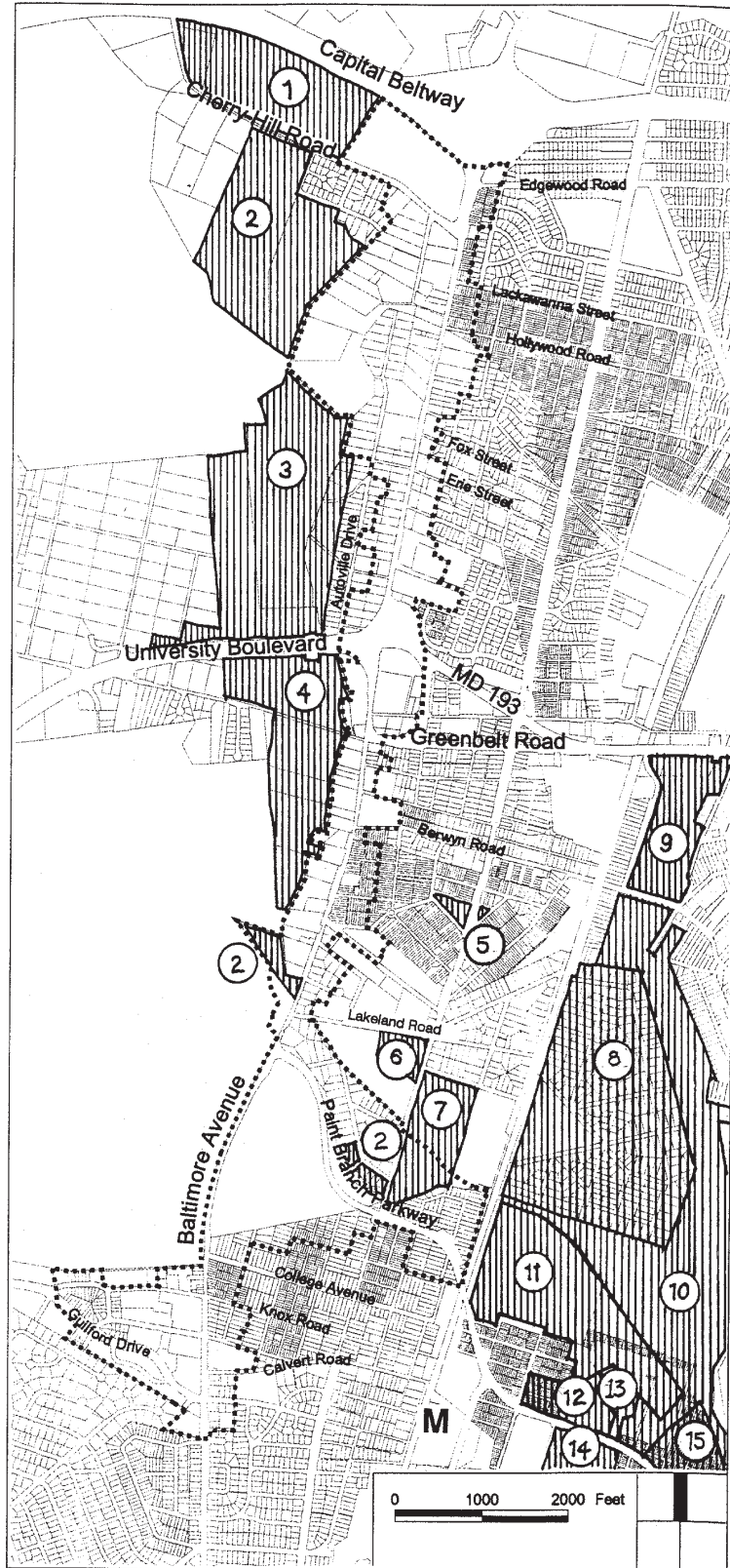
- County library service for corridor residents and adjoining areas is provided by four local libraries—Beltsville, Berwyn Heights, Hyattsville and Greenbelt. Additional and specialized library service is provided in part by The University of Maryland at the McKeldin and Hornbake libraries on the university campus, and by the National Agricultural Library at the Beltsville Agricultural Research Center.

RECREATION

- The College Park Community Center on Pierce Avenue is a recreational focus for local residents. It contains meeting rooms and includes facilities for basketball, volleyball and other activities, with public

LEGEND

- ① Cherry Hill Road Community Park
- ② Paint Branch Stream Valley Park
- ③ Paint Branch Golf Course
- ④ Acredale Community Park
- ⑤ Berwyn Neighborhood Playground
- ⑥ Lakeland Neighborhood Park
- ⑦ College Park Community Center
- ⑧ Lake Artemesia
- ⑨ Indian Creek Stream Valley Park
- ⑩ NE Branch Stream Valley Park
- ⑪ College Park Airport & Museum
- ⑫ Junior Tennis Champions Center
- ⑬ 94th Aero-Squadron Restaurant
- ⑭ Ellen E. Linson Swimming Pool & Herbert W. Wells Ice Rink
- ⑮ Paint Branch Parkway Community Park



Park Facilities

Map 13

parking, restrooms, a playground and a ballfield.

POLICE

■ The entire corridor is within and is served by Prince George’s County Police District I, with facilities located on Rhode Island Avenue in Hyattsville. The sector plan area is covered by beats A-6, B-3, B-6 and B-7. Additional police service is provided in part by the Maryland State Police, Barrack Q, and The University of Maryland (Campus) Police. The City of College Park does not have a municipal police department.

FIRE AND RESCUE

■ Due to the elongated nature of the sector plan area, fire and rescue service is provided by six separate stations. Only the College Park station is physically within the sector plan area. Collectively they provide engine, ladder, ambulance and paramedic service. Table 6 indicates recommended response times for fire and rescue services. Table 7 lists the six stations serving the sector plan area.

HEALTH

■ The four primary sources of public health care serving residents of the area are the Prince George’s County Health Department headquarters facilities and the Prince George’s Hospital, both in Cheverly; the

Belcrest Road Health Center in Hyattsville; and the Greater Laurel Hospital.



The College Park station serves as a landmark and provides fire and rescue service for the community.

Response Time	Service	Area
3.25 min.	Engine	Multifamily/Commercial
4.25 min.	Ambulance	Multifamily/Commercial
4.25 min.	Ladder truck	Multifamily/Commercial
5.25 min.	Engine	Single-family
6.25 min.	Ambulance	Single-family
7.25 min.	Paramedic	All uses

Co. No. and Name	Address	Services Available
Co. 12 College Park	8115 Baltimore Blvd.	paramedic, engine, ambulance, ladder
Co. 31 Beltsville	4911 Prince George’s Ave.	engine, ambulance, ladder
Co. 14 Berwyn Heights	8811 60th Avenue	ladder, ambulance, rescue
Co. 11 Branchville	4905 Branchville Road	engine, ambulance
Co. 7 Riverdale	4714 Queensbury Road	engine, ambulance, ladder
Co. 1 Hyattsville	6200 Belcrest Road	engine, ladder, ambulance

CONCEPT

The existing network of public facilities and services for the sector plan area is an appropriate match for the type and style of land use in the corridor and surrounding areas. The recommendations of this plan will result in a range of uses and a more modern, aesthetically pleasing style of development. Residential population in the area will increase, commercial growth will have a different form, and buildings will be built according to the innovative recommendations of this plan. These and other significant changes should be monitored over time and their effect on needed public facilities and services evaluated. Any modifications needed in individual delivery systems will be identified and transmitted to the appropriate agency.

Table 8 compares the estimated buildout of the sector plan area based on existing zoning

to buildout of the plan based on proposed land use. Buildout under existing zoning assumes development on currently vacant or undeveloped properties. The plan proposed buildout estimates are derived from the illustrative concept sketches (Maps 8-1 to 8-6) and generally assume redevelopment of existing properties in accordance with the development character desired in each area and subarea.

RECOMMENDATIONS

There are no identified capital improvement program impacts at this time for any of the above public facilities with the possible exception of Company 31 in Beltsville. This station provides secondary service to the sector plan area and is proposed for relocation in FY 2002 at a cost of approximately \$3.5 million. Table 9 identifies roadway, bikeway and trail improvements, along with projected

**Table 8
Estimated Buildout Under Existing Zoning and Plan Proposals**

	Buildout Under Existing Zoning	Buildout Under Plan Proposal
Office Employment	1,160	4,160
Retail Employment	3,600	2,500
Institutional Employment	1,330	790
Total Employment	6,090	7,450
Multifamily Dwelling Units	600	1,420
Single-family Dwelling Units	220	470
Group Quarters	770	1,620
Total Units	1,590	3,510
Multifamily Population	1,220	2,870
Single-family Population	560	1,200
Group Population	1,130	2,970
Total Population	2,910	7,040

Source: M-NCPPC Community Planning Division

Estimates based on average development yields for employment, dwelling units and population. Institutional employment, group quarter dwellings and population are related to student housing on campus.

Existing zoning multifamily dwellings and population are largely university-related.

costs, that are recommended to enhance existing development and support future development. Each public agency will evaluate the sector plan's existing and projected development yields for potential impacts upon agency operations. At this time the plan recommends retention of existing facilities and their expansion as necessary to maintain adequate levels of service for each facility or service.

**Table 9
Public Facility Cost Estimates**

Council Bill CB17-1998 requires all approved master or sector plans to estimate the cost of all public facilities which must be acquired or constructed to carry out the objectives and requirements of the plan, as a way to gauge potential fiscal impact of public facilities recommended or required by the plan. The following estimates are based on 1999 dollars and should be considered as estimates. It is important to note that some of the referenced transportation improvements in whole or in part will be or could be built by developers.

Project Name And Description

1. Baltimore Avenue (US 1)	(\$81.5 - \$92.5 Million)
<p>The Maryland State Highway Administration (SHA) is currently studying the feasibility to reconstruct US 1 as a four-lane divided gateway boulevard with streetscape treatment along the shoulders and within its median. Based on the latest cost estimates prepared by the SHA, the total estimated cost for this improvement will range from \$81.5 to \$92.5 million. The cost estimate does not include the \$5 to \$7 million cost for the relocation of overhead facilities or the placement underground which is estimated to be in the range of \$24 to \$27 million. Based on ongoing engineering studies, these cost figures may be adjusted by SHA.</p> <p>Included in the cost estimates are some at-grade improvements for the intersection of US 1 with Cherry Hill Road. This project is currently funded in the State’s Consolidated Transportation Programs (CTP) only for project planning. This project is included in the county’s construction priority list which has been submitted to SHA.</p>	
2. US 1/Cherry Hill Road Grade-Separated Interchange	(\$25 to \$29 Million)
<p>SHA has developed an interchange concept based on the recommendations of the plan that would carry Cherry Hill Road under the existing grade of US 1. The through traffic along US 1 will pass through without any interruption. The SHA’s cost estimate for this improvement ranges from \$25 to \$29 million. Based on ongoing engineering studies, this cost estimate may be adjusted by SHA. This improvement could be incorporated with the US 1 improvement option or could be constructed as a separate project.</p>	
3. Cherry Hill Road and Autoville Drive North Intersection Relocation	(\$300,000 to \$400,000)
<p>This would improve traffic movements to and from Autoville Drive North and its intersection with the shopping center’s access driveway and Cherry Hill Road. The improvement will result in a four-way signalized intersection. The estimated cost ranges from \$300,000 to \$400,000. Not included in this cost estimate are right-of-way and beautification costs.</p>	
4. Autoville Drive North	(\$400,000 to \$550,000)
<p>An internal two-lane loop extension of Autoville Drive North is recommended to provide access to and from US 1 to subareas 4f and 5b. The estimated cost for this improvement ranges from \$400,000 to \$550,000. This improvement is needed to accommodate planned development in this area. It is also recommended that the developers dedicate the needed right-of-way and fund the construction of this roadway.</p>	
5. Paint Branch Parkway dualization and landscaping	(\$1,000,000 to \$1,200,000)
<p>The Paint Branch Parkway between US 1 and River Road is recommended to be improved to a four-lane divided collector roadway with landscaped median. A similar improvement is recommended by the College Park-Riverdale Transit District Development Plan for the segment of Paint Branch Parkway between Kenilworth Avenue and River Road.</p>	

6. Guilford Drive dualization and landscaping	(\$300,000 to \$400,000)
<p>The portion of Guilford Drive within the sector plan boundary is recommended to be improved to a four-lane divided collector roadway with an open landscaped median. The estimated cost for this improvement ranges from \$300,000 to \$400,000. This improvement is included as part of the redevelopment of the “Knox Boxes” area. It is recommended that the developers dedicate the needed right-of-way and fund the construction of this roadway. For the areas outside the sector plan boundaries, construction funding may be provided by the university, county or others.</p> <p>The following trail/bike lane cost estimates have been determined using standard costs per square or linear foot for trail construction or bike lane striping, plus costs for bikeway signage. Ultimate costs will depend on the type of facility chosen. On-road facilities can range from striped bike lanes with signage to wide curb lanes or shoulders.</p>	
<u>Facility</u>	<u>Cost</u> <u>CIP #</u>
Three trail links from US 1 to the university	\$258,000
Paint Branch Trail	NA (existing)
Edgewood Road Bike Lanes/Signage	\$708
Hollywood Road Bike Lanes/Signage	\$708
Greenbelt Road (vicinity of sector plan)	\$772
Berwyn Road Bike Lanes/Signage	\$676
Rosborough Lane Bike Lanes/Signage	\$772
Lakeland Road Bike Lanes/Signage	\$676
Knox Road Bike Lanes/Signage	\$1,384
College Avenue Bike Lanes/Signage	\$1,256
Columbia Avenue Bike Lanes/Signage	\$532
Calvert Road Bike Lanes/Signage	\$1,256
Guilford Drive Bike Lanes/Signage	\$820
Cherry Hill Road Bike Lanes & Trail	\$89,604** FD665611
Kiernan Road Bike Lanes/Signage	\$320
Park Drive Bike Lanes/Signage	\$64
Paint Branch Parkway Bike Lanes & Trail	\$160,628 **
TOTAL	\$518,176
<p>* Total cost assumes three trail links of approximately 2,000 feet with a 80-foot x 40-foot steel bridge over Paint Branch.</p> <p>** Total cost assumes an 80-foot-wide trail on one side of the road and on-road bike lanes.</p>	

ECONOMIC DEVELOPMENT STRATEGY

The sector plan's land use and zoning strategies establish a flexible policy and regulatory framework to facilitate market-based decisions by the private sector. However, the economic development strategy is intended to offer guidance and market-based development tools and incentives for consideration by the community-at-large in implementing corridor revitalization. The strategy is based on market findings that indicate development opportunities in all categories (residential, office, retail and lodging), particularly in relation to the needs of The University of Maryland. To realize these opportunities, the strategy identifies several barriers to overcome and possible solutions. It will take a joint commitment by both the public and private sectors to realize the goal of a revitalized mixed-use corridor.

GOALS

- Strengthen existing partnerships between public agencies and engage with the private sector to encourage redevelopment projects.
- Utilize incentives to promote private sector development of housing, especially as part of mixed-use development.
- Capitalize on existing corridor strengths, especially current mixed-use areas.



The University of Maryland commands a dominant visual and economic presence along US 1.

BACKGROUND AND ISSUES

As part of the planning process, an economic study was conducted to evaluate market support for achieving the type and mix of land uses proposed in the sector plan. Also, an economic development strategy and action plan were developed to guide redevelopment in accordance with the sector plan's land use and zoning recommendations.

MARKET CONDITIONS

Market support for various land uses was assessed based on existing market studies, interviews with stakeholders, and research on key market indicators. The assessment assumed that the private sector would respond to market opportunities independently with no economic development program in place or direct assistance from the public sector or university. The following identifies opportunities for new development and constraints that impede redevelopment.

Opportunities for Development

Residential: There is strong demand for both student and conventional multifamily housing as evidenced by a vacancy rate of less than one percent, well below the industry threshold of five percent. Rental rates among relatively new, competitive multifamily residential projects within the study area indicate support for new multifamily projects. Rates for the fully occupied 300-unit Wynfield Park range from \$955 to \$1,030 for one-bedroom units per month to \$1,570 for three-bedroom units in 2000. These rates are 40 to 50 percent higher than those of the majority or area garden apartments. Also, The University of Maryland has acknowledged a critical demand for student housing, citing market support for a total 1,135 to 2,380 additional student beds as of 1998. In August 2000, the University Courtyard student apartments were opened, containing 704 beds in 224 fully occupied units. Potential sites identified for residential development include:

Area 5b, south of Cherry Hill Road, could support new market rate multifamily housing oriented to young professionals within the next two to seven years. Also in the short-term (within two years from plan approval), moderate to high density residential development could occur on land that is assembled in Area 5, and/or on the bowling alley site in Area 4. Within a period of three to ten years, there should be support for redevelopment of the “Knox Boxes” within Area 1 for new multifamily units.

Office: Market indicators demonstrate support for near-term commercial office development. In fall 2000, there was 408,745 square feet of office space in 12 multitenant buildings within the study area, of which 42,870 square feet was vacant. Office lease rates vary depending on corridor location,



Infill commercial office development in downtown College Park partially satisfies market demand.



A recently constructed office building located in the northern portion of the corridor has convenient access to the Beltway.

building age and amenities; from \$10 to \$15 for Class-C space to \$19 to \$21 for Class-A office space in 2000. The university leased 62,000 square feet of office space within walking distance to the campus (17 percent of the city’s occupied office space) and another 50,000 square feet in nearby communities in 2000. With the university’s continuing office space need, existing vacant space will continue to be absorbed within the near-term. Also, the university’s rise in national prominence in the fields of computer science and electrical engineering has begun to attract private sector interest in co-locating research facilities adjacent to the university. However, these positive indicators are not likely to be translated into a new office building until an anchor tenant is identified to lease a significant amount of space for a long-term period. Because the university is particularly concerned about the scarcity of quality office space, it should consider taking the lead and commit to a large block of space for a long-term lease. Potential sites identified for office development include:

A single office building (50,000 to 80,000 square feet) with ground floor retail could be supported over the next two to seven years where land can be acquired in the main street area sufficient to support surface parking. Potential locations for such an office are the Koons Ford western sales display lot (owned by the university in Area 3a), property near the MD 193 interchange (Area 4c), or property near Metro (Area 1e).



A new commercial building complements the development character in downtown College Park.



Opportunities exist for new infill commercial development all along the corridor.

Retail and Service Commercial: There is significant retail development potential in the US 1 corridor. In 2000, approximately 820,000 square feet of retail exists within the sector plan area, with half of this space in shopping centers and the remainder in stand alone buildings. The vacancy rate among shopping centers is approximately five percent and is approximately eight percent for stand alone buildings. The overall six percent vacancy rate is considered high for a commercial corridor.

There is an estimated \$270 million retail sales potential in the sector plan market area, which includes approximately \$108 million for city residents, \$127 million for university-related students/faculty/employees, with the remainder being visitors and others. The analysis indicates that two-thirds of the spending demand is being captured by the US 1 commercial corridor which is dominated by auto-related businesses, even though an estimated 70 percent of sales potential is non-auto-related. Potential sites identified for retail/service commercial development include:

Stand-alone retail uses will continue to be developed near the Beltway in Area 6, and throughout the corridor because of high traffic volume. In the main street and town center (Areas 1b, 1c, 1d, and 3a, 3b, respectively) the greatest potential exists for multitenant retail projects. While the lack of land and parking constrain potential, the plan's relaxed parking regulations should act as an incentive encouraging mixed-use

development. Opportunities do exist however, such as the old gas station site along the east side of US 1 at Hartwick Road. This site has been identified as a potential short-term project for mixed-use of office, retail and residential development. Overall, new retail and mixed-use development will likely take sales from existing stores in obsolete environments and recapture a share of their sales in new buildings. For automotive sales and service-related uses, the findings indicate that significant public/quasi-public intervention would be required to offer and encourage auto-related uses to relocate into an autopark suggested as an option in Areas 6b and 4f.

Hotel: The magnitude of demand for a full-service hotel is difficult to discern given the lack of similar facilities in College Park. However, there is near-term development



A newly constructed hotel assists in meeting the demand for rooms.



Older, strip-oriented motels do not meet the demand for quality lodging needed along US 1.

potential for an additional limited service hotel or an extended stay hotel. In addition, the university indicates a continuing need for a full-service hotel to support visitation and the marketing of their business development efforts. Also, local hotel/motel occupancy and average daily rates have consistently increased over the last six years. With the university's continuing interest and need for providing adequate visitor space, together with the performance of nearby Greenbelt hotels, there appears to be mid-term support for a new full-service hotel in College Park over the next three to ten years if the right incentive package were available. Potential sites identified for hotel development include:

Area 6 is an attractive hotel location because of convenient Beltway access. The Jiffy Lube site, in Area 4a, including the abandoned buildings to its south is another short-term possibility for a hotel/motel provided land assembly occurs. Area 1d is attractive because of its location in East Campus. A more detailed market and financial analysis is necessary to determine the feasibility of a full-service hotel in College Park.

Constraints to Development

Small Parcel Sizes and Multiple Ownership Patterns

The primary means to achieve the plan's intended intensification of land use in compact and mixed-use development projects requires that small, multiple properties be acquired and assembled for redevelopment. Currently the pattern of small parcels, fragmented in multiple ownership, impede significant redevelopment as developers must negotiate a variety of land disposition agreements with multiple owners. This process adds time, risk, and cost to the redevelopment process.

High Land Prices

Inflated land asking prices along the US 1 corridor threaten redevelopment potential. Stakeholder interviews reveal that prices are inflated either due to owners' disinterest in selling or an inflated concept of value. Pro-

spective developers consistently cite the failure of property acquisition transactions because of an inability to negotiate a fair selling price. It is not unusual within a commercial corridor for property owners to value land based on recent sale transactions involving a fast-food restaurant or a gas station. However, such land uses are only suitable for limited sites along a corridor and, if used to gauge an asking price, often lead to unrealistic perceptions of land value resulting in no sales.

Inappropriate Parking Regulations

Prior to the new regulations approved in this sector plan, parking requirements had been suburban in nature and utilized countywide mandated minimum parking ratios. For mixed-use development in an urban setting, such minimum parking requirements are often too high because each use has its own requirement and little opportunity exists for shared parking. There were no incentives or regulations that acknowledged the benefit that mixed-use development and multi-purpose trips could have on decreasing parking demand in compact mixed-use areas.

Another constraint is the lack of and high cost of providing structured parking in the town center and main street areas. The problem is that market rents for office space in College Park are not high enough to support the construction of structured parking and provide a reasonable yield on investment to the developer.

Inappropriate Zoning

Zoning prior to sector plan approval limited mixed-use development except in certain instances where residential use is allowed by special exception above the third floor of a commercially zoned building or a commercial building contains no more than three dwelling units. The inability to provide opportunities for a variety of mixed-use development hampered redevelopment efforts. Therefore, it is essential that zoning policies be changed to encourage the type of infill and mixed-use development envisioned for the corridor.

POTENTIAL DEVELOPMENT INCENTIVES AND INITIATIVES

The primary challenge in encouraging economic reinvestment and redevelopment along the corridor is to develop a strategy tailored to the area's specific opportunities and constraints. The following incentives and techniques can be employed to remove constraints. They range from relatively "soft" incentives and programs to "hard" subsidies or direct assistance by the public sector.

Marketing and Promotion

Achieving a high quality mixed-use environment will require the recruitment of speciality retailers, high-end hotel franchises, high-technology firms, and expanding companies. The vision and implementation recommendations embodied in this sector plan can form the basis for developing a marketing and promotion campaign to attract such development. Marketing and promoting the corridor's strategic location, its proximity to the university, together with available development incentives should result in attracting interest from the development community.

The Prince George's County Economic Development Corporation (PGCEDC) has programs to market the county as a business location and assist prospective businesses through recruitment and retention programs. A partnership between the PGCEDC and the College Park City University Partnership (CPCUP) will greatly enhance opportunities to specifically market development opportunities such as the County's High Technology Triangle, which includes The University of Maryland and City of College Park. In addition, the City of College Park could expand its economic development and business retention program via increasing use of its web site as a marketing clearing house. Also, expansion of the city's merchants association outside of the downtown area would assist in managing and promoting corridor business activities.

Streamlined Development Approval Process

Providing technical assistance with the development review process and streamlining development regulations are techniques being used by many jurisdictions to enhance their competitive position in the marketplace. A development approval process that is streamlined and understandable to the development community will help reduce time and financial risk exposure. The development community has consistently encouraged a development process that is predictable but flexible in its application. Flexibility is considered attractive, uncertainty is not. Typically, a developer's equity investment begins very early in the development approval process, which can prove to be a very costly and risky endeavor. Investors measure their financial return on the equity (valued as time and money) they have invested in the project. Providing staff support and assistance throughout the development approval process lowers investor risk.

Efforts to publicize the development review process and assist the development community in understanding what is expected and what incentives are available can help induce redevelopment. The Development District Overlay Zone (DDOZ) and its development district regulations have been developed to specify minimum standards so that developers can ascertain expectations and risks before committing to the project and review process. The DDOZ regulations simplify the rezoning process and retain flexibility to allow reasonable deviations to development regulations during the site plan approval process. Also, the Mixed-Use Infill (M-U-I) Zone is intended to simplify approval procedures for mixed-use development by removing the requirement to seek rezoning of a commercial zone to allow residential use. This is a dramatic departure from the time consuming and very risky conventional rezoning process which requires a developer to prove that either the existing zoning is a mistake or that conditions have changed so dramatically to render the existing zoning obsolete.

Flexible Parking Regulations

Parking regulations that minimize the provision of on-site parking and maximize the opportunities for shared-use parking in mixed-use development areas are an incentive that can help attract new development. Accordingly, the sector plan's DDOZ parking regulations have been designed to support, not penalize, mixed-use development. Parking maximums have been developed along with parking credit reductions where shared use and structured parking is provided.

Public Parking

Using public sector financing to develop structured parking in support of mixed-use development is a major incentive for attracting infill redevelopment into areas with no land available for surface parking, such as the town center and main street areas. Public parking is appropriate when a range of land uses benefit from the parking, not simply one user. Also, market rents in the sector plan area are not strong enough to support the private financing of structured parking.

The Prince George's County Parking Authority's mission is to encourage economic development and revitalization by financing, constructing, and operating parking and related facilities. The Parking Authority could construct and operate public parking in College Park since it is likely that the financing would come from the county, other non-city sources and parking revenue. Alternatively, the city could expand its municipal parking operations to take on the provision and maintenance of structured parking facilities.

The plan's DDOZ regulations allow a waiver of all on-site parking requirements, provided an in-lieu parking fee program is established to encourage public parking in the town center and main street areas. New development would pay either a one-time up-front fee or an annual fee to help compensate for the cost of public parking. The in-lieu parking fee is typically reasonable, and rarely does it approach the true cost of providing parking. In-lieu parking fees can be used to offset operating or capital costs

Tax Credits and Abatement

The use of property tax credits to encourage reinvestment and redevelopment is a common incentive used throughout the country and within the county. By providing property tax incentives in return for reinvestment into specially designated areas, the local government can facilitate reinvestment and redevelopment.

The majority of the sector plan area is within census tracts that are eligible for the county's Revitalization Property Tax Credit program. The only area not eligible is a portion of Census Tract 69, located north of Cherry Hill Road and along the east side of US 1, from the Capital Beltway south to Indian Lane. This program uses a diminishing county property tax credit over several years for assessable improvements made to commercial, industrial, and residential projects. The tax credits are intended to help enhance the financial feasibility of a project by reducing operating costs. Qualifying commercial projects receive a graduated 20 percent tax credit over 5 years, beginning with a 100 percent credit the first year and dropping to 80 percent in the second year, 60 percent in the third year, 40 percent in the fourth year, and 20 percent in the fifth year. Residential project taxes are abated 100 percent in the first year, 66 percent in the second year, and 33 percent in the third year.

The PGCEDC provides the High-Technology Growth and Development Incentive Package. This program provides a three-part incentive program for high technology companies expanding in or newly locating within the county. Property tax credits are available for both new or substantial renovation or expansion of high-technology businesses at the same amount and time period as those discussed above. Personal property tax exemptions are also available for up to 100 percent for certain property used in research and development. The third component of this package program includes a Fast Track Site Development Plan Process which expedites the site plan approval process.

Special Assessment Districts

Special assessment districts are often designated as a means of providing a steady revenue source to fund improvements and/or programs intended to benefit all properties or businesses within the district. Typically private property owners pay an additional assessment fee to support various initiatives within a special assessment district. In Maryland this is called a Commercial District Management Authority. Once collected, the revenue is then reinvested back into the area for district self-directed improvements and/or programs. Special assessment districts could be a valuable tool in funding parking development and operations, public amenities (parks and art), other pedestrian/bicycle-friendly infrastructure and services (maintenance, marketing, etc.) in the town center and main street areas.

Presently the US 1 corridor lacks a means to collectively raise revenue from existing businesses that can then be returned to these same businesses and/or property owners in terms of area-wide benefits. To institute a special assessment district requires that the property owners within the proposed district agree to (1) a special assessment and (2) the use of the additional special assessment revenue within the assessment district. It is likely that such a district would be proposed in concert with a major public investment benefiting all property owners within the district. An example would be a special assessment on commercial properties to help pay for higher levels of landscape maintenance along US 1 than could be provided by limited state funding.

Tax Increment Financing Districts

Tax increment financing (TIF) is a flexible economic development tool used by many jurisdictions across the country. Under this technique, property tax revenues are frozen at the time a TIF district is established. This base level of revenue will continue to flow to the taxing entities over the life of the district. However, as development and redevelopment occur in the district, property tax revenues increase. This increase in property tax

revenue from the base year (or the increment) is retained in a special allocation fund (TIF fund). The monies in the TIF fund are typically placed in a special account and reinvested back into the TIF district. These funds can be used to purchase land and/or fund capital investment through TIF revenue bonds. Use of TIF programs can be an important source of financing joint development projects.

Overall, TIF revenues ensure that the success in a given district generates revenues to support additional investment in the district. Tax increment financing does not increase property taxes. The revenues generated from the district could help support land assembly, land write-downs, and infrastructure development for target projects in the corridor area as well as the provision of amenities. However, it is important to recognize that the use of TIF financing restricts county access to special purpose tax revenues collected solely to benefit the TIF district, thus making this revenue not available for county general fund purposes.

Capital Cost Subsidies

The most desired type of development incentive to encourage involvement by the private sector is to reduce up-front capital cost and risk. Incentive programs that reduce capital cost are more valuable than property tax abatement, accelerated approvals or business development assistance. Public assistance in subsidizing up-front capital cost reduces risk and enhances the financial feasibility of the project. It is a common practice in joint developments to maximize or leverage the use of public resources available for eligible costs. For example, land costs are often absorbed by the public sector in order to make it feasible to finance a higher quality private sector building.

It is important that the state, county and city consider use of capital cost subsidy programs as part of any economic development strategy. The type of capital subsidy used depends on the type of financing sought and the constraints imposed on the use of those public funds. The most common forms of capital

reduction employed by the public/quasi-public sector are as follows:

- Infrastructure development for roads, sidewalks, and landscaping as is presently being programmed and expended on the US 1 roadway improvements study underway
- Provision of public parking facilities
- Assistance with land and building acquisition
- Funding certain soft costs such as technical studies and facade designs

General Obligation and Industrial Revenue Bonds

The sale of general obligation and industrial revenue bonds are common techniques by which the county and city can utilize their financial resources to directly help subsidize public and private sector reinvestment and redevelopment in the sector plan area. It is generally acknowledged by stakeholders within the sector plan area that active investment by the public sector is needed to overcome many of the constraints discussed above. The financing of infrastructure such as roads and parking can be funded by the county and/or city using tax-exempt general obligation bonds. Tax exempt bonds supported by the full faith and credit of the jurisdiction will carry lower interest rates as compared to other financing instruments. It is not unusual for jurisdictions to fund public parking using general obligation bonds.

The county and/or city can also utilize industrial revenue bonds which are tax-exempt bonds that can be used to support manufacturing and 501(c)(3) nonprofit organizations. Funds can be used for land and/or new or used building acquisition, purchase of new machinery or equipment, and some soft costs.

Community Financing Programs

Various financial programs exist at the state, county and city level that can be used to help underwrite the costs for financing reinvestment and redevelopment. Programs such as the Maryland Industrial and Commercial Re-

development Fund (MICRF) provide financial assistance to political jurisdictions attempting to develop or redevelop commercial or industrial enterprises. The jurisdiction may qualify for up to \$3 million, but also must provide an 11 percent funding contribution and pledge its full faith and credit. Funds can be used for fixed assets (land, buildings, machinery/equipment), infrastructure (roads, utilities, telecommunications), and technical assistance studies and some other soft costs. The jurisdiction may loan funds to a business or use funds to guarantee an outside lender loan.

Also, the state offers the Local Government Infrastructure Financing Program to local jurisdictions to provide an efficient and economical means to access capital markets to finance specific infrastructure projects. Examples of eligible projects that could be utilized within the sector plan area are bridges, streets and sidewalks, street lighting, landscaping and other amenities, including stormwater control and drainage facilities. The full faith and credit of the jurisdiction is required as security. In addition, the State Department of Business and Economic Development and the Prince George's County Economic Development Corporation have several other economic development initiatives and programs that may be appropriate for use in the sector plan area.

The PGCEDC has developed the Commercial Building Loan Fund (CBLF) to assist with building renovation, reuse and construction. The Shopping Center Rehabilitation Program (SCRCP) assists owners of older centers to reinvest in rehabilitation of the building, grounds and equipment by providing up to 25 percent of the funding needed. The Commercial Area Revitalization Effort (CARE) Program assists older neighborhood commercial areas by providing low interest loans (up to 50 percent of project cost not exceeding \$300,000), grants, marketing and technical assistance. Lastly, the PGCEDC offers the New Building Loan Program (NBLP) to encourage new retail and commercial projects in inner-beltway communities. This program provides up to 50

percent but no more than \$2 million of required financing.

The Maryland Department of Community Development's Community Legacy Program was developed in 2001 with the aim of halting decline and disinvestment in older areas of the state. The program is focused on at-risk areas that show signs of decline but have underlying strengths that will allow a modest level of investment to improve quality of life. A wide range of capital and noncapital expenses can be funded by the program. These include commercial building renovation, housing renovation, public infrastructure that supports redevelopment projects, building demolition, land banking as part of a redevelopment strategy, and staff support for revitalization projects. In July 2001, the City of College Park made application to the program for designation of US 1 as a legacy area. In January 2002, the city was awarded \$425,000 from the program for use in land acquisition and demolition of two long-time boarded-up structures. The award was based largely on the sector plan's economic development strategy recommendations.

Leveraging In-Kind Contributions: Land

Contributing land is a direct way for the public sector to help initiate redevelopment. As right-of-way and property acquisition occurs as part of the US 1 highway improvement plans and the university acts to redevelop some of its properties, opportunities may arise to offer these public lands as part of a redevelopment project.

To the extent that they own land in the sector plan area, the state, county, city and university could leverage these assets to attract desired development. Typically, this is done by creating or using a quasi-public entity such as the College Park City University Partnership (CPCUP) to market the land. This entity could interest private development on the land by either selling the land below market price, donating the land, and/or leasing the land with attractive lease provisions. Examples of attractive lease provisions include: below market lease rates, a nominal fixed-lease

rate with participation occurring when certain performance thresholds are met; nominal fixed-lease rates until the project is sold or refinanced; or a combination thereof.

Land Readjustment Programs

Also known as land consolidation or land pooling, this approach involves the private sector pooling land for the purpose of creating a larger unified development site. It allows property owners to retain the incremental value gained from the development of their land to more intensive use rather than having the benefit accrue to the developer after the land is sold. Properties are consolidated through a private corporation, landowner's association, a public corporation, or a public agency. Each owner is accorded a share relating their assessed property value as a percentage of the total value of all properties combined. The land is then planned without regard to property lines and is resubdivided and returned to individual property owners, with all development requirements having been satisfied. This results in lots reverting back to the owners as fully serviced, compatible with new development requirements and community design standards, thus bringing substantial value back to the original owners. The project can then be built out separately or by a single developer. Some lots may be sold to offset the cost of improving infrastructure. This concept may be attractive to existing property owners who wish to participate in redevelopment while retaining an ownership interest in the new development.

Public Land Acquisition and Eminent Domain

Acquiring private land for a public purpose such as redevelopment has long been a direct method of implementing redevelopment, provided a fair market value is paid for the property. As described earlier, land assembly is difficult along the US 1 corridor for three reasons: (1) small parcels; (2) fragmented property ownership; and (3) land price speculation. Often the public/private sector can mitigate these constraints by using the power of eminent domain to acquire and assemble

property to support public purposes such as desired redevelopment and private reinvestment. Under eminent domain the price of land is determined by an independent appraiser. If the purchase price cannot be agreed to with the owner, then the courts will decide the fair market value. Often just the concept of eminent domain can effectively reduce land asking price speculation.

The Redevelopment Authority of Prince George's County is authorized by the State of Maryland to: acquire, by legal means (including the power of eminent domain), land or property for development or redevelopment; to develop or redevelop land for residential, commercial, or industrial purposes; to dispose of land for residential, commercial, or industrial purposes; to issue bonds to finance or refinance the cost of acquisition, development, and/or redevelopment; and, to accept grants from, make loans to, and enter into contracts with federal, state or local agencies, or private entities or parties.

The Redevelopment Authority's project selection criteria are as follows:

- Quality of life enhancement
- Quality development
- Economic impact for local community and the county
- Availability of private sector funds
- Financial feasibility and consistency with redevelopment goals and plans
- Builds on existing or prior efforts
- Catalysts for other improvements
- Project's readiness to become self-supporting
- Potential for success
- Level of community support
- Enhancement of balanced growth
- Opportunities to create job training and employment

This broad criteria should enable the Redevelopment Authority to play a major role in helping implement the sector plan, espe-

cially where new jobs are created and business/residential displacements are minimized. The Redevelopment Authority can actively enter into joint development projects with the private sector and has the ability to use eminent domain, if necessary.

The city also has the power under its urban renewal authority to use eminent domain for public purposes. However, the city must meet certain criteria demonstrating a level of blight to justify use of eminent domain. The city's eminent domain power is far less flexible than the Redevelopment Authority's powers. While less flexible, there may be occasions where the city's eminent domain powers may be appropriately utilized.

RECOMMENDATIONS

ECONOMIC DEVELOPMENT STRATEGY OBJECTIVES

The recommendations contained in this section make up the economic development strategy and include specific actions and development project opportunities to be implemented in the short-, mid- and long-term. Short-term implementation is projected to occur within approximately two years from plan approval, mid-term within three to seven years, and long-term beyond seven years.

Strategy I: Bolster Existing Mixed-Use Environments

College Park's existing downtown possesses many of the characteristics sought in an expanded town center: a main street development character, compact and vertical development, a mixed-use center, and a pedestrian environment. Improving the town center will have an immediate impact on the economy, the university's image and competitiveness, and the investment climate. The following actions build on the town center's assets:

- Reduce the amount of surface parking lots and encourage shared and structured parking
- Require new buildings to maintain or reinforce existing building edges along the street frontage

- Expand the mix of land uses beyond the existing high concentration of eating and drinking establishments and/or convenience goods stores that dominate downtown
- Encourage mixed-use redevelopment of the “Knox Boxes” area and the East Campus area



South Campus Commons, the university’s 1,031-bed student housing project, was completed in summer 2002 along Knox Road.

Strategy 2: Promote Market Strengths

There is market support for the development of housing (conventional multifamily and student housing), office (if supported by the university), retail, and hotel uses within the sector plan area. Proactive marketing of developable sites coupled with a commitment to assist the developer in the pre-development approval process can achieve results. This strategy seeks to capitalize on these opportunities by promoting the short-term redevelopment projects discussed later.

Strategy 3: Engage in Joint Development Partnerships

Implementing many of the sector plan’s land use concepts will require assistance from the public/quasi-public sector to overcome the constraints identified earlier. Public sector involvement in the financial side of development is encouraged to help analyze how public investment can overcome development barriers, and analyze options for using corridor-wide investment tools, especially a tax-increment financing district. Projects that may especially benefit from such assistance are those where:

- Major redevelopment of existing land uses is proposed on parcels in multiple ownership
- Lower-density residential development replaces commercial land uses (such as the areas south of Erie Street (Area 4d)
- Alternatives such as the consolidation of automotive sales and service uses as suggested in portions of the central gateway mixed-use area (Areas 4e and 4f)

Strategy 4: Support The University of Maryland’s Development Needs

The University of Maryland is the corridor’s economic engine. To a large extent, the economic success of the city and business community depends on the success of the university. To be competitive the university must offer an excellent education, be physically attractive to students, have strong research and development infrastructure, and have strong ties to new economy players. Therefore, the economic development strategy must be designed to support the university’s efforts to enhance its competitiveness. To do so requires the following:

- Increasing the amount and quality of student housing on and near campus
- Strengthening relationships between the university and business community
- Strengthening the role of the nonprofit College Park City University Partnership (CPCUP) in coordinating university and city policies and enhancing initiatives
- Improving infrastructure (parking, pedestrian facilities and bridges, etc.) to support the university

Strategy 5: Link the Route 1 Corridor to Neighborhoods, The University of Maryland Campus, Transit Districts and Regional Trail System

Physical linkages among and between activity centers are critical to establishing functional synergy between the community, campus, main street business areas, and the Metro station. These linkages must be

designed and executed in a manner that creates more than greenspace, but an amenity that adds value to neighboring properties. This strategy can be implemented by the following:

- Work with the Department of Parks and Recreation to enhance pedestrian trail connections between the Anacostia Tributary Trail System and areas adjacent to the stream valley park
- Encourage new development to provide enhancements and extensions to the existing network to improve access to the regional trail system

SHORT-TERM ECONOMIC DEVELOPMENT ACTION PROGRAM

The following short-term “program” actions, together with specific brick and mortar “project” actions should be taken by community stakeholders within two years of plan approval. Program actions relate to administrative and management activities that facilitate revitalization and redevelopment. Project actions relate to specific potential development opportunities.

Program Action 1: Broaden the role of the College Park City - University Partnership (CPCUP)

The function of the College Park City-University Partnership (CPCUP) should be broadened to manage implementation of the sector plan’s economic development strategy. The CPCUP should establish joint development priorities for the corridor, engage appropriate entities in joint development, be a steward for a given project through the approval process, manage special assessment district revenues, and be accountable for the successful implementation of the sector plan’s economic development strategy.

Program Action 2: Establish a development facilitation program

It is important that the community project a proactive attitude toward attracting private sector investment along the corridor. This can be accomplished by (1) clearly articulating

**Program Action 1
Related Strategies:**

- Support UMD
- Promote market strengths
- Use partnerships

Program Tasks:

1. Amend the CPCUP’s bylaws to incorporate new functions and broaden representation on the Board of Directors.
2. Appoint additional board members to include, but not necessarily be limited to, appropriate representatives from the County Redevelopment Authority and Parking Authority, and finance professionals.
3. Identify target projects and coordinate implementation with the city, university, the County Redevelopment and Parking Authorities, the Economic Development Corporation and other stakeholders.
4. Provide development facilitation services.
5. Market the corridor to retain and recruit retailers and small businesses and work with the Economic Development Corporation in marketing to larger businesses.

how the sector plan’s recommendations change the current regulatory and approval requirements; (2) improving the efficiency of the approval process; and (3) providing a development liaison to assist the private developer through the approval process.

**Program Action 2
Related Strategies:**

- Use partnerships
- Promote market strengths

Program Tasks:

1. Create a sector plan brochure summarizing land use objectives and regulatory changes.
2. Appoint a development facilitator to work with the public sector agencies on behalf of the private developers in order to expedite the commencement of private development projects

Program Action 3: Develop a parking strategy for the financing and development of structured parking in the town center and main street areas

Infill development cannot occur in these areas to the extent envisioned with parking provided on-site. A parking management strategy must be developed immediately to deal with the financing and development of structured parking in the town center area. In-lieu parking fees should be investigated. CPCUP could take a leadership role in managing this process with funding likely coming from the city, county and university. This action should also be coordinated with the programming of the East Campus project.

<p>Program Action 3 Related Strategies:</p> <ul style="list-style-type: none"> • Support UMD • Bolster mixed-use development • Promote market strengths • Use partnerships
<p>Program Tasks:</p> <ol style="list-style-type: none"> 1. Determine the boundaries for the parking management strategy. 2. Retain a parking consultant to evaluate current parking conditions, projected conditions, parking operations, boundaries, and alternative management strategies. 3. Work with property owners and other stakeholders to identify an effective and equitable parking management strategy.

Program Action 4: Establish long-term revenue sources

The CPCUP and other partners, including the county, should evaluate the feasibility of implementing some revenue generating management systems such as a tax increment financing district, special assessment district, and parking revenue systems discussed below. Long-term revenue sources will encourage investment by the private sector as funding will be available to sustain reinvestment back into the area to be used for maintenance, underwriting new development

projects, or other related district programs. It is recommended that the following actions be implemented within two years following sector plan approval.

Program Action 4.1: Establish a Tax Increment Financing (TIF) District for all or part of the sector plan area

There must be a source of revenue to support the joint development activities necessary to implement sector plan goals. A TIF district is an excellent long-term financing vehicle. The city and CPCUP would likely lead this effort.

The East Campus redevelopment project could be an excellent target project justifying the establishment of a TIF district. Currently, no property taxes are generated from this land because it is used by the university. Alternatively, private redevelopment will generate new property taxes. Also the university could consider payments in lieu of property taxes (PILOTs) to offset lost property tax revenue in instances where the university maintains ownership of the land.

<p>Program Action 4.1 Related Strategies:</p> <ul style="list-style-type: none"> • Support UMD • Promote market strengths
<p>Program Tasks:</p> <ol style="list-style-type: none"> 1. The university and city meets with county officials to discuss the establishment of a TIF district. 2. CPCUP retains an economic consultant to draft a plan and determine appropriate boundaries for a TIF district. 3. CPCUP and County Redevelopment Authority identify projects for TIF funding. 4. Simultaneous with developing an East Campus general development agreement (or other catalyst project), establish the TIF district.

Program Action 4.2: Evaluate the feasibility of an in-lieu parking fee program

Assuming public parking is developed in association with the East Campus mixed-use town center project or other projects in the

town center, an in-lieu parking fee program may be warranted. Such a program provides a long-term source of revenue to support parking while enhancing potential for infill development. The Development District Standards section establishes the basis for an in-lieu parking program.

**Program Action 4.2
Related Strategies:**

- Promote market strengths
- Use partnerships

Program Tasks:

1. Require the parking management strategy consultant to evaluate and recommend an in-lieu parking fee program.

Program Action 4.3: Evaluate the merits of establishing a special assessment district

A special assessment district such as a Commercial District Management Authority (CDMA) or a business improvement district (BID) may be supported if marketed with major redevelopment projects like the East Campus mixed-use and the “Knox Boxes” redevelopment in Area 1a. Property owners may be willing to accept additional property assessments when understood in the context of the improvements planned for these projects. Physical linkages

**Program Action 4.3
Related Strategies:**

- Promote market strengths

Program Tasks:

1. CPCUP or city meets with property owners to discuss the establishment of a special assessment district and potential benefits.
2. CPCUP or city retains an economic consultant to draft a specific plan and determine appropriate boundaries and tax rate for a special assessment district.
3. CPCUP and city identifies specific uses for assessment revenue.
4. Simultaneous with an East Campus general development agreement (or other catalyst project), establish the special assessment district.

or other infrastructure improvements may be necessary to interface these projects into the existing urban fabric. The city’s Planning Department would likely lead the research into such an effort.

The following section of recommendations identifies those brick and mortar “project” opportunities that are deemed possible in the short-term given current market conditions and availability of the properties for redevelopment.

Project Action 1: Pursue an office complex in Area 3: (Main Street Area)

The university is in need of additional office space in the vicinity of the campus. The main street area (Areas 3a and 3b) is within easy walking distance to the university campus. This area is envisioned as a mixed-use, pedestrian/bicycle-friendly environment. The development of office uses in this area can

**Project Action 1
Related Strategies:**

- Support UMD
- Promote market strengths
- Use partnerships

Program Tasks:

1. Obtain a commitment from the university to lease a significant amount of office space for an extended term.
2. Obtain a commitment in principle from the Redevelopment Authority to use its power of eminent domain to assemble an acceptable site.
3. Pursue funding for pedestrian/bicycle connections between the office complex project, through the Paint Branch Stream Valley, to the campus area.
4. CPCUP issues a developer request for proposal (RFP) to implement the office complex and proposes a phasing plan and joint development arrangement.
5. Select a developer and negotiate a general development agreement.
6. Obtain funding to construct bridges linking the main street area to the campus via the Paint Branch Stream Valley.

satisfy both the plan’s objectives and a near-term university need.

The barriers to office development in these locations are significant: there are existing land uses on target properties some of which will be difficult to re-locate in an office building; most of the parcels on the western side of US 1, in the main street area (Area 3a), are under separate ownership; rents preclude structured parking; and target properties are in the floodplain. To implement this project will likely require public/quasi-public sector participation in land assembly, land cost write-downs, and infrastructure development. The CPCUP could take the lead role in project planning.

A comprehensive, phased planning approach is recommended to maximize the redevelopment potential of the land and generate maximum benefit to the university, the main street area and the tax base. A comprehensively planned joint development project achieves economies from early land assembly, an efficient road network and centralized parking and amenities. As envisioned, 240,000 square feet of office space, (including the existing 42,000 square foot former bank building) and another 30,000 square feet of ground floor retail (including existing space) could be ultimately developed on land west of US 1 in Area 3a, provided structured parking is built by the public/quasi-public sector. To achieve this level of building intensity will require that the land area be held in single ownership.

Project Action 2: Work with the bowling alley site owner to implement redevelopment

The proactive marketing of immediately developable sites coupled with a commitment to assist the developer in the pre-development approval process can achieve immediate results. There is a three-acre site just south of University Boulevard in single ownership that is developed with a bowling alley and Pizza Hut restaurant. Assuming this property is available in the market, it could be developed for residential and/or mixed uses. Because of its size, access, and proxim-

ity to the university campus and Beltway, it is well positioned for multifamily residences with a limited amount of retail to support the project. The existing, on-site Pizza Hut could be relocated into the project. The CPCUP could take the lead in this joint development effort.

<p>Project Action 2 Related Strategies:</p> <ul style="list-style-type: none">• Promote market strengths• Use partnerships
<p>Project Tasks:</p> <ol style="list-style-type: none">1. CPCUP meets with property owners to discuss (1) the site’s redevelopment potential in light of sector plan zoning changes and (2) the owner’s investment objectives.2. CPCUP works with local brokers, the city, and others to market the property to developers.3. CPCUP works with the developer to facilitate the development approval process.

Project Action 3: Work with the owner of the old gas station site at Hartwick Road and US 1 to implement redevelopment

The sector plan’s changes in development regulations and parking requirements should enhance the development potential of small sites in the town center. The old gas station located in the northeast corner of US 1 and Hartwick Road is such a site. It comprises approximately 17,000 square feet. The site can be developed for retail on the ground floor with apartment units above and/or incorporated into a mixed-use project for the entire block. This project may not require a subsidy and could be led by CPCUP. Also, opportunities may exist for shared parking as other adjacent properties are assembled or redeveloped.

**Project Action 3
Related Strategies:**

- Bolster mixed-use development
- Promote market strengths
- Use partnerships

Project Tasks:

1. CPCUP meets with property owners to discuss (1) the site’s redevelopment potential in light of sector plan zoning changes (2) environmental mitigation issues and (3) the owner’s investment objectives.
2. CPCUP works with local brokers, the city, and others to market the property to developers.
3. CPCUP works with the developer to facilitate the development approval process.

Project Action 4: Program the East Campus project and solicit developer interest

In the near-term, the East Campus mixed-use town center redevelopment project should be initiated in the southeast corner of Paint Branch Parkway and US 1. This gateway project will have a catalytic effect on the existing town center area, the main street area, and Paint Branch Parkway because of its high visibility. The development strategy must clearly articulate the timing of development, the incentives necessary to attract the level of desired investment, and the role and responsibility of various public, quasi-public and private entities. The property is recommended for rezoning by the sectional map amendment to implement the plan’s mixed-use recommendations and to provide an early incentive for development by removing the rezoning hurdle.

The university should take the lead in this initiative as the owner of all the East Campus land. The university will, however, have to work closely with CPCUP because capital incentives will likely be necessary to support this project. The roles and responsibilities of various entities and the sources of pub-

lic/quasi-public financing should be coordinated in the short-term, before a request for qualifications or a request for proposals is issued soliciting developer interest.

**Project Action 4
Related Strategies:**

- Support UMD
- Bolster mixed-use development
- Promote market strengths
- Use partnerships

Project Tasks:

1. Conduct a university-sponsored feasibility study to determine a realistic development program, financial feasibility, and joint development requirements.
2. Craft a joint development strategy enlisting the necessary stakeholders (the university, the city, the County’s Redevelopment and Parking Authorities, the Economic Development Corporation, the county, and state).
3. Determine how to finance structured parking.
4. Issue a developer request for proposal (RFP) with the issuing agency determined as part of Task 2 above.
5. Apply for available pedestrian and bicycle program funding to develop pedestrian and bike amenities linking the East Campus project to the Metro, the traditional town center, the main street area, and the university campus.

MID-TERM ECONOMIC DEVELOPMENT ACTION PROGRAM

The mid-term program action and two project actions that follow have been identified for possible implementation within three to seven years following sector plan approval.

Program Action 5: Leverage excess State Highway Administration (SHA) land to enhance redevelopment potential of land south of the existing Jiffy Lube property (Area 4a)

There is a market for a variety of land uses in the corridor today, but a chief barrier to development is land availability. US 1’s reconstruction will require that a number of parcels be acquired via eminent domain for highway construction purposes. Residual land will likely be available after the highway improvements are complete. CPCUP and/or the city should seek acquisition of these parcels and use them to leverage desired development.

There are currently two boarded-up buildings and an additional vacant parcel of land north of Ruatan Street on the western side of US 1 in Area 4a. These properties are obvious candidates for redevelopment except that environmental constraints are a limitation that must be addressed. The rear one-third of the boarded-up properties’ land is undevelopable because of steep slope limitations.

There is a Jiffy Lube immediately north of these abandoned buildings. The Jiffy Lube parcel potentially could be taken by the SHA to implement the US 1 improvements. The portion of the site remaining after US 1 improvements could be used to enhance the development potential of the under-utilized land to the south.

<p>Program Action 5 Related Strategies:</p> <ul style="list-style-type: none"> • Use partnerships
<p>Project Tasks:</p> <ol style="list-style-type: none"> 1. CPCUP purchases SHA excess land to enhance development potential of targeted properties. 2. CPCUP provides land for redevelopment.

Project Action 5: Program and implement redevelopment of the “Knox Box” area

Redevelopment of the apartments in the Knox Box section of the town center (Area 1a) is considered a mid-term project because of its complexity, not because of market demand. The concept for this area envisions a transformation in density from small fragmented development to moderate- to high-density coordinated development. As envisioned, the Knox Box area contains a concentration of student housing as well as supporting commercial land uses.

While there is market potential to implement such a concept it will be difficult to implement because of the number of property owners involved and the value of the land in this area. Land assembly will be required for the project to be successful. The Redevelopment Authority or city should support land assembly in proximity to the university. The lack of assistance with land assembly issues could make redevelopment impossible. After property is

<p>Project Action 5 Related Strategies:</p> <ul style="list-style-type: none"> • Support UMD • Bolster mixed-use development • Promote market strengths • Use partnerships
<p>Project Tasks:</p> <ol style="list-style-type: none"> 1. Conduct a CPCUP-sponsored feasibility study to determine a realistic development program, financial feasibility, and joint development requirements. 2. Craft a joint development strategy enlisting the necessary stakeholders (the university, city, the County’s Redevelopment and Parking Authorities, the County Economic Development Corporation, the county, and state). 3. Determine how to finance structured parking. 4. Issue a developer request for proposal (RFP) with the issuing agency determined as part of Project Task 2 above.

assembled, appropriate mixed-use zoning should be sought under the Development District Overlay Zone provisions.

Project Action 6: Market and facilitate a comprehensively planned residential development south of Cherry Hill Road

One mid-term development site in Area 5b includes the land south of Cherry Hill Road, across from the College Park Marketplace, and west of Autoville Drive North, including a narrow strip along the east side. The sector plan recommends comprehensively planned single-family and multifamily development in this area. This property’s location near the Capital Beltway makes it an attractive development opportunity for the private sector to pursue.

<p>Project Action 6 Related Strategies:</p> <ul style="list-style-type: none"> • Promote market strengths • Create pedestrian linkages • Use partnerships
<p>Project Tasks:</p> <ol style="list-style-type: none"> 1. CPCUP works with brokers, the city, and others to market opportunities to developers. 2. CPCUP works with developers to facilitate the development approval process.

LONG-TERM ECONOMIC DEVELOPMENT ACTIONS

The following long-term actions are contingent upon the results of actions taken in the short and mid-term.

Project Action 7: Engage in joint development to realize goals

Achieving the plan’s long-term objectives for redeveloping the US 1 corridor will require public investment in partnership with the private sector. Potential special assessment district and tax increment financing district revenues should be leveraged with resources from the city, county, and state to provide the

level of public incentives necessary to attract private sector reinvestment and redevelopment. Once such programs are established and development partnerships are created, possible long-term development opportunities could include development of townhouse residential along US 1 (Areas 4a, b, d, e, and 5b) ; the redevelopment of the existing shopping center at the corner of Hartwick Road and US 1 (Area 1b); and the development of an autopark in portions of Areas 4e and 4f.

The development of townhouses along US 1 will require that CPCUP or others assemble land and convey it to private developers at a significantly discounted cost. The development of an autopark will require that land be assembled by the county’s Redevelopment Authority or the city for auto-related uses. The land would then be conveyed on a lease basis to auto-related businesses. The implementation of the autopark concept will require patient equity. To implement the redevelopment of the shopping center will require significant capital investment in structured parking on the part of the public/quasi-public sector.



Implementation



SECTIONAL MAP AMENDMENT

INTRODUCTION

The comprehensive rezoning process, also known as the sectional map amendment process in Prince George's County, allows for the rezoning of a section of the overall county Zoning Map in order to bring zoning into conformance with adopted county plans and policies. This chapter contains the approved sectional map amendment (SMA) for the College Park US 1 Corridor Sector Plan. The SMA is intended to implement the land use recommendations of the approved sector plan for the foreseeable future.

The SMA was initiated via Council Resolution CR-37-2000, with the expressed intent to process the SMA concurrent with the sector plan. The procedure followed is in accordance with Council Bill CB-33-1992, which establishes the framework for the process. The SMA formally incorporates the zoning recommendations as an amendment to the official Zoning Maps. Existing zoning which hinders such development will be corrected, and piecemeal rezonings will be minimized by this comprehensive approach. The procedural sequence chart for this concurrent process is illustrated in Appendix B.

The approval of the zoning pattern recommended by the sector plan and implemented by this SMA brings zoning into greater conformity with county land use goals and policies as they apply to the College Park US 1 Corridor, thereby enhancing the health, safety and general welfare of all Prince George's County citizens.

The County's Capital Improvement Program and Ten-Year Water and Sewerage Plan, as well as existing land use and zoning, and pending zoning applications were examined and evaluated in the preparation of both the land use plan and SMA. Consideration has also been given to the environmental and economic impact of the land use and zoning recommendations. The approval of the SMA results in the revision of the official 1"= 200' Zoning Map(s) for this portion of Planning Area 66 that includes the sector plan bound-

ary. Future comprehensive examinations of the zoning within the area will occur in accordance with the procedures established for sectional map amendments.

The College Park US 1 Corridor within this sector plan was adopted into the Maryland-Washington Regional District on November 29, 1949. The last comprehensive rezoning of this area took place on May 1, 1990, with the approval of the SMA for Langley Park, College Park, Greenbelt and Vicinity (Planning Areas 65, 66 and 67) by Council Resolution CR-39-1990.

COMPREHENSIVE REZONING IMPLEMENTATION POLICIES

A number of established comprehensive rezoning implementation policies are utilized as necessary guidelines for developing the SMA.

PUBLIC LAND POLICY

The established public land policy states that all public land should be placed in the most restrictive and/or dominant adjacent zone, whichever bears the closest relationship to the intended character of the area. Therefore the zoning of public land, just as private land, should be compatible with surrounding zones. This policy should eliminate any "islands" of inharmonious zoning, while still providing for the public use. It should further assure compatibility of any future development or uses if the property is returned to private ownership.

A distinction is made where a large parcel of land has been set aside specifically for public open space. In this case the R-O-S Zone is applied.

Federal and state government property, which is scattered throughout the county, is not subject to the requirements of the Zoning Ordinance. The intent of the comprehensive rezoning process is to apply a zoning category to all land, including federal and state property, without regard to its unique zoning status. The R-O-S Zone is generally applied

to federal and state properties, unless specific uses of the property or intended character of the property and/or area should warrant another zoning category. The University of Maryland owns a significant amount of land within the sector plan boundary. The base zone for The University of Maryland is the R-R Zone. Some university properties have been retained in the R-R base zone. However, the M-U-I Zone is approved for all university properties east of US 1 and south of Paint Branch Parkway in an effort to implement the vision for the East Campus area.

ZONING IN PUBLIC RIGHTS-OF-WAY

Policies governing the zoning of public street and railroad rights-of-way (both existing and proposed) are contained in Section 27-111 of the Prince George's County Zoning Ordinance. This SMA has been prepared in accordance with this section.

LIMITATIONS ON THE USE OF ZONES

Appendix A is the "Guide to Zoning Categories" used in Prince George's County. Zoning classifications used in this SMA are limited only by the range of zones within the Ordinance available at the time of final action by the District Council. However, there are certain restrictions on when these may be applied to properties (Section 27-223 of the Zoning Ordinance).

Reclassification of an existing zone to a less intense zone is prohibited where:

(g)(1) "The property has been rezoned by Zoning Map Amendment within five (5) years prior to the initiation of the Sectional Map Amendment or during the period between initiation and transmittal to the District Council, and the property owner has not consented in writing to such rezoning;" or

(g)(2) "Based on existing physical development at the time of adoption of the Sectional Map Amendment, the rezoning would create a nonconforming use. This rezoning may be approved, however, if there is a significant public benefit to be served by the rezoning based on facts pe-

culiar to the subject property and the immediate neighborhood. In recommending the rezoning, the Planning Board shall identify these properties and provide written justification supporting the rezoning at the time of transmittal. The failure of either the Planning Board or property owner to identify these properties, or a failure of the Planning Board to provide the written justification, shall not invalidate any Council action in the approval of the Sectional Map Amendment."

Finally, in order to clarify the extent to which a given parcel of land is protected from less intensive rezoning by virtue of physical development, the Zoning Ordinance states in Section 27-223(h) that:

"The area of the 'property,' as the word is used in Subsection (g)(2), above, is the minimum required by the Zoning Ordinance which makes the use legally existing when the Sectional Map Amendment is approved."

GUIDELINES FOR COMMERCIAL ZONING

The comprehensive rezoning proposal will recommend the most appropriate of the "use-oriented" commercial zones listed in the Prince George's County Zoning Ordinance. The choice of zone is determined by the commercial needs of the area, the sector plan recommendations, and the type of use and status of the development on the property and surrounding area.

Existing C-1, C-C, C-G, C-H and C-2 Zones will be converted to the new "use-oriented" commercial zones in accordance with the commercial rezoning policies endorsed by the Planning Board and the County Council in previously adopted SMAs. Exceptions will be made where: (1) the old commercial zone has conditions attached to it that should be brought forward in the SMA; and/or (2) because of previous zoning decisions, development or the existing character of the area, commercial zoning in the new "use-oriented" zones is not considered appropriate and/or (3) the property is recommended for mixed-use, in which case an appropriate mixed-use zone may be applied. In these

circumstances, the existing commercial zone (with the zoning application number) will be placed on the new Zoning Map as a specific reference for future development or subsequent rezoning actions on the site.

CONDITIONAL ZONING

The inclusion of safeguards, requirements, and conditions beyond the normal provisions of the Zoning Ordinance which can be attached to individual zoning map amendments via “Conditional Zoning” cannot be utilized in SMAs. In the piecemeal rezoning process, conditions are used to: (1) protect surrounding properties from potential adverse effects which might accrue from a specific zoning map amendment; and/or (2) to enhance coordinated, harmonious, and systematic development of the Regional District. When approved by the District Council, and accepted by the zoning applicant, “conditions” become part of the County Zoning Map requirements applicable to a specific property and are as binding as any provision of the County Zoning Ordinance [see Conditional Zoning Procedures, Section 27-157(b)].

In theory, zoning actions taken as part of the comprehensive rezoning (SMA) process should be compatible with other land uses without the use of conditions. However, it is not the intent of an SMA to repeal the additional requirements determined via “conditional” zoning cases that have been approved prior to the initiation of a sectional map amendment. As such, it is appropriate that, when special conditions to development of specific properties have been publicly agreed upon and have become part of the existing Zoning Map applicable to the site, those same conditions shall be brought forward in the SMA. This is accomplished by continuing the approved zoning with “conditions” and showing the zoning application number on the newly adopted Zoning Map. This would take place only when it is found that the existing zoning is compatible with the intended zoning pattern or when Ordinance limitations preclude a rezoning. Similarly, findings contained in previously approved SMAs shall be brought forward in the SMA

where the previous zoning category has been maintained. There are no conditionally zoned properties within the sector plan boundary.

COMPREHENSIVE DESIGN ZONES

Comprehensive Design Zones (CDZ) may be included in a sectional map amendment. However, the flexible nature of these zones requires a Basic Plan of development to be submitted through the zoning application process (Zoning Map Amendment) in order to evaluate the comprehensive design proposal. It is only through approval of a Basic Plan, which identifies land use types, quantities, and relationships, that a CDZ can be recognized. Therefore, an application must be filed, including a Basic Plan; and the Planning Board must have considered and made a recommendation on the zoning application in order for the CDZ to be included within the SMA. During the comprehensive rezoning, prior to the submission of such proposals, property must be classified in a conventional zone that provides an appropriate “base density” for development. In theory, the “base density” zone allows for an acceptable level of alternative development should the owner choose not to pursue full development potential indicated by the sector plan. [See Section 27-223(b); Section 27-225(b)(1); Section 27-226(a)(2); and Section 27-226(f)(4).] There are no pending CDZs within the sector plan area.

COMPREHENSIVE REZONING CHANGES

To implement the sector plan’s policies and land use recommendations contained in the preceding chapters, many parcels of land must be rezoned to bring the zoning into conformance with the sector plan. The comprehensive rezoning process (via the SMA) provides the most appropriate mechanism for the public sector to achieve this. As such, the SMA is approved as an amendment to the official Zoning Map(s) concurrently with sector plan approval.

The approved SMA includes 62 zoning changes which include a Development District Overlay Zone (DDOZ). The DDOZ is

superimposed over the College Park US 1 Corridor Sector Plan area to ensure that the development of land meets the goals established in the sector plan. (See Map 14.) The DDOZ development standards are more particularly described in the College Park US 1 Corridor Development District Standards section of the plan document.

The SMA is organized using the sector plan's subareas identified on Map 15. The 62 zoning changes result in a new zoning inventory for the area (Table 10). Table 11 lists the aggregate changes in zoning by zone classification and indicates the acreage comprising each zoning change. Specific zoning changes are shown on individual subarea page-size maps and are described in Table 12. Also, these maps and the approved SMA zoning map (Map 16) are included for illustrative purposes only. The 1"=200' scale zoning maps will represent the official zoning boundaries. There were no zoning applications pending during the preparation of this SMA.

APPLICATION OF THE MIXED-USE INFILL (M-U-I) ZONE

The Mixed-Use Infill (M-U-I) Zone has been developed to implement the land use recommendations of the sector plan. Its primary purpose is to encourage residential, commercial and mixed-residential and commercial development in established communities. Property in a DDOZ area may be reclassified from its underlying zone to the M-U-I Zone as part of the SMA or through the property owner application process (Section 27-548.26(b)) of the Zoning Ordinance. The uses permitted in the M-U-I Zone are the same as those permitted by right or by special exception in the Commercial Shopping Center (C-S-C) Zone. However, for use category (3) Miscellaneous and use category (6) Residential/Lodging, the uses allowed are those permitted in the medium-residential density R-18 Zone, except that hotel/motel uses are permitted in the C-S-C Zone. Also, the College Park US 1 Corridor Sector Plan's Development District Overlay Zone (DDOZ) further amends the range of uses permitted in the M-U-I Zone to those uses listed as permitted and prohibited

in Tables 17 and 18 of the Development District Standards section.

**Table 10
Approved Zoning Inventory
College Park US 1 Corridor
(acres)**

Zone	Prior Zoning	Net Change	Approved Zoning (per SMA)
M-U-I	0.0	+241.2	241.2
C-S-C	118.3	-90.8	27.5
C-2	11.7	-11.7	0.0
C-O	44.6	-34.5	10.1
R-R	74.0	-54.4	19.6
R-55	68.0	-43.3	24.7
R-T	3.9	0.0	3.9
R-10	4.6	0.0	4.6
R-18	20.5	-1.9	18.6
O-S	8.6	-4.6	4.0
R-O-S	21.9	0.0	21.9
Subtotal	376.1	0.0	376.1
Rights-of-Way	65.7	0.0	65.7
Total	441.8	0.0	441.8

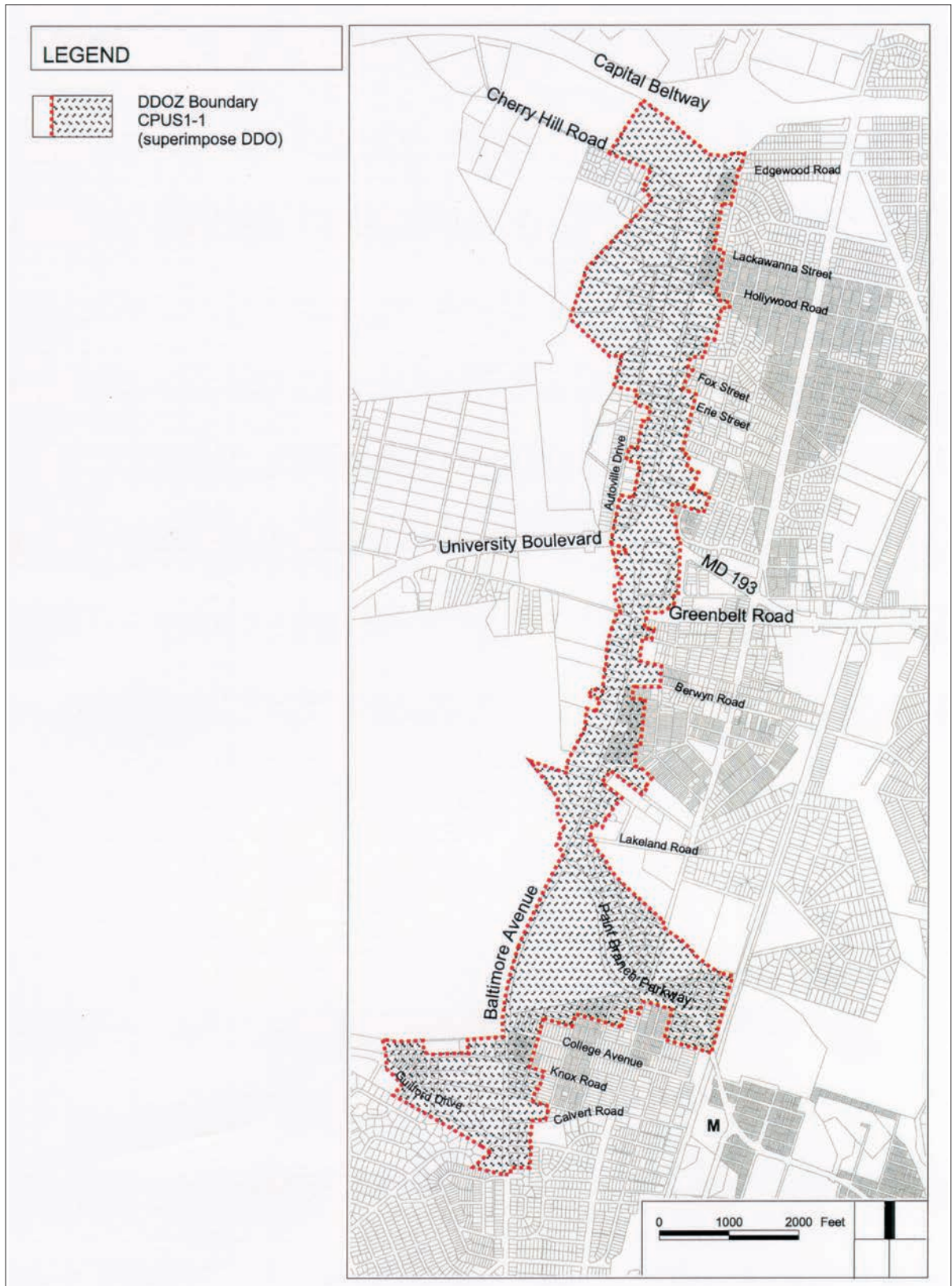
Source: M-NCPPC, April 2002.

Note: All acreage is approximate.

**Table 11
Aggregate Inventory of Zoning Changes
Part of Planning Area 66**

O-S to R-R	1.28		C-S-C to M-U-I	.24
Subtotal	1.28			.41
O-S to M-U-I	.45			7.66
	2.88			3.51
Subtotal	3.33			2.16
R-R to M-U-I	44.33			.86
	9.69			1.91
	.41			1.88
Subtotal	54.43			.08
R-55 to M-U-I	.60			1.12
	.28			.60
	5.48			.12
	4.17			1.03
	.65			1.31
	.10			6.31
	.63			3.00
	.25			.85
	.84			1.42
	1.91			2.86
	28.41			.44
Subtotal	43.32			2.05
R-18 to M-U-I	1.93			1.71
Subtotal	1.93			.61
C-2 to R-R	1.03			.38
Subtotal	1.03			3.95
C-2 to M-U-I	.86			4.50
	2.00			3.01
	7.83			.87
Subtotal	10.69			.42
				.55

C-O to M-U-I	1.80	C-S-C to M-U-I (Continued)	8.45
	3.47		1.36
	.91		.48
	.78		5.99
	1.36		1.37
	1.68		.97
	1.53		3.43
	.47		1.40
	22.50		1.19
Subtotal	34.50		.78
			6.83
			1.31
			1.39
		Subtotal	90.77
Total Approved Zoning Changes			241.28



DDOZ Boundary

Map 14

LEGEND

. Sector Plan Boundary

Area 1:

- 1a. West of Downtown
- 1b. West side of US 1
- 1c. East side of US 1
- 1d. South of Paint Branch Parkway
- 1e. Metro-related area

Area 2:

- 2a. West side of US 1
- 2b. North of Paint Branch Parkway

Area 3:

- 3a. West side of US 1
- 3b. East side of US 1

Area 4:

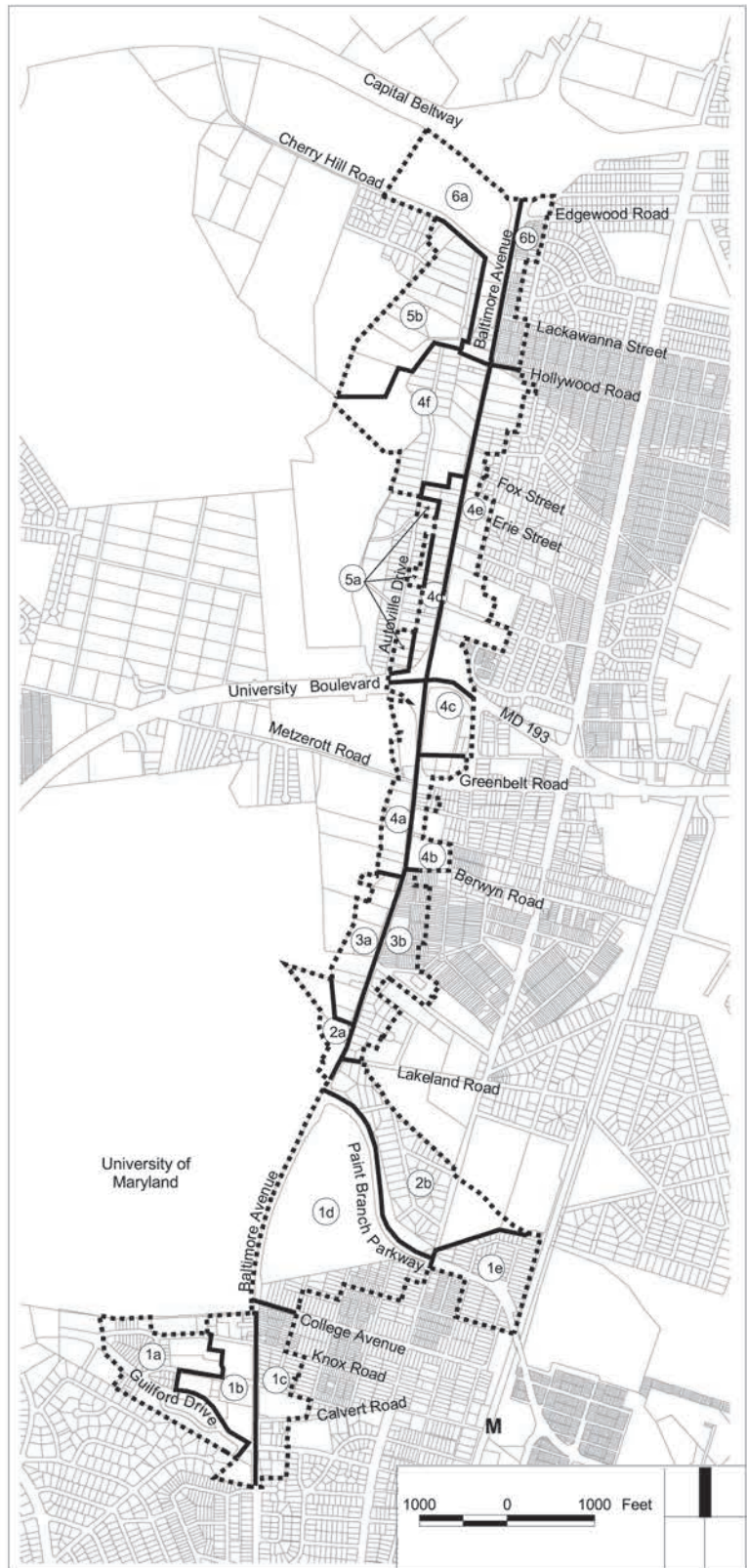
- 4a. West side of US 1, south of MD 193
- 4b. East side of US 1, south of Branchville Road
- 4c. SE corner of US 1/MD 193
- 4d. West side of US 1, north of MD 193
- 4e. East side of US 1, north of MD 193
- 4f. West side, between Fox Street & Hollywood Road

Area 5:

- 5a. Autoville Drive - South
- 5b. Autoville Drive - North

Area 6:

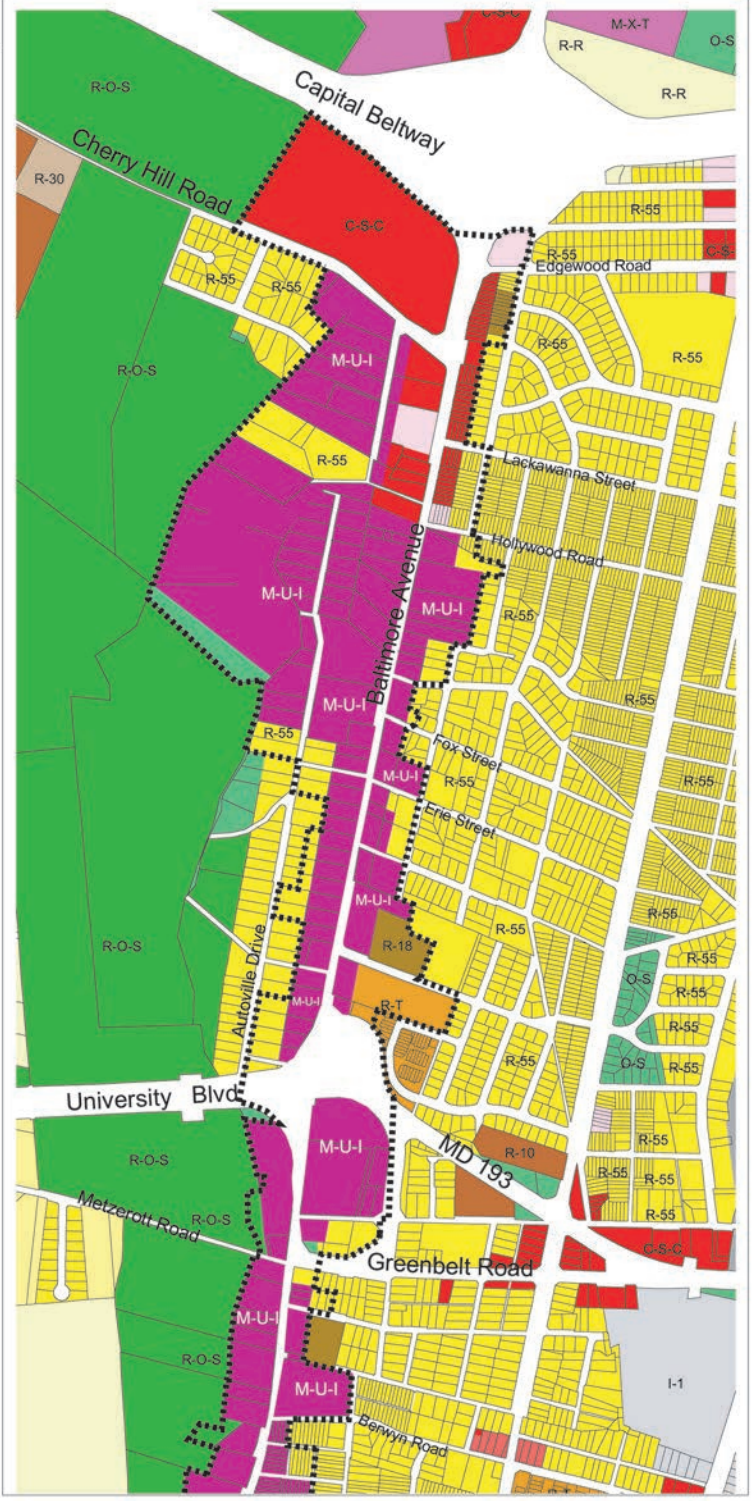
- 6a. West side of US 1
- 6b. East side of US 1



Areas and Subareas

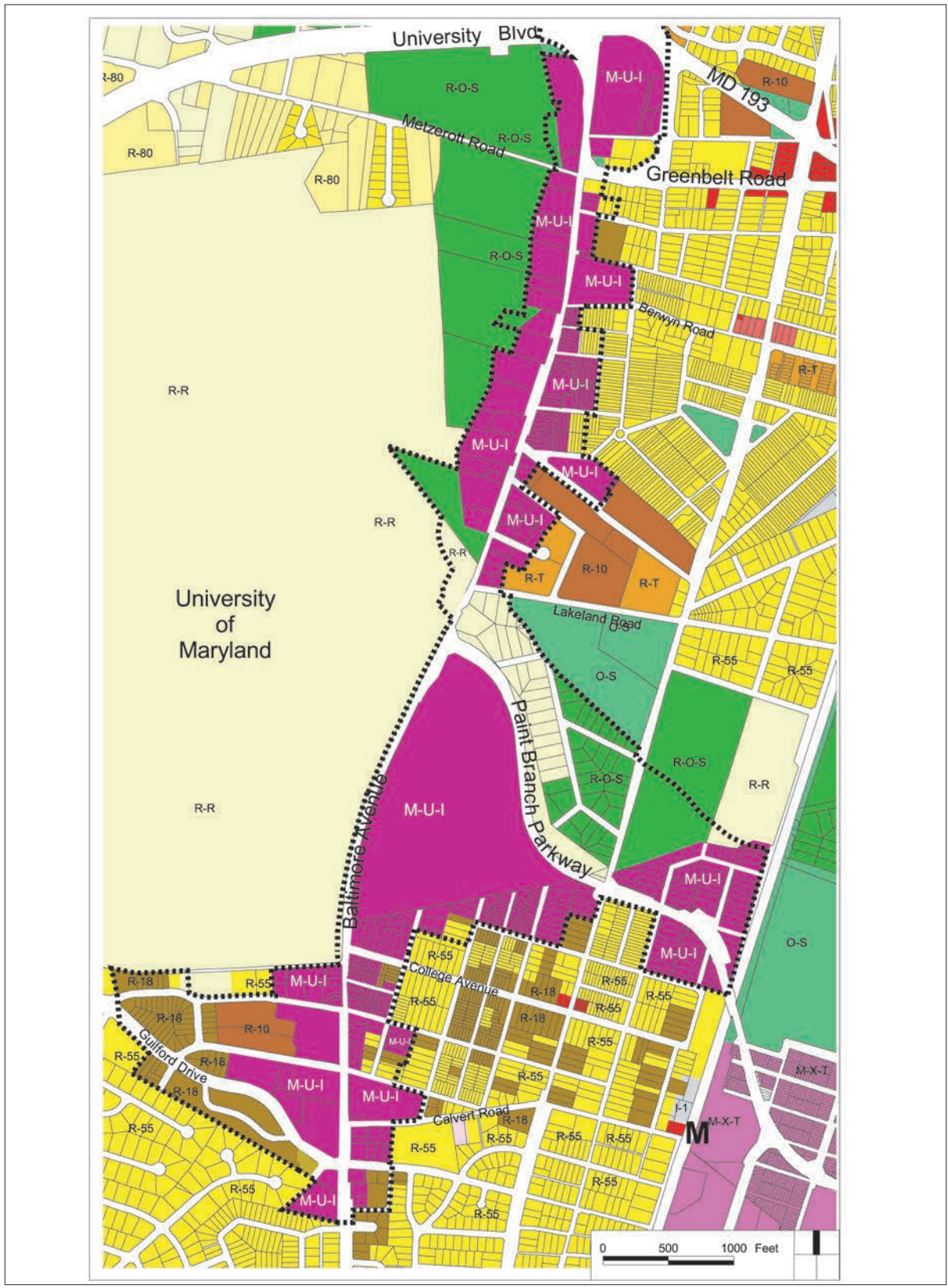
Map 15

LEGEND	
.....	Sector Plan Boundary
	M-U-I: 241.2 acres
	C-O: 10.1 acres
	C-S-C: 27.5 acres
	O-S: 4.0 acres
	R-18: 18.6 acres
	R-55: 24.7 acres
	R-R: 19.6 acres
	R-T: 3.9 acres
	R-10: 4.6 acres
	R-O-S: 21.9 acres



Approved Zoning–North

Map 16a



Approved Zoning—South

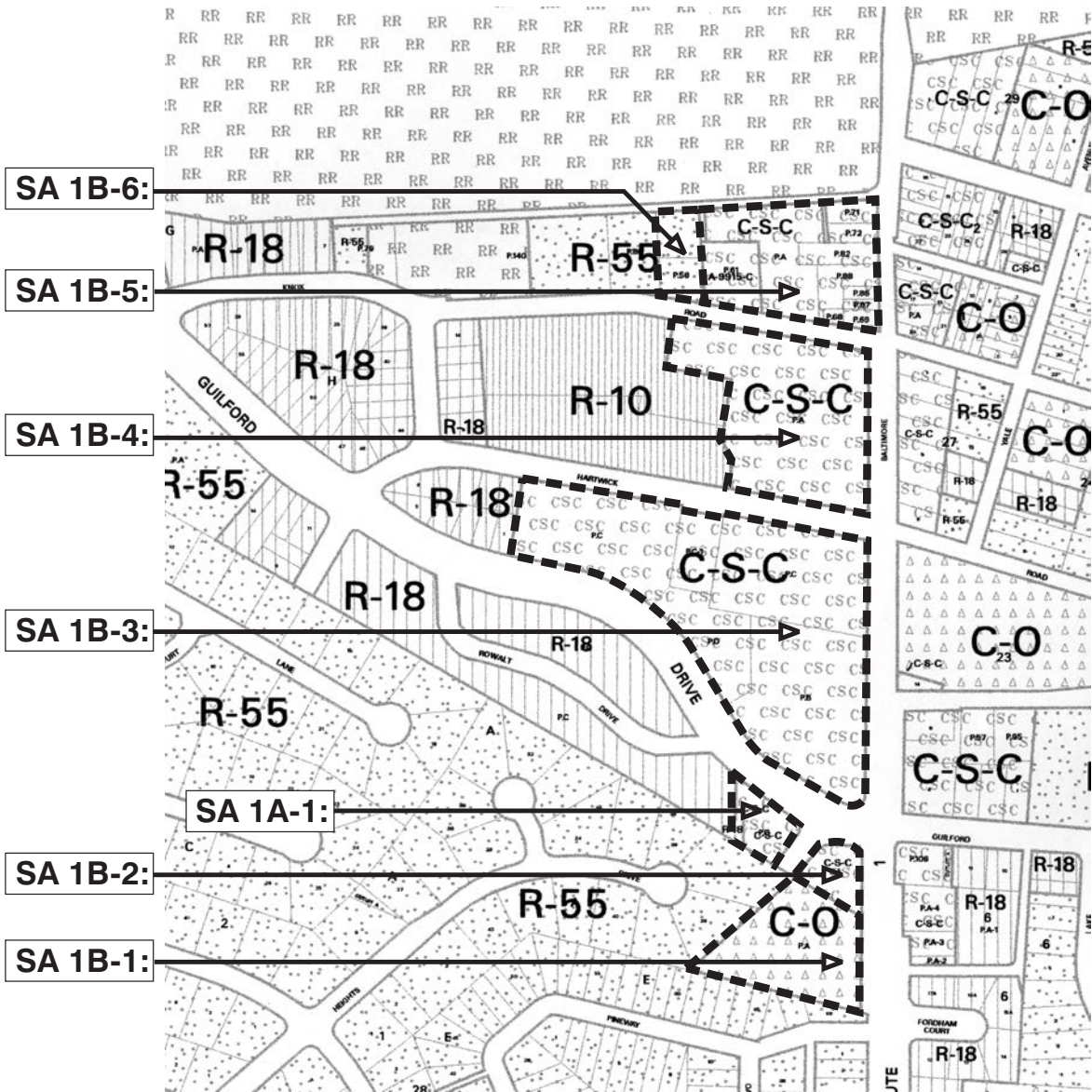
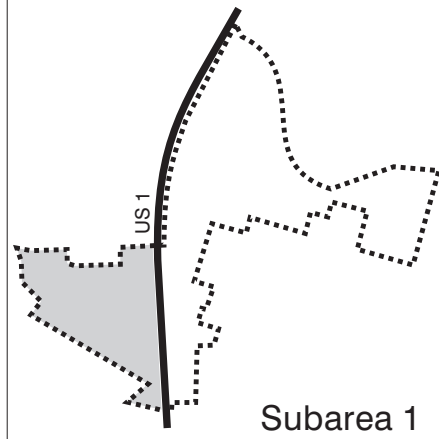
Map 16b

Subarea 1A (SA 1A)

SA 1A-1: C-S-C to R-18

Subarea 1B (SA 1B)

- SA 1B-1: C-O to M-U-I**
- SA 1B-2: C-S-C to M-U-I**
- SA 1B-3: C-S-C to M-U-I**
- SA 1B-4: C-S-C to M-U-I**
- SA 1B-5: C-S-C to M-U-I**
- SA 1B-6: R-55 to M-U-I**



Approved Zoning Changes



**Table 12
Approved Zoning Changes
US 1 Corridor Sector Plan**

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
CP US1-1	Superimposed DDOZ	443 ac.	SMA	1990	208 NE 4L, 4R 209 NE 4L, 4R 210 NE 4L, 4R 211 NE 4L, 4R 212 NE 4L, 4R

Use and Location:

All property within the College Park US 1 Corridor Sector Plan boundary from the Capital Beltway to Fordham Court. For boundary description see the resolution of initiation of the SMA (CR-37-2000) and accompanying map. Also see Map 15.

Discussion:

The Development District Overlay Zone (DDOZ) is proposed to ensure that development of land along the corridor meets the design goals established in the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1A-1	C-S-C to R-18	.24 ac.	SMA	1990	209NE 4L

Use and Location:

Parking for the graduate apartment housing located south of Guilford Drive and west of the stub street (Oakridge Drive) (TM 33, Grid C-4, College Park Subdivision, part of Parcel C, SDAT 1160)

Discussion:

This property is owned by The University of Maryland and is used as a parking lot for graduate students living in the adjacent R-18-zoned "Graduate Apartments" complex along Guilford Drive.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1B-1	C-O to C-S-C	1.80 ac.	SMA	1990	209NE 4L

Use and Location:

Office building located at 7100 Baltimore Avenue. (TM 33, Grid C-4, College Heights Subdivision, SDAT A-4774, Block E, Parcel A)

Discussion:

This use exists in an area determined to be appropriate for development as a mixed-use town center. Rezoning to M-U-1 will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1B-2	C-S-C to M-U-I	.41 ac.	SMA SE-3284 (gas station)	1990 4/7/82	209NE 4L

Use and Location:

Exxon station located at 7110 Baltimore Avenue in the southwest quadrant of the intersection of Baltimore Avenue with Guilford Avenue. (TM 33, Grid C-4, Lord Calvert Subdivision of College Park, Parcel A, SDAT 1160)

Discussion:

This use exists in an area determined to be appropriate for development as a mixed-use-town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1B-3	C-S-C to M-U-I	7.66 ac.	SMA	1990	209NE 4L

Use and Location:

Plato's diner, Quality Inn, Applebees and Kinkos, known as 7150-7250 Baltimore Avenue, respectively; and 4401, 4417 and 4321 Hartwick Road. (TM 33, Grid C-4; SDAT 4821, Plat 164066, Parcel C; SDAT 5370, Plat A, 4449, Parcel C-1, College Park Subdivision, SDAT 4880, Plat A-1160, Parcels B and D; College Park Shopping Center, SDAT 3500, Plat A-1910, part of Parcel A; and SDAT 2961, part of Parcel A)

Discussion:

This mix of uses exists in an area determined to be appropriate for development as a mixed-use-town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1B-4	C-S-C to M-U-I	3.51 ac.	SMA SE-487 (sign)	1990 5/20/59	209NE 4L

Use and Location:

College Park Shopping Center including addresses 7300-7350 Baltimore Avenue and Bank of America at 7370 Baltimore Avenue. (TM 33, Grid C-4, College Park Shopping Center Subdivision, Plat A-1910, part of Parcel A, SDAT 1703)

Discussion:

This mix of uses exists in an area determined to be appropriate for development as a mixed-use-town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1B-5	C-S-C to M-U-I	2.16 ac.	SMA	1990	209NE 4L

Use and Location:

A compact mix of commercial uses located at 4340-70 and 4404-12 Knox Road and 7400-7422 Baltimore Avenue, containing Pasta Amore, barber, Today's Hair, tanning salon, Record Exchange, Deli, Rice Bowl, Mailboxes etc., Santa Fe Café, 7-Eleven, China Café, bicycles, Bargains Unlimited, Terp Station, Rerun Apparel, Chevy Chase bank, University Greek Shop and Ratzies, located in the northwest quadrant of the intersection of Baltimore Avenue and Knox Road. (TM 33, Grids C-4 and C-3, College Park Subdivision, Parcels 88, 82, 72, 71, 61, 68, 6, 81, 86, 87, 69, SDAT 4448, part of Parcels 2 and 3)

Discussion:

This mix of uses exists in an area determined to be appropriate for development as a mixed-use-town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1B-6	R-55 to M-U-I	.60 ac.	SMA	1990	209NE 4L

Location and Use:

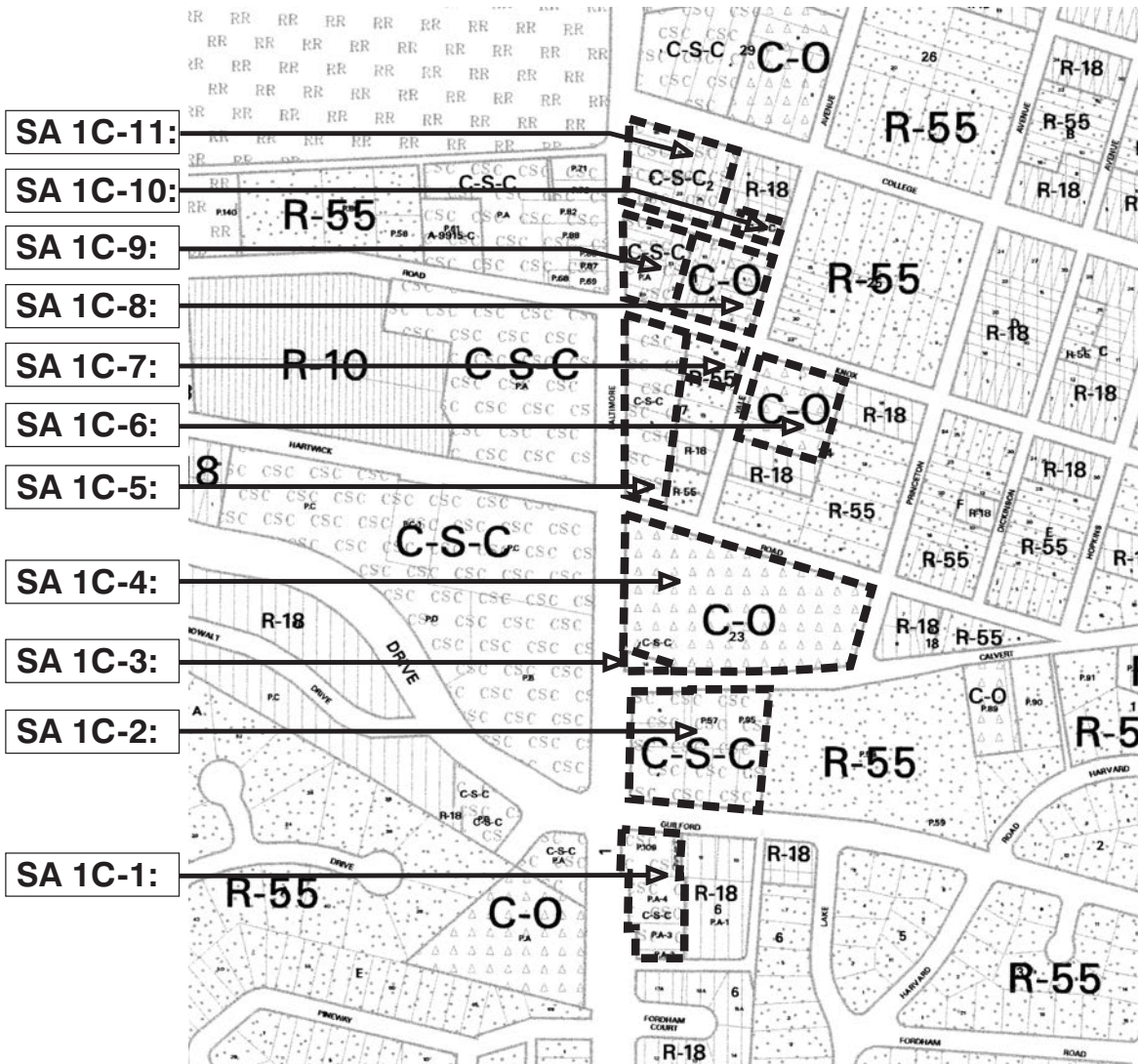
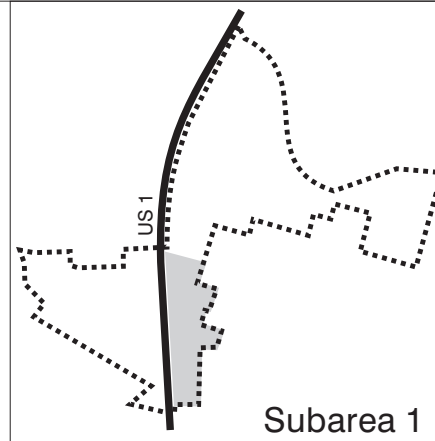
These lots are part of The University of Maryland's new 1,031-bed South Campus Commons student apartment complex along the north side of Knox Road. (TM 33, Grids C-3 and C-4, Parcels 1 and 58)

Discussion:

This property is being developed as part of the university's South Campus Commons apartment complex. Rezoning to the M-U-I Zone provides the most compatible zone for the new use which is compatible with the mixed-use zoning recommended on adjacent property.

Subarea 1C (SA 1C)

- SA 1C-1: C-S-C to M-U-I
- SA 1C-2: C-S-C to M-U-I
- SA 1C-3: C-S-C to M-U-I
- SA 1C-4: C-O to M-U-I
- SA 1C-5: C-S-C to M-U-I
- SA 1C-6: C-O to M-U-I
- SA 1C-7: R-55 to M-U-I
- SA 1C-8: C-O to M-U-I
- SA 1C-9: C-S-C to M-U-I
- SA 1C-10: C-S-C to M-U-I
- SA 1C-11: C-S-C to M-U-I



Subarea 1C

Approved Zoning Changes



Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1C-1	C-S-C to M-U-I	.86 ac.	SMA	1990	209NE 4L

Use and Location:

College Park Auto Parts, Soaps laundry and church located on the east side between Guilford Road and Fordham Lane. (TM 33, Grid C-4, College Park Homes Subdivision, SDAT 1403, Block 6, Parcels A-2, A-3, A-4 and 109)

Discussion:

This mix of uses exists in an area determined to be appropriate for development as a mixed-use town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1C-2	C-S-C to M-U-I	1.91 ac. 1.88 ac.	SMA	1990	209NE 4L

Use and Location:

A compact mix of commercial uses located at 7201-7215 Baltimore Avenue, containing dry cleaners, photos, insurance, keys and Showcase Furniture between Guilford Road and Calvert Road, including commercially zoned dwellings and apartments located at 4505-4509 Calvert Road and 4502-4506 Guilford Road. (TM 33, Grid C-4, College Park Homes Subdivision, SDAT 0633, Lots 4-6; SDAT 0632, Lots 1-3; Plat A-0633, Block 1A and Parcels 57 and 95)

Discussion:

This mix of uses exists in an area determined to be appropriate for development as a mixed-use town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1C-3	C-S-C to M-U-I	.08 ac.	SMA SE-3479 (parking modification)	1990 3/14/84	209NE 4L

Use and Location:

T-shirt factory located at 7301 Baltimore Avenue. (TM 33, Grid C-4, Johnson and Curriden's Subdivision of College Park, SDAT 1237, Plat E-1237, Block 23, Lot 14)

Discussion:

This use exists in an area determined to be appropriate for development as a mixed-use town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1C-4	C-O to M-U-I	3.47 ac.	SMA	1990	209NE 4L

Use and Location:

College Park Professional Center condominiums located at 7305, 7307 and 7309 Baltimore Avenue between Hartwick and Knox Roads. (TM 33, Grid C-4, SDAT 135031, Plat 35031, Block 23, Lots 1-20)

Discussion:

This use exists in an area determined to be appropriate for development as a mixed-use town center. Rezoning of this commercial office development to M-U-I is in conformance with the goals of the sector plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1C-5	C-S-C to M-U-I	1.12 ac.	SMA SE-46 (gas station) SE-3548 (parking modification)	1990 2/9/81 11/13/84	209NE 4L

Use and Location:

A compact mix of commercial uses located at 7315-25 Baltimore Avenue, containing former Sunoco station, comics store, Taglio for Hair, R.J. Bentley and Cornerstone Grill, and Loft located in the southeast quadrant of the intersection of Baltimore Avenue and Knox Road. (TM 33, Grid C-4, Johnson and Curriden's Subdivision of College Park, Block 27, Lots 1-8, Plat E-1237 and A-1237)

Discussion:

This mix of uses exists in an area determined to be appropriate for development as a mixed-use town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1C-6	C-O to M-U-I	.91 ac.	SMA	1990	209NE 4L

Location and Use:

A commercial office building located in the southeast quadrant of the intersection of Knox Road and Yale Avenue at 4511 Knox Road. (TM 33, Grids C-4 and D-4, Johnson and Curriden's Subdivision of College Park, SDAT 1237, Plat A-1237, Block 24, Lots 1-4)

Discussion:

This office use exists in an area determined to be appropriate for development as a mixed-use town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1C-7	R-55 to M-U-I	.28 ac.	SMA	1990	209NE 4L

Use and Location:

City of College Park municipal parking lot located at the southwest corner of the intersection of Lehigh Road and Yale Avenue. (TM 33, Grid C-4: Johnson and Curriden's Subdivision of College Park, SDAT1237/Plat A-1237, Block 27, Lots 15-16)

Discussion:

This city parking lot exists in an area determined to be appropriate for development as a mixed-use town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1C-8	C-O to M-U-I	.78 ac.	SMA	1990	209NE 4L

Use and Location:

College Park City Hall located at 4500 Knox Road. (TM 33, Grid C-4: Hannah L. Kelly Subdivision of Johnson and Curriden's Subdivision, SDAT07972/Plat A-7972, Block 1, Lots 22-25, part of Lot 21, and Lots 1-11)

Discussion:

These city properties exist in an area determined to be appropriate for development as a mixed-use town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1C-9	C-S-C to M-U-I	.60 ac.	SMA	1990	209NE 4L

Use and Location:

A compact mix of commercial uses located at 7403-7413 Baltimore Avenue, containing Philadelphia Cheesesteak Factory, Subway subs, Penguin Pizza and Grill, Haircuttery, Smoothie King and Council Travel located in the northeast quadrant of the intersection of Baltimore Avenue and Knox Road. (TM 34, Grid C-4, College Park Subdivision, SDAT 1414, Plat A-4414, Parcel A, SDAT 7972, Plat A-7972, Lots 12-16 and part of Lots 17-19 (Parcel A), Lot 20 and part of Lot 21)

Discussion:

This mix of uses exists in an area determined to be appropriate for development as a mixed-use town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Approved SMA/ZAPS/SE Date	200' Scale Index Map
1C-10	C-S-C to M-U-I	.12 ac.	SMA	1990	209NE 4L

Use and Location:

Christian Science store located in the northwest corner of the intersection of Lehigh Road and Yale Avenue, known as 4500 to 4510 Lehigh Road. (TM 33, Grid C-4, Johnson and Curriden’s Subdivision of College Park, SDAT 7972, Plat A-7972, Block 2, Lots 1 and 2)

Discussion:

This use exists in an area determined to be appropriate for development as a mixed-use town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Approved SMA/ZAPS/SE Date	200' Scale Index Map
1C-11	C-S-C to M-U-I	1.03 ac.	SMA	1990	209NE 4L

Use and Location:

A compact mix of commercial uses located between 4715 and 4723 Baltimore Avenue and 4507 to 4513 College Avenue, containing the Bagel Place, Outlet sports, Hair Design, Paperworks, Kemp Mill Music, Cluck-U Chicken, located in the southeast quadrant of the intersection of College Avenue and Baltimore Avenue. (TM 33, Grid C-3, College Park Subdivision, SDAT 7972, Plat A-7972, Block 2, Lots 11-26)

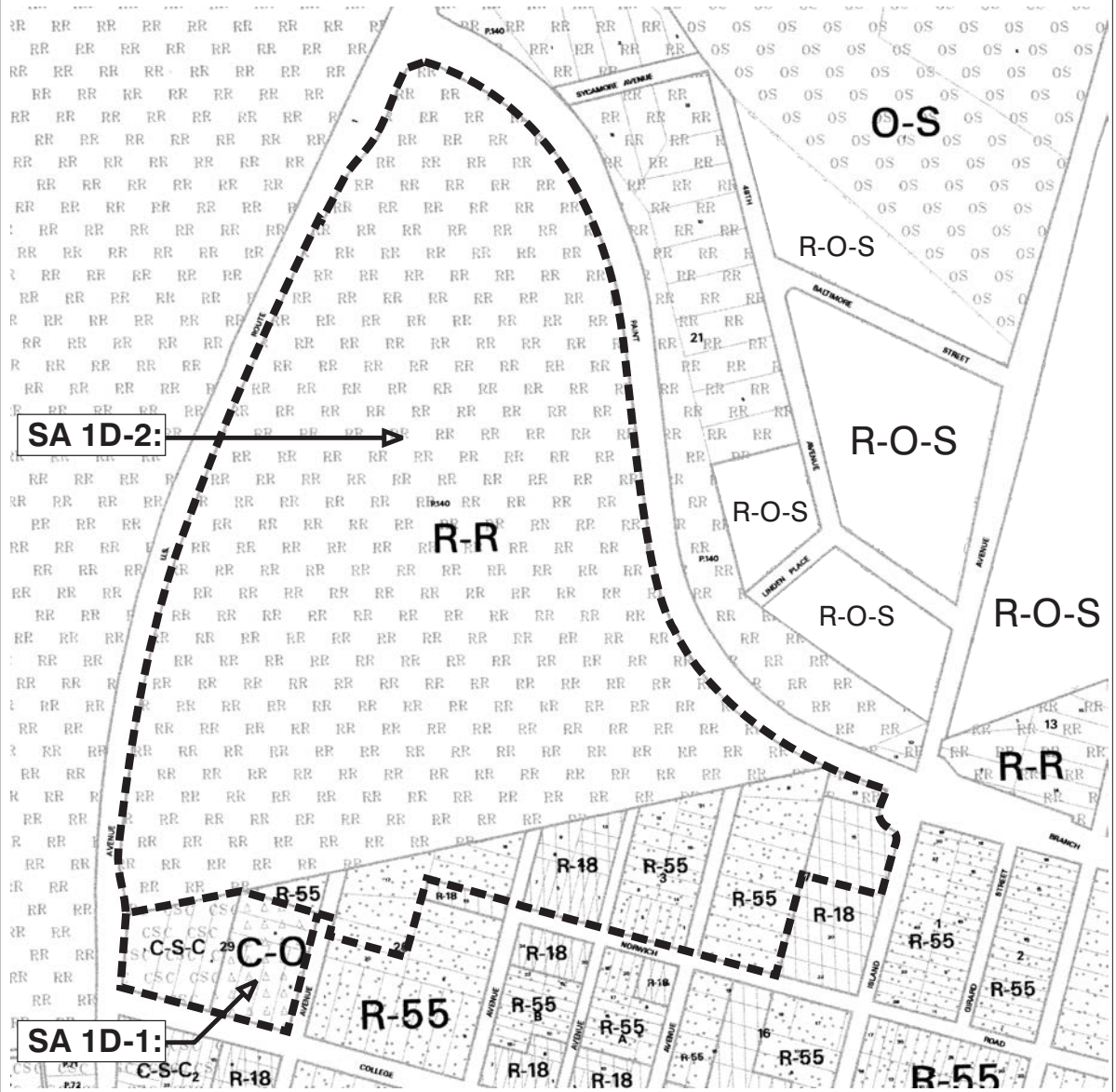
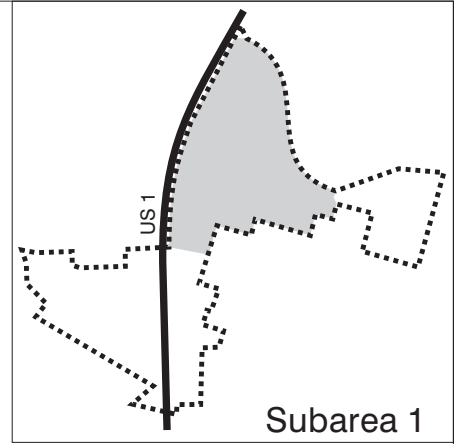
Discussion:

This mix of uses exists in an area determined to be appropriate for development as a mixed-use town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Subarea 1D (SA 1D)

SA 1D-1: C-O and C-S-C to M-U-I

SA 1D-2: R-R, R-18, R-55 to M-U-I



Subarea 1D

Approved Zoning Changes



Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1D-1	C-S-C to M-U-I C-O to M-U-I	1.31 ac. 1.36 ac.	SMA	1990	209NE 4L

Use and Location:

Maryland Book Exchange, located at 7505 Baltimore Avenue and adjoining C-O-zoned parking lot. (TM 33, Grid C-3, SDAT 01237, Johnson and Curriden’s Subdivision of College Park, Plat A-1237, Block 29, Lots 1-10)

Discussion:

These properties exist in an area determined to be appropriate for development as a mixed-use town center. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1D-2	R-R to M-U-I R-18 to M-U-I R-55 to M-U-I	44.33 ac. 1.93 ac. 5.48 ac.	SMA	1990	209NE 4R

Use and Location:

University of Maryland property, including Fraternity Row and Ritchie Coliseum, physical plant, campus post office, shuttle, research green houses, and the Leonard Town apartments located east of US 1 and south of Paint Branch Parkway, including premises known as 4600 Norwich Road, 7512 Princeton Avenue and 7505 Yale Avenue. (TM 33, Grids C-3, D-2 and D-3: College Park Subdivision of College Park, part of Parcel 140, Block 29, Lot 11; Block 26, Lots 14-19; SDAT 0806, Block 3, Lots 1-21, Block 4, Lots 1-13, Block 17, Lots 1-9 and 13-17)

Discussion:

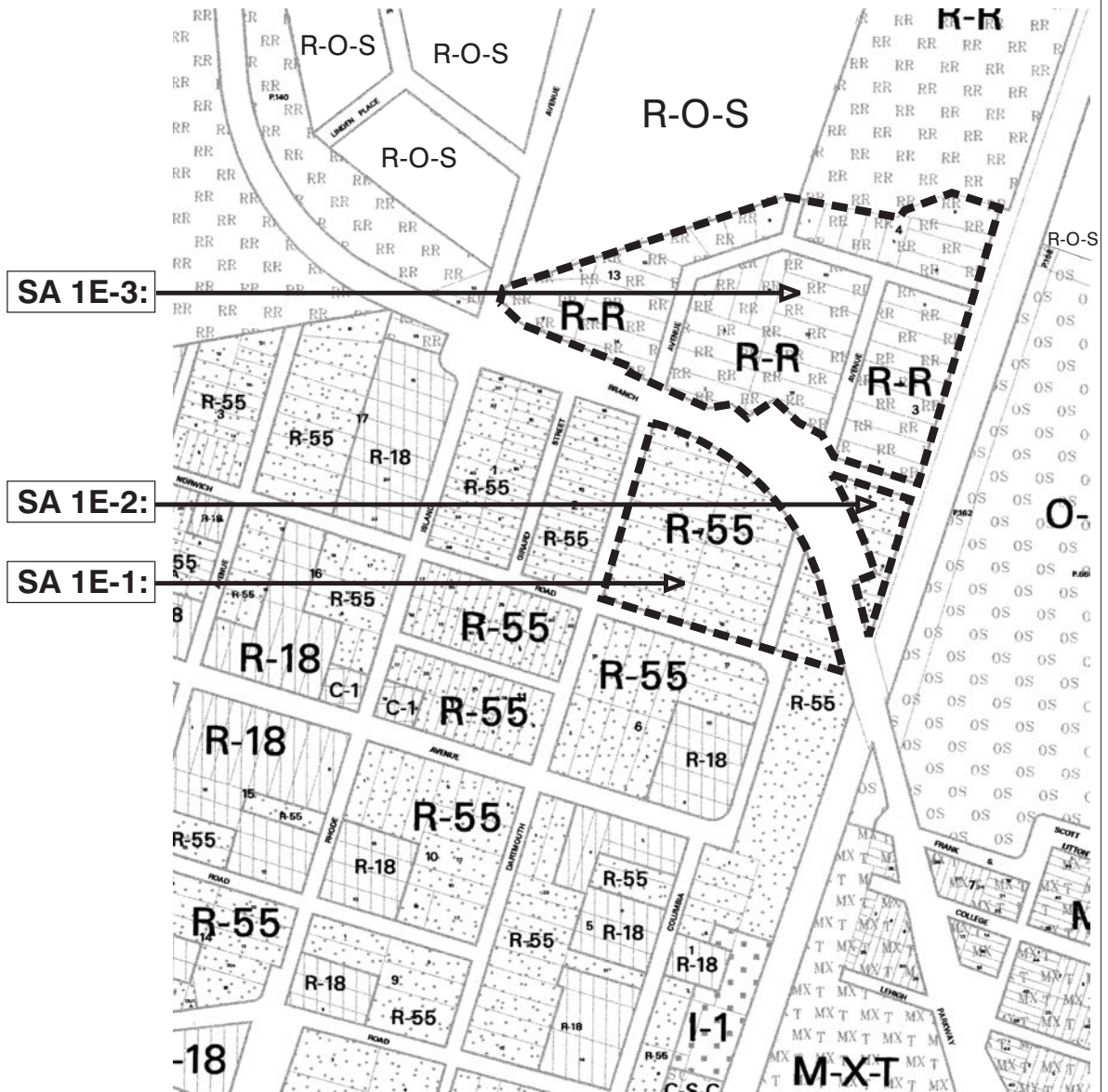
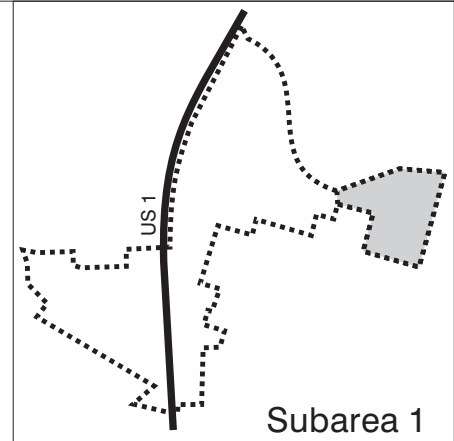
These properties exist in an area determined to be appropriate for development as a mixed-use town center. This rezoning provides an opportunity for mixed-use development incorporating appropriate public spaces as part of the designated town center, including retention of Fraternity Row and the physical plant. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Subarea 1E (SA 1E)

SA 1E-1: R-55 to M-U-I

SA 1E-2: R-55 to M-U-I

SA 1E-3: R-R to M-U-I



Subarea 1E

Approved Zoning Changes



Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1E-1	R-55 to M-U-I	4.17 ac.	SMA	1990	209NE 4R

Use and Location:

The University of Maryland physical distribution center located south of the Paint Branch Parkway, north of Norwich Avenue, west of Columbia Avenue, and east of Dartmouth Avenue. (TM 33, Grids D-3 and E-3 and E-4: Johnson and Curriden's Subdivision of College Park, SDAT 01237/Plat E-1237, SDAT 09972/Plat A-7972, Block 7, Lots 1-18; Block 2 Lots 1-3, part of Lots 4 and 5)

Discussion:

This rezoning creates the development opportunity to provide office/high-tech uses in a compact development form in proximity to the College Park-University of Maryland Metro Station. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1E-2	R-55 to M-U-I	.65 ac.	SMA	1990	209NE 4R

Use and Location:

Vacant University of Maryland property, located east of Columbia Avenue and northeast of Paint Branch Parkway. (TM 33, Grid E-3: Johnson and Curriden's Subdivision of College Park, SDAT01237Plat A-1237, Block 2, part of Lots 4-9)

Discussion:

This rezoning provides the development opportunity to provide office/high-tech uses in a compact development form in proximity to the College Park-University of Maryland Metro Station. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
1E-3	R-R to M-U-I	9.69 ac.	SMA	1990	209NE 4R

Use and Location:

The University of Maryland Fire and Rescue Institute, located on the north side of Paint Branch Parkway and west of the CSX railroad tracks. (TM 33, Grids D-3 and E-3: Paint Branch Elementary Subdivision, Plat A-7688, SDAT07688, Parcel A; College Park Subdivision, Plat E-1237, Block 4, Lots 1-9; Block 3, Lots 1-9; Block 8, Lots 2-16, part of Lots 1, 2, 17 and 18; Block 13, Lots 3-14, part of Lots 2 and 15)

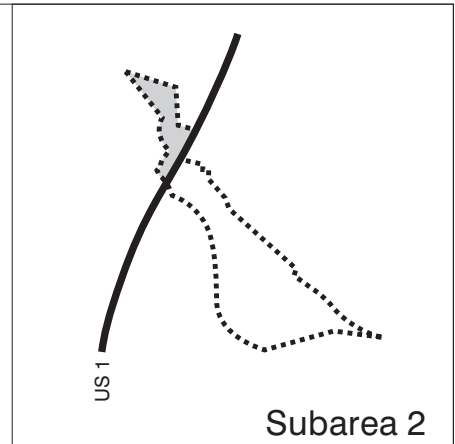
Discussion:

This rezoning provides the development opportunity to provide office/high-tech uses in a compact development form in proximity to the College Park-University of Maryland Metro Station. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

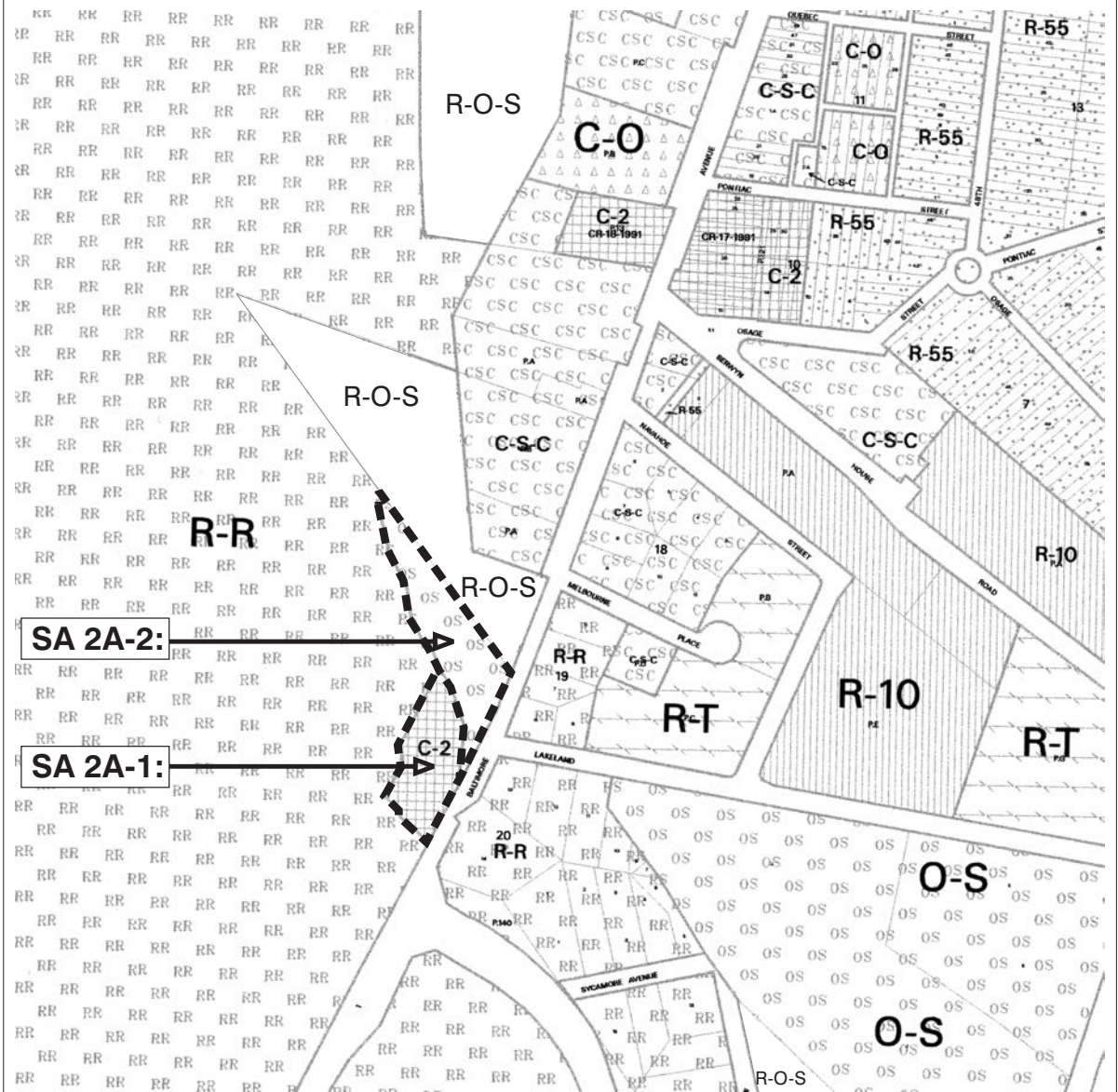
Subarea 2A (SA 2A)

SA 2A-1: C-2 to R-R

SA 2A-2: O-S to R-R



Subarea 2



Subarea 2A

Approved Zoning Changes



Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
2A-1	C-2 to R-R	1.03 ac.	SMA	1990	210NE 4L, 4R

Use and Location:

Undeveloped University of Maryland open space within the floodplain located west of Baltimore Avenue, opposite its intersection with Lakeland Road. The southern portion of the property is developed with surface parking for the university. (TM 33, Grid D-2: College Park Subdivision, part of Parcel 140)

Discussion:

The base zone for The University of Maryland is R-R. Although this portion of the university lies within the stream valley, rezoning to the R-R Zone is in conformance with the university's overall base zone.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
2A-2	O-S to R-R	1.28 ac.	SMA	1990	210NE 4R, 4L

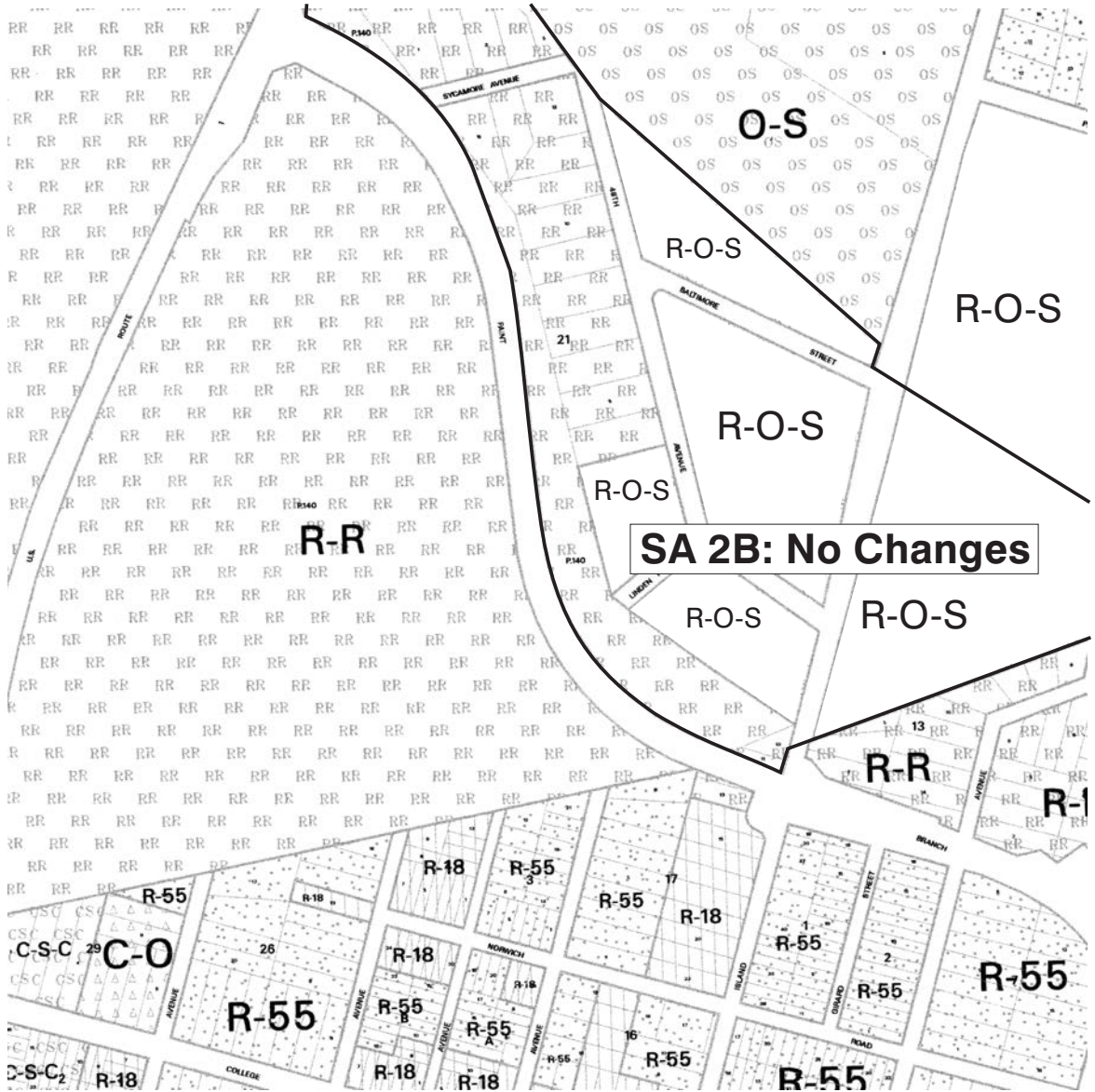
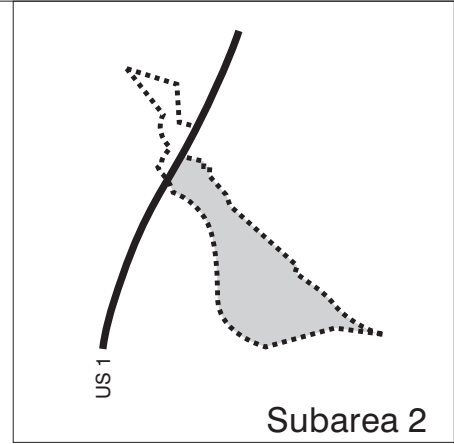
Use and Location:

Undeveloped University of Maryland property located west of Baltimore Avenue within the Paint Branch Stream Valley. (TM 33, Grid D-2: part of Parcel 140)

Discussion:

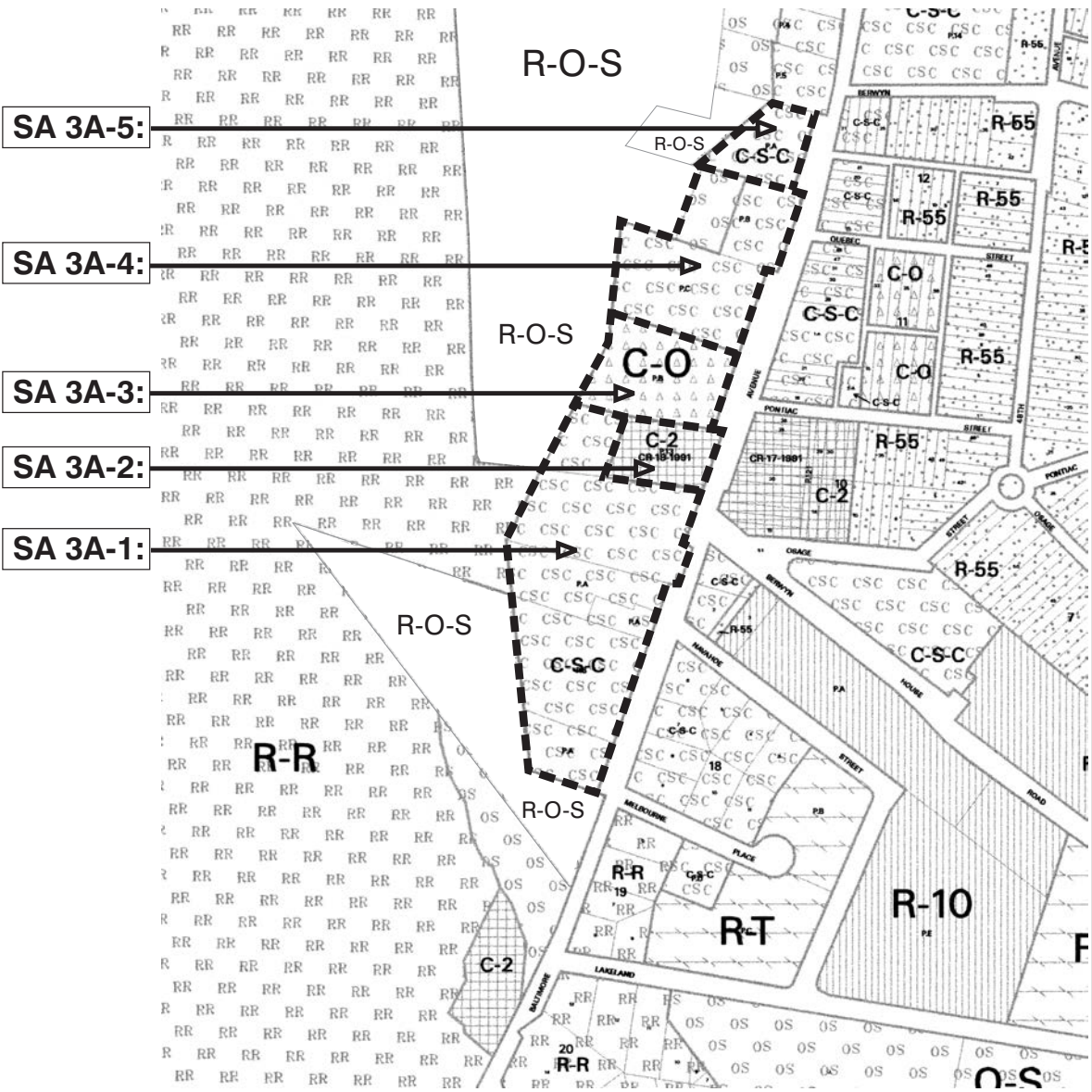
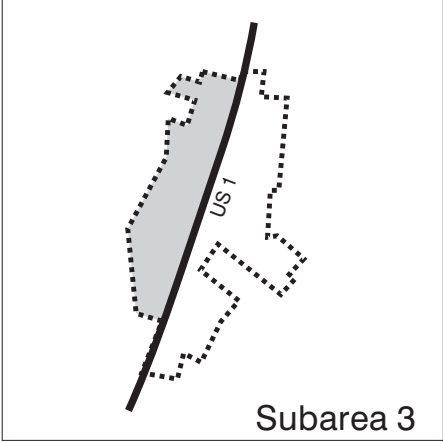
The base zone for the university is the R-R Zone. Although this portion of the university lies within the stream valley, rezoning to the R-R Zone is in conformance with the university's overall base zone.

Subarea 2B (SA 2B)
No Changes



Subarea 3A (SA 3A)

- SA 3A-1: C-S-C to M-U-I**
- SA 3A-2: C-2 to M-U-I**
- SA 3A-3: C-O to M-U-I**
- SA 3A-4: C-S-C and O-S to M-U-I**
- SA 3A-5: C-S-C to M-U-I**



Subarea 3A

Approved Zoning Changes



Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3A-1	C-S-C to M-U-I	6.31 ac.	SMA SE-3227 (auto parts w/installation)	1990 1/21/81	210NE 4R
			SE-4213 (fast food)	10/28/96	

Use and Location:

Jerry's Subs, former restaurant, Merchants Tire, liquor store and part of Koons used car lot located on the west side at 8136, 8150, 8200, 8204 and Baltimore Avenue. (TM 33, Grids D-1 and D-2, SDAT 07361, Plat A-7361, Parcel B; SDAT 06974, Parcel A; SDAT 07158, Plat A-7158, Kramer Subdivision; SDAT 9374, Parcel A; Parcel 121; Raymond Towers Subdivision, SDAT 07336, Plat A-7336, part of Parcel B)

Discussion:

These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3A-2	C-2 to M-U-I	.86 ac.	SMA	1990	210NE 4R

Use and Location:

Koons Ford used car dealership located on the west side at 8320 Baltimore Avenue. (TM 33, Grid D-1: College Park Subdivision, Parcel 13)

Discussion:

Rezoning to M-U-I allows conversion of an obsolete zoning category (i.e., C-2) to eliminate same from the adopted zoning maps. Rezoning to M-U-I will also create flexible development opportunities consistent with the goals of the plan. The existing auto sales and service use in Subarea 3 is recognized as a permitted use within the Development District Overlay Zone (DDOZ), because the DDOZ eliminates the need for a special exception for auto sales and service uses.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3A-3	C-O to M-U-I	1.68 ac.	SMA	1990	210NE 4R

Use and Location:

Office building located on the west side at 8400 Baltimore Avenue. (TM 33, Grid D-1, Raymond Towers Subdivision; SDAT 07336, Plat A-7336, Parcel B)

Discussion:

Rezoning to M-U-I provides a wider range of uses than C-O Zone. Rezoning to M-U-I will also create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3A-4	C-S-C to M-U-I O-S to M-U-I	3.00 ac. .45 ac.	SMA SE-4122 (fast food)	1990 3/14/94	210NE 4R

Use and Location:

Taco Bell and former restaurant located on the west side at 8424, 8428 and 8430 Baltimore Avenue and undeveloped land behind 8430 Baltimore Avenue. (TM 33, Grid D, Parcel 8 and Parcel C, Raymond Towers Subdivision, SDAT 07336, Plat A-7336)

Discussion:

The part of Parcel 8 which fronts US 1 is zoned C-S-C. The rear portion of the property adjacent to the Paint Branch Stream Valley is zoned O-S. Rezoning these parcels to M-U-I will create flexible development opportunities consistent with the goals of the plan. Environmentally sensitive lands will be identified during site plan review under DDOZ provisions.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3A-5	C-S-C to M-U-I	.85 ac.	SMA	1990	210NE 4R

Use and Location:

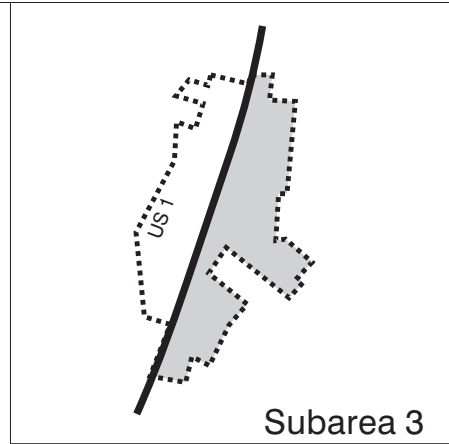
Burger King located on the west side at 8510 Baltimore Avenue. (TM 33, Grid D-1, Inman Turner Subdivision, SDAT 09076, Plat A-9076, Parcel A)

Discussion:

This use exists in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Subarea 3B (SA 3B)

- SA 3B-1: R-R & C-S-C to M-U-I**
- SA 3B-2: C-S-C to M-U-I**
- SA 3B-3: C-S-C & R-55 to M-U-I**
- SA 3B-4: C-S-C to M-U-I**
- SA 3B-5: C-2 to M-U-I**
- SA 3B-6: C-O to M-U-I**
- SA 3B-7: C-S-C to M-U-I**
- SA 3B-8: C-S-C to M-U-I**
- SA 3B-9: C-S-C to M-U-I**



Subarea 3B

Approved Zoning Changes



Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3B-1	R-R to M-U-I	.41 ac.	SMA	1990	210NE 4R
	C-S-C to M-U-I	1.42 ac.			

Use and Location:

College Park Fire Department located in the northeast quadrant of the intersection of Baltimore Avenue and Lakeland Road. Office building at 4705 Melbourne Place, located south of Melbourne Place, to the rear of the College Park Fire Department. (TM 33, Grid D-2, Lakeland Subdivision of College Park, SDAT01008/Plat A-1008, Block 19, Lots 5-7, and part of Lot 8; Lakeland Subdivision of College Park, SDAT 108082, Parcel D)

Discussion:

This public use and its rezoning is in conformance with the established public lands policy, which states that all land in public/quasi-public use should be placed in the most restrictive and/or dominant adjacent zone, whichever bears the closest relationship to the intended character of the area. The adjacent office building is placed in the M-U-I Zone to be consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3B-2	C-S-C to M-U-I	2.86 ac.	SMA	1990	210NE 4R
			SE-1557 (gas station)	11/8/90	

Use and Location:

Town Hall liquor/lounge, Solar Shield window tinting, Amoco station, located on the east side at 8131, 8139, 8141, 8153 Baltimore Avenue and the Campus Village Shops located at 4731-4745 Navahoe Street. (TM 33, Grid D-2, Lakeland Subdivision of College Park, SDAT 01008, Plat A-1008, Block 18, Lots 3-11)

Discussion:

These commercial uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3B-3	R-55 to M-U-I	.10 ac.	SMA	1990	210NE 4R
	C-S-C to M-U-I	.44 ac.			

Use and Location:

McDonald's restaurant located on the east side at 8301 Baltimore Avenue and adjoining parking lot. (TM 33; Grid D-2, Lakeland Subdivision of College Park; SDAT01008/Plat A-1008, Block 17, Lots 1, 2 and 3)

Discussion:

The restaurant which is zoned C-S-C and the parking lot which is zoned R-55 is part of the McDonald's commercial enterprise. Rezoning the entire parcel to M-U-I will create a flexible development opportunity consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3B-4	C-S-C to M-U-I	2.05 ac.	SMA	1990	210NE 4R

Use and Location:

Commercial office condominiums located at 4700 Berwyn House Road, south of Campus View Terrace. (TM 33, Grid D-2, College Park Subdivision, SDAT 118081, Parcel B, and SDAT 121046, Parcel PH1)

Discussion:

This use exists in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3B-5	C-2 to M-U-I	2.00 ac.	SMA	1990	210NE 4R

Use and Location:

Koons Ford new car dealership and service department located on the east side at 8315 Baltimore Avenue. (TM 33, Grid D-1: Addition to Berwyn Subdivision of College Park, SDAT0637/Plat E-00637, Block 10, Lots 11-32; part of Parcel 121)

Discussion:

Rezoning to M-U-I allows conversion of an obsolete zoning category (i.e., C-2) to eliminate same from the adopted zoning maps. Rezoning to M-U-I will also create flexible development opportunities consistent with the goals of the plan. The existing auto sales and service uses in Subarea 3 are recognized as a permitted use within the Development District Overlay Zone (DDOZ) because the DDOZ eliminates the need for a special exception for auto sales and service. The undeveloped portion of the Koons property in the R-55 Zone is retained at this time.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3B-6	C-O to M-U-I	1.53 ac.	SMA	1990	210NE 4R

Location and Use:

Office building and adjacent parking lot located at 4716 Quebec Street and owned by University College and used for office and classroom space. (TM 33, Grid D-1, Addition to Berwyn Subdivision of College Park, SDAT 00637/Plat E-0637, Block 11, Lots 19-15 and 32-38)

Discussion:

This use exists in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3B-7	C-S-C to M-U-I	1.71 ac.	SMA SE-4093 (gas station)	1990 1/28/93	210NE 4R

Use and Location:

Exxon station and College Park Motel located on the east side at 8401 and 8419 Baltimore Avenue. (TM 33, Grid D-1, Addition to Berwyn, Subdivision of College Park, SDAT 00637, Plat A-0637, Block 11, Lots 18-21, 28-31 and Lots 47 and 48; SDAT 01448, Plat E-1448, Lots 1A and 2A)

Discussion:

These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3B-8	C-S-C to M-U-I	.61 ac.	SMA	1990	210NE 4R

Use and Location:

Seven Seas restaurant and office building located on the east side at 8503 and 8505 Baltimore Avenue. (TM 33, Grid D-1, SDAT 00766, Plat A-0766, Addition to Berwyn, College Park Subdivision, Block 12, Lots 15-20 and 41)

Discussion:

These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
3B-9	C-S-C to M-U-I	.38 ac.	SMA	1990	210NE 4R

Use and Location:

Nations Trust Mortgage at 8505 Baltimore Avenue, Genesis Communication at 8507 Baltimore Avenue, and vacant building (former Yates Auto Parts) located at 4703 Berwyn Road. (TM 33, Grid D-1, Addition to Berwyn, Subdivision of College Park, SDAT 00766, Plat A-0766, Block 12, Lots 21-25)

Discussion:

These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Subarea 4A (SA 4A)

SA 4A-1: O-S & C-S-C to M-U-I

SA 4A-2: C-S-C to M-U-I

Subarea 4B (SA 4B)

SA 4B-1: R-55 & C-S-C to M-U-I

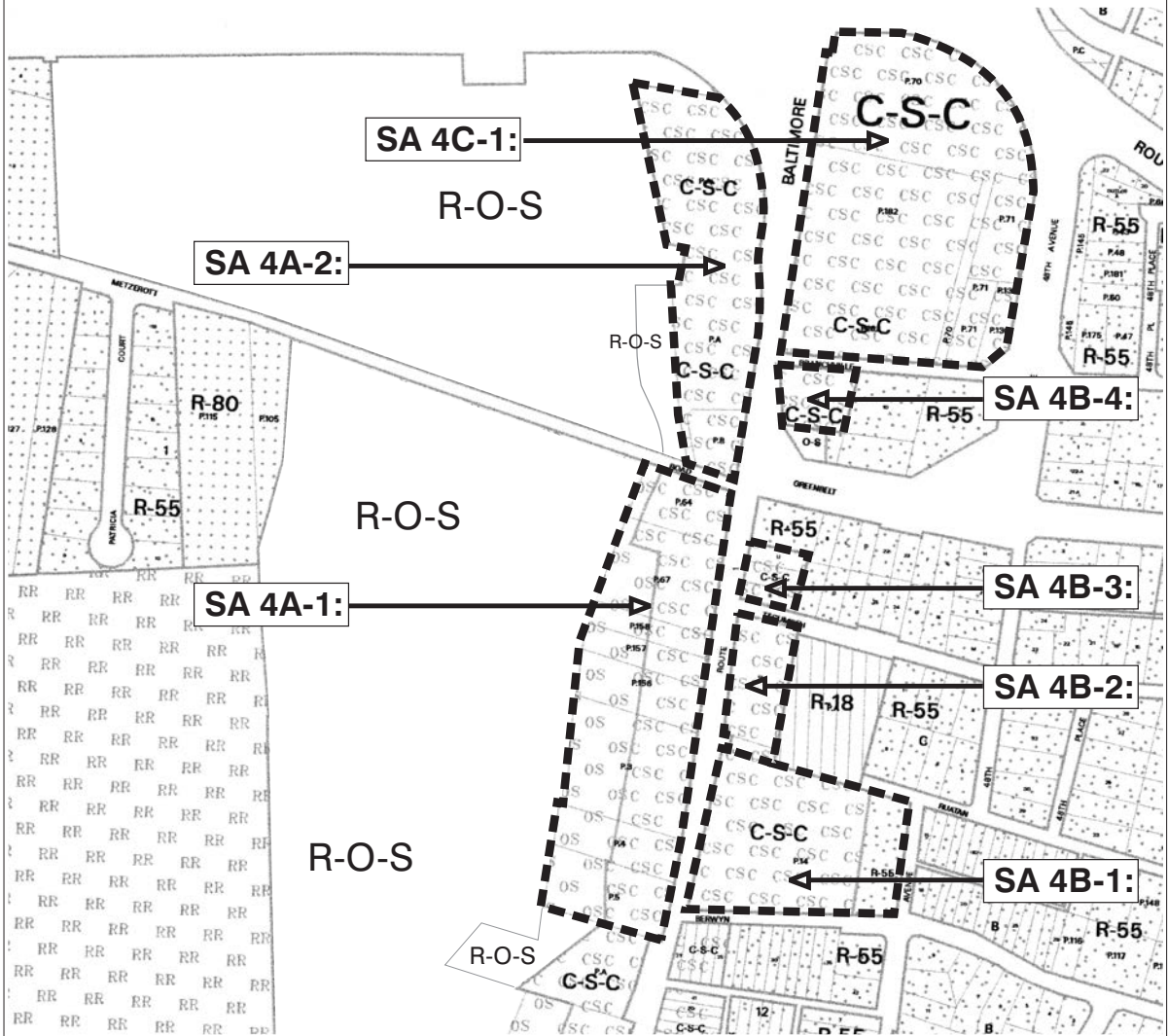
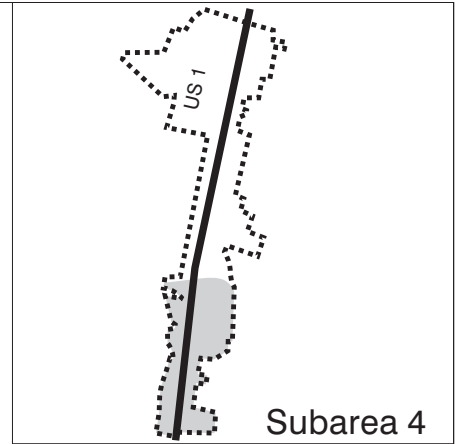
SA 4B-2: C-S-C to M-U-I

SA 4B-3: C-S-C to M-U-I

SA 4B-4: C-S-C to M-U-I

Subarea 4C (SA 4C)

SA 4C-1: C-S-C to M-U-I



Subarea 4A, Subarea 4B and Subarea 4C

Approved Zoning Changes



Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4A-1	O-S to M-U-I	2.88 ac.	SMA	1990	210NE 4R
	C-S-C to M-U-I	3.95 ac.	SE-3597 (auto parts w/ installation)	5/30/85	
			SE-4015 (parking lot-commercial)	6/22/92	
			SE-4115 (car wash)	3/14/94	

Use and Location:

The 7-Eleven convenience store, at 8600 Baltimore Avenue, Skate Board Store at 8608 Baltimore Avenue, car wash at 8616 Baltimore Avenue, abandoned buildings at 8704, 8708, and 8800 Baltimore Avenue, Jiffy Lube at 8808 Baltimore Avenue, all on the west side. (TM 33; Grid D-1; Parcel 4; SDAT00436/Plat 436, Ardwick Stephen & Randles 1st addn to CLM, Parcel 5, Parcel 8, Parcel 7, Parcel 85, Parcel 3, Parcels 156-158, Parcel 67; TM 25, Grid D-4: Parcels 63 and 64)

Discussion:

The subject properties are split zoned. Portions of these properties fronting US 1 are zoned and developed commercially. The rear of these properties is undeveloped and zoned O-S. The commercial frontage is shallow and poses development constraints. The rear portions of the properties also pose development constraints based on the topography. Given these constraints, redevelopment in accordance with the goals of the plan is unlikely unless an incentive is provided. Rezoning these properties to the M-U-I Zone creates an incentive to redevelop the entire property in each case, in accordance with the goals of the plan, while providing maximum design flexibility to demonstrate code compliance at time of redevelopment.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4A-2	C-S-C to M-U-I	4.5 ac.	SMA	1990	211NE 4R

Use and Location:

American Outlet Carpets and Comfort Inn located on the west side at 8900 and 9020 Baltimore Avenue. (TM 25, Grid D-4, SDAT 118096, Sealing Subdivision, Plat 118096, Parcels A and B)

Discussion:

These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4B-1	R-55 to M-U-I	.63 ac.	SMA	1990	210NE 4R
	C-S-C to M-U-I	3.01 ac.			

Use and Location:

Rear parking for the Best Western Hotel located in the R-55 Zone and the C-S-C-zoned hotel property located at 8601 Baltimore Avenue on the east side. (TM 33, Grid D-1: W&B Pike Subdivision of College Park, Parcel 14)

Discussion:

A portion of this commercially developed property used for parking is zoned R-55. Rezoning the entire property to M-U-I will create a flexible development opportunity consistent with the goals of the plan. A 200±-foot-long strip along Berwyn Road is incorrectly shown in the R-55 Zone on the May 1990 SMA, 200-scale zoning map.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4B-2	C-S-C to M-U-I	.87 ac.	SMA SE-2115	1990 12/19/69	211NE 4R

Use and Location:

Gas station (Xtramart) located on the east side at 8721 Baltimore Avenue, south of Tecumseh Street. (TM 33, Grid D-1, SDAT 00660, Bewley Estates Subdivision of College Park, Plat A-0660, Lot 13, and part of SDAT 6976, Tecumseh Gardens Parcel A)

Discussion:

This use exists in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4B-3	C-S-C to M-U-I	.42 ac.	SMA SE-3069 (auto sales used)	1990 12/5/77	211NE 4R

Use and Location:

Used car sales located on the east side at 8801 Baltimore Avenue north of Tecumseh Street. (TM 25, Grid D-1, SDAT 00660, Bewley Estates Subdivision of College Park, Plat C-0660, Lots S, T and U)

Discussion:

This use exists in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4B-4	C-S-C to M-U-I	.55 ac.	SMA	1990	211NE 4R

Use and Location:

Cass Trailer Sales and U-Haul located on the east side at 8907 Baltimore Avenue. (TM 25, Grid D-4, SDAT 00637, Bewley Estates Subdivision of College Park, Plat E-0637, Lot 11)

Discussion:

These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan. The existing mobile home trailer sales and service facilities in Subarea 4 are recognized as a permitted use within the Development District Overlay Zone (DDOZ) because the DDOZ eliminates the need for a special exception for auto sales and service uses.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4C-1	C-S-C to M-U-I	8.45 ac.	SMA SE-601 (bowling center)	1990 11/1/60	211NE 4R

Use and Location:

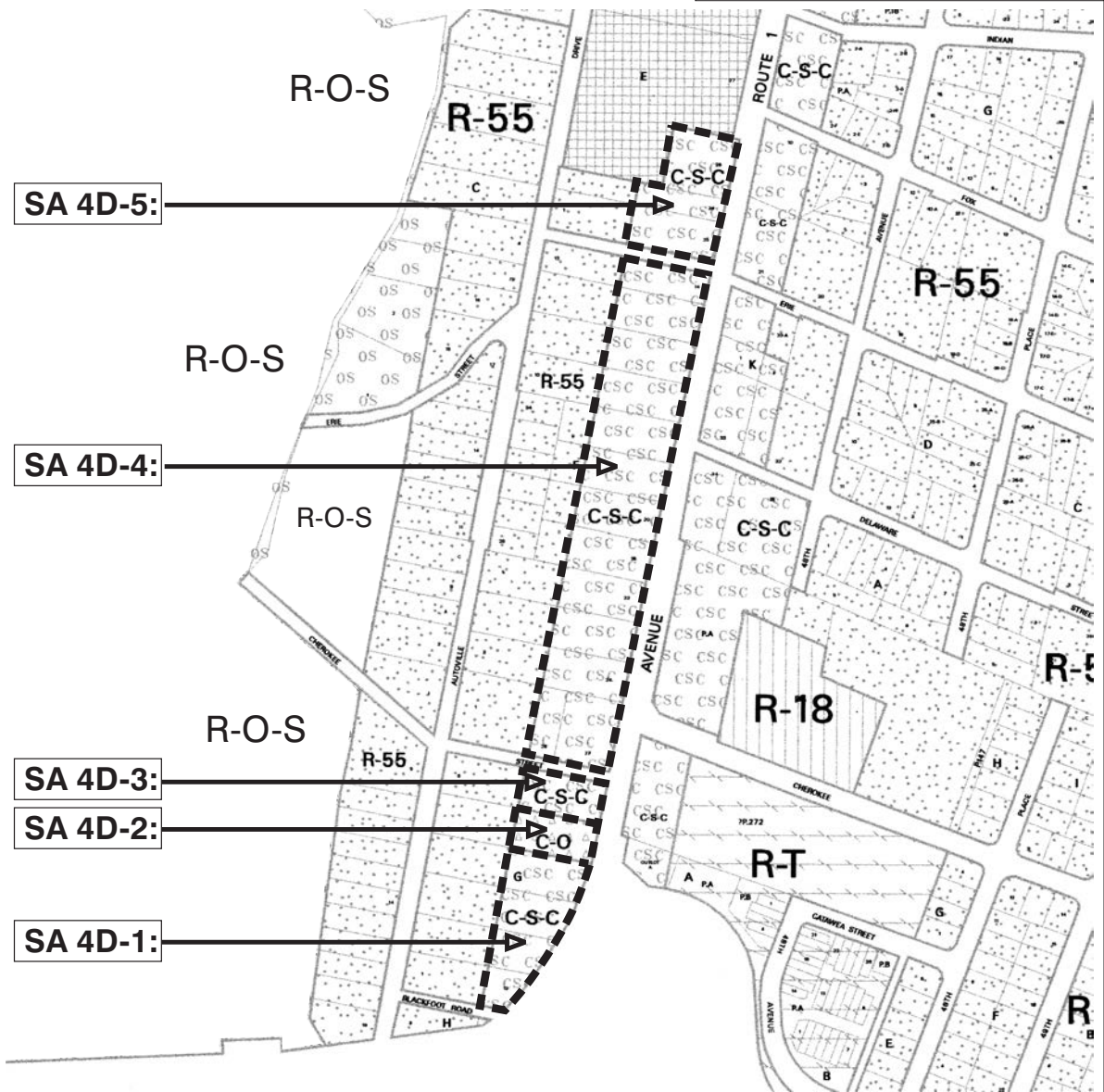
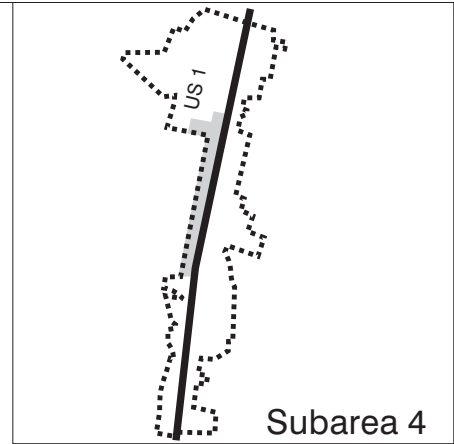
Jenkins Garage (9001), Pizza Hut (9015), AMF College Park Bowling Lanes (9021), Empty business—formerly SECU of Maryland (9031), Atomic Music (9035), CD Depot (9039), and dwelling located at 4714 Branchville Road. (TM 25, Grid D-4, Branchville Road Subdivision of College Park, Parcels 70, 71, 89 and 134)

Discussion:

These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Subarea 4D (SA 4D)

- SA 4D-1: C-S-C to M-U-I
- SA 4D-2: C-O to M-U-I
- SA 4D-3: C-S-C to M-U-I
- SA 4D-4: C-S-C to M-U-I
- SA 4D-5: C-S-C to M-U-I



Subarea 4D

Approved Zoning Changes



Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4D-1	C-S-C to M-U-I	1.36 ac.	SMA	1990	211NE 4R

Use and Location:

Wood's Florist and a vacant residential dwelling located on the west side at 9066 and 9078 Baltimore Avenue, respectively. (TM 25, Grid D-4: Autoville Subdivision, SDAT 01010, Plat A-1010, Block G, Lots 10-12 and part of Lot 9)

Discussion:

These properties are in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4D-2	C-O to M-U-I	.47 ac.	SMA	1990	211NE 4R

Use and Location:

Long and Foster Real Estate office located on the west side at 9094 Baltimore Avenue. (TM 25, Grid D-4, Autoville Subdivision, SDAT 01010, Plat A-1010, Block G, part of Lots 8 and 9)

Discussion:

Rezoning to M-U-I provides a wider range of uses than the C-O Zone. Rezoning to M-U-I will also create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4D-3	C-S-C to M-U-I	.48 ac.	SMA	1990	211NE 4R

Use and Location:

China Buffet restaurant located on the west side at 9098 Baltimore Avenue, with frontage on Cherokee Street. (TM 25, Grid D-3, SDAT 01010, Autoville Subdivision, Plat A-1010, Block G, Lot 7 and part of Lot 8)

Discussion:

This property is in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4D-4	C-S-C to M-U-I	5.99 ac.	SMA SE-3718 (auto parts w/ installation) SE-3973 (commercial parking lot)	1990 11/17/86 11/15/91	211NE 4R

Use and Location:

Next Day Computers, AC Delco, two vacant properties, Hillcrest Hotel, Lasick's Restaurant, Super 8 Motel, Barnside Diner, former Aqualand Pets, Alario's Pizza, American Legion, located on the west side at 9104, 9112, 9122, 9128, 9150, 9204, 9206, 9208, 9210 and 9218 Baltimore Avenue. (TM 25, Grid D-3, SDAT 02985, Autoville resubdivision, Plat A-2985, Block F, Lots 27 and 28, SDAT 01010, Autoville subdivision, Plat E-1010, Block F, Lots 14-20 and 22-24; SDAT 7002, Lot 28)

Discussion:

These properties are in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

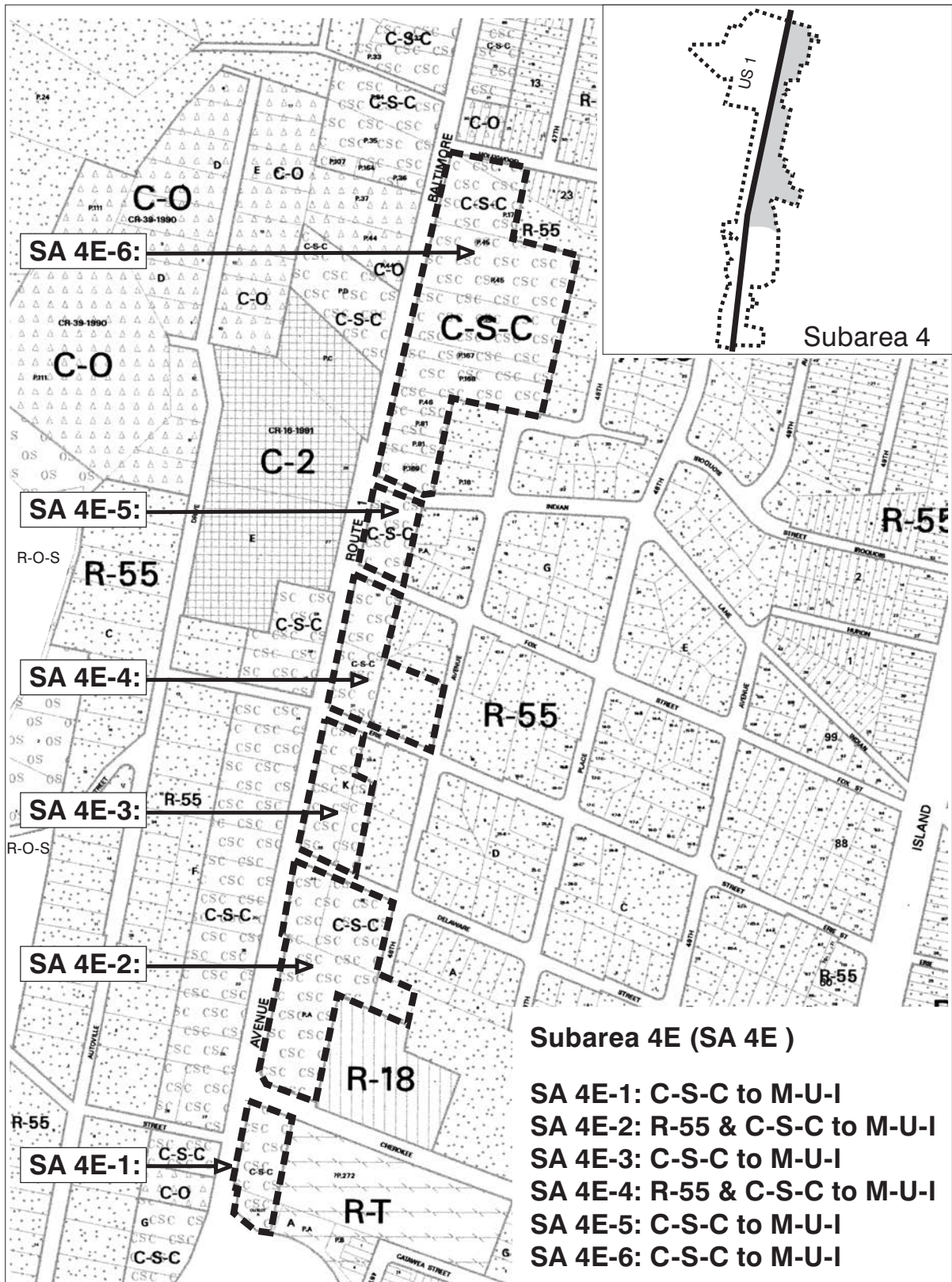
Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4D-5	C-S-C to M-U-I	1.37 ac.	SMA SE-1247 (gas station)	1990 4/28/85	211NE 4R

Use and Location:

Barefeet Shoes, Shell gas station located on the west side at 9300 and 9322 Baltimore Avenue, north of Erie Street. (TM 25, Grid D-3, SDAT 01010, Autoville subdivision, Plat E-1010, Lots 24 and 25, SDAT 05963, Plat A-5963, Block E, Lot 26)

Discussion:

These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.



Subarea 4E

Approved Zoning Changes



Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4E-1	C-S-C to M-U-I	.97 ac.	SMA	1990	211NE 4R

Use and Location :

Atlantic Paging and Cellular and Mandalay Restaurant & Café, located on the east side at 9091 Baltimore Avenue and a vacant former dwelling at 9099 Baltimore Avenue. (TM 25, Grids D-3 and D-4, west portion of Parcels 72 and 69)

Discussion:

These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4E-2	R-55 to M-U-I	.25 ac.	SMA	1990	211NR 4R
	C-S-C to M-U-I	3.43 ac.	SE-1058 (swimming pool)	7/16/64	

Use and Location:

Motels and ancillary parking for the Howard Johnson Express Motel, Ramada Inn and Days Inn, located between Cherokee Street and Delaware Street on the east side of US 1, including the south terminus of 48th Avenue. (TM 25, Grid D-3, E-3: Sims Tract, SDAT09115/Plat A-9115, part of Parcel A; Daniels Park Subdivision, SDAT 02309, Plat A-2309, Lots 34 and 35)

Discussion:

A portion of this commercially developed property is zoned R-55 and used for parking; the balance is zoned C-S-C. Rezoning to M-U-I is made in recognition of the commercial use of the entire property.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4E-3	C-S-C to M-U-I	1.40 ac.	SMA	1990	211NE 4R
			SE-804 (auto body garage)	10/19/62	

Use and Location:

Single-family house, Dinette Gallery, United Window, Proteus bicycle and fitness, Electronics, Inc., Integrated Computers and Memories of Finland, located between Delaware and Erie Streets on the east side at 4704 Delaware Street and 9203, 9213, 9217, 9223 Baltimore Avenue. (TM 25, Grid D-3, Daniels Park Resubdivision of College Park, SDAT 002309, Plat E-2309, Block K, Lot 33, Plat A-2309, Lot 22, SDAT 03245, Plat A-3245, Lot 1)

Discussion:

These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4E-4	R-55 to M-U-I	.84 ac.	SMA	1990	211NE 4R
	C-S-C to M-U-I	1.19 ac.			

Use and Location:

Compassion Center located at 9303 Baltimore Avenue and 4700 Erie Street; Enterprise Rent-A-Car, Charlie's Crab House, Waterbed Store located on the east side at 9321, 9339 Baltimore Avenue; and a dwelling at 4701 Fox Street. (TM 25, Grids D-3 and E-3: Daniels Park Subdivision, SDAT02309, Plat A-2309, Lots 20, 21 and Lot 10)

Discussion:

These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan. Also, rezoning will accommodate the planned buildout and expansion of the existing eleemosynary use (Compassion Center)

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4E-5	C-S-C to M-U-I	.78 ac.	SMA	1990	211NE 4R

Use and Location:

Mason Printing, Route 1 Auto Repair, Great Southern Tattoo, a single-family dwelling, picture framing, Madame Flora, located on the east side between Fox Street and Indian Lane, at 9401, 9403, 9409 and 9413 Baltimore Avenue. (TM 25, Grid E-3, SDAT 02309, Plat A-2309, Block 1, Lot B, Daniels Park resubdivision, Lot 1A)

Discussion:

These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4E-6	C-S-C to M-U-I	6.83 ac.	SMA	1990	211NE 4R
			SE-3419 (auto, truck, trailer sales/rental)	10/22/84	

Use and Location:

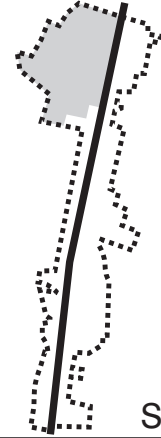
Duron Paint store, Home/Office furniture, Competitive Concrete Foundations Inc., Mr. Sign, Unic America, Silver Spoon, Drums Unlimited, Queenstown Motors & Marine, College Park Boat and RV Center, U.S. Post Office, located on the east side, north of Indian Lane, at 9501, 9505, 9515, 9517, 9521, 9523, 9533, 9591 Baltimore Avenue. (TM 25, Grid E-2, College Park Subdivision, Parcels 169, 81, 46, 168, 167, 153, 45 and 174)

Discussion:

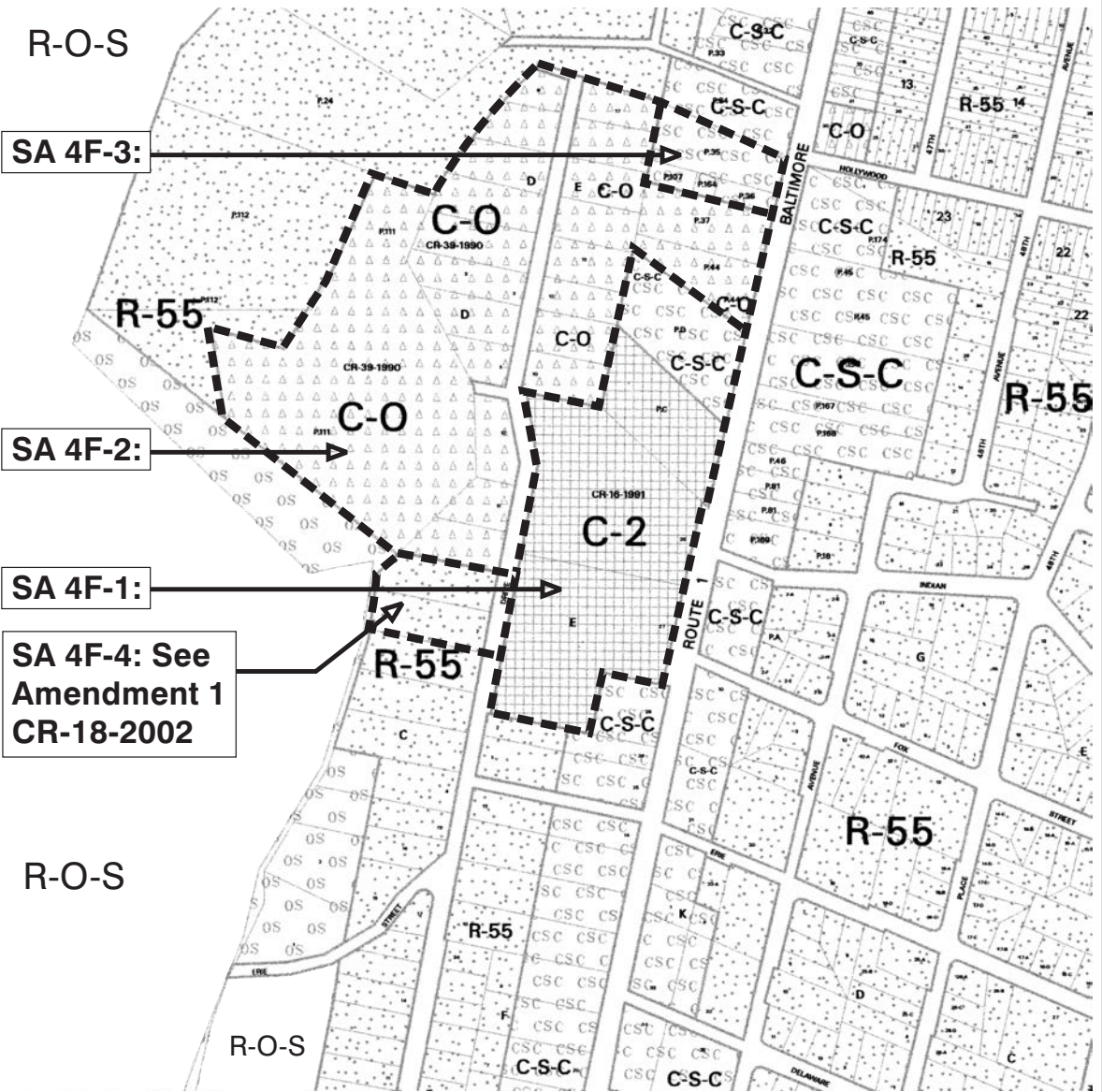
These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Subarea 4F (SA 4F)

- SA 4F-1: C-2 & C-S-C to M-U-I**
- SA 4F-2: C-O to M-U-I**
- SA 4F-3: C-S-C to M-U-I**
- SA 4F-4: R-55 to M-U-I**



Subarea 4



Subarea 4F

Approved Zoning Changes



Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4F-1	C-2 to M-U-I	7.83 ac.	SMA	1990	211NE 4R
	C-S-C to M-U-I	1.31 ac.	SE-1679 (enclosed warehouse)	2/21/68	
			SE-1938 (retail sales & service)	3/19/69	
			SE-862 (auto repair/service)	3/15/63	
			SE-1830 (auto repair/service)	9/18/68	

Use and Location:

Piano store at 9520 Baltimore Avenue, used Honda dealership at 9500 Baltimore Avenue, new Honda dealer at 9400 Baltimore Avenue, VW dealer at 9330 Baltimore Avenue. (TM 25, Grids D-2 and D-3, Autoville Subdivision, SDAT 05354, Plat A-5354, Parcel C; SDAT 02927, Parcel D; SDAT 09397, Plat A-9397, Block E, Lots 27 and 28)

Discussion:

Rezoning to M-U-I allows conversion of an obsolete zoning category (i.e., C-2) to eliminate same from the adopted zoning maps. Rezoning to M-U-I will also create flexible development opportunities consistent with the goals of the plan. The existing auto sales and service businesses in Subarea 6 are recognized as permitted uses within the Development District Overlay Zone (DDOZ) because the DDOZ eliminates the need for a special exception for auto sales and service uses.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4F-2	C-O to M-U-I	22.5 ac.	SMA	1990	211NE 4R

Use and Location:

Undeveloped Mazza property located east and west of Autoville Drive and west of US 1. (TM 25, Grids D-2 and E-2, Parcels 37, 44 and 111; part of Autoville Subdivision, SDAT 01010, Plat E-1010, Block C, Lots 10, 11, 12, Block D, Lots 1-8 and 10-17)

Discussion:

The Mazza property is a significant undeveloped property abutting the Paint Branch Stream Valley Park and is adjacent to US 1. Redevelopment of this property can contribute significantly to the implementation of the US 1 Corridor Sector Plan. Rezoning to M-U-I provides a wider range of uses than the C-O Zone and will create flexible development opportunities consistent with the goals of the plan. "Findings" contained within the 1990 SMA are no longer pertinent as development must conform to the provisions of the Development District Overlay Zone (DDOZ).

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4F-3	C-S-C to M-U-I	1.39 ac.	SMA SE-4174 (mobile home, camping trailer repair/service)	1990 6/26/95 Revised 6/5/00	211NE 4R

Use and Location:

Cycle accessories, Auto Effects and Auto Stereo located on the west side at 9592 and 9600 Baltimore Avenue. (TM 25, Grid D-2 and E-2, W&B Boulevard Subdivision of College Park, Parcels 35, 36, 107 and 164)

Discussion:

These uses exist in an area determined to be appropriate for mixed-use development. Rezoning to M-U-I will create flexible development opportunities consistent with the goals of the plan.

Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
4F-4	R-55 to M-U-I	1.91 ac.	SMA	1990	211NE 4R

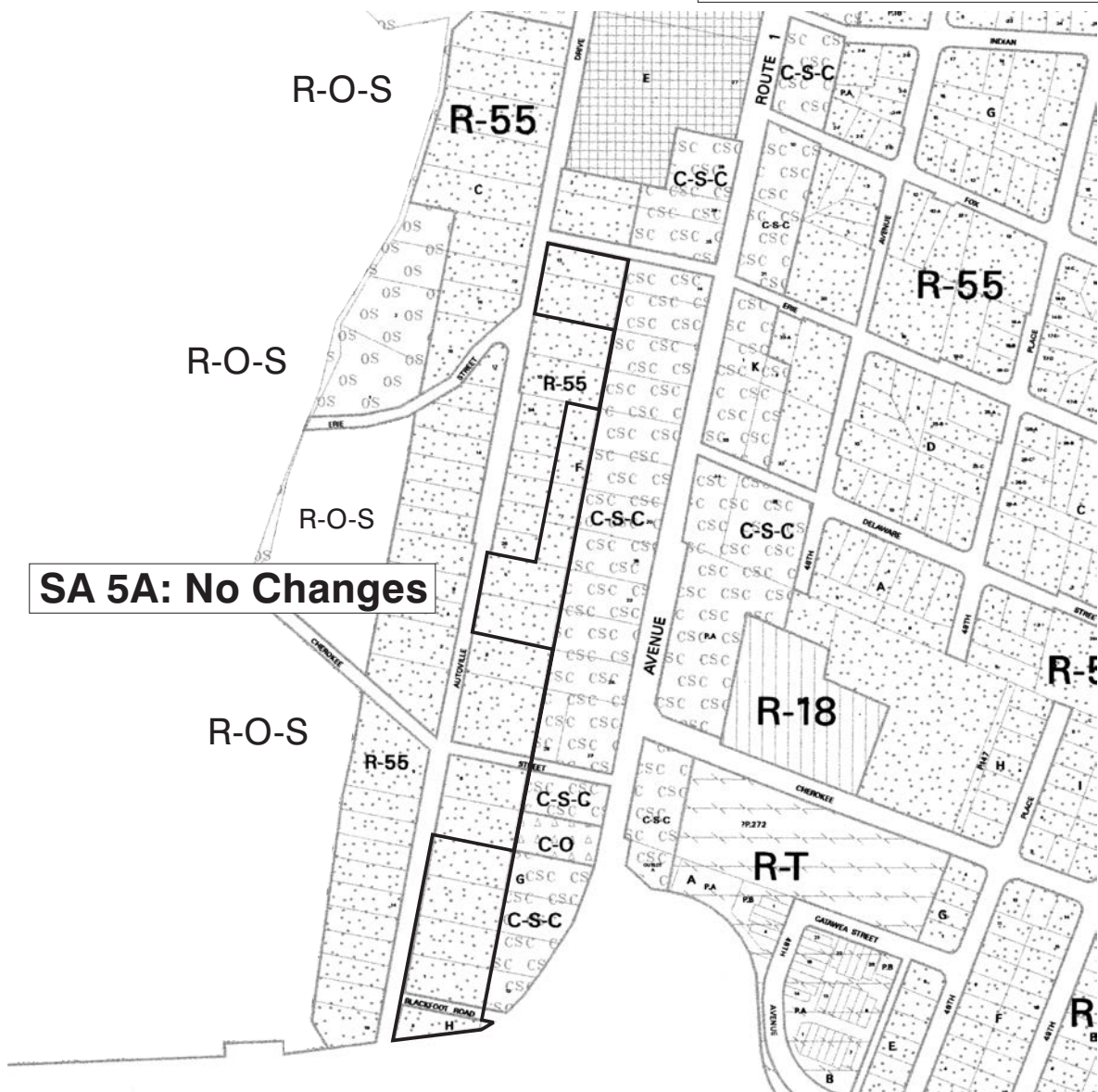
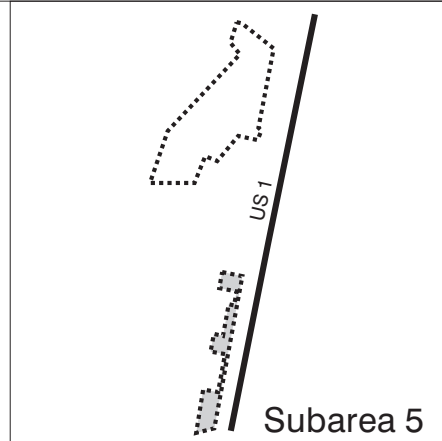
Use and Location:

Two undeveloped lots located on the west side of the undeveloped street known as Autoville Drive and south of the Mazza property. (TM 25, Grids D-3, Autoville Subdivision, SDAT A21-1010, Block C, Lots 8 and 9)

Discussion:

These two lots adjoin the owners' three undeveloped lots that were placed in the M-U-I Zone, thereby eliminating split zoning of the combined property. Rezoning to M-U-I provides flexible development opportunities consistent with the goals of the plan. These lots were rezoned via Amendment 1 of CR-18-2002.

**Subarea 5A (SA 5A)
No Changes**

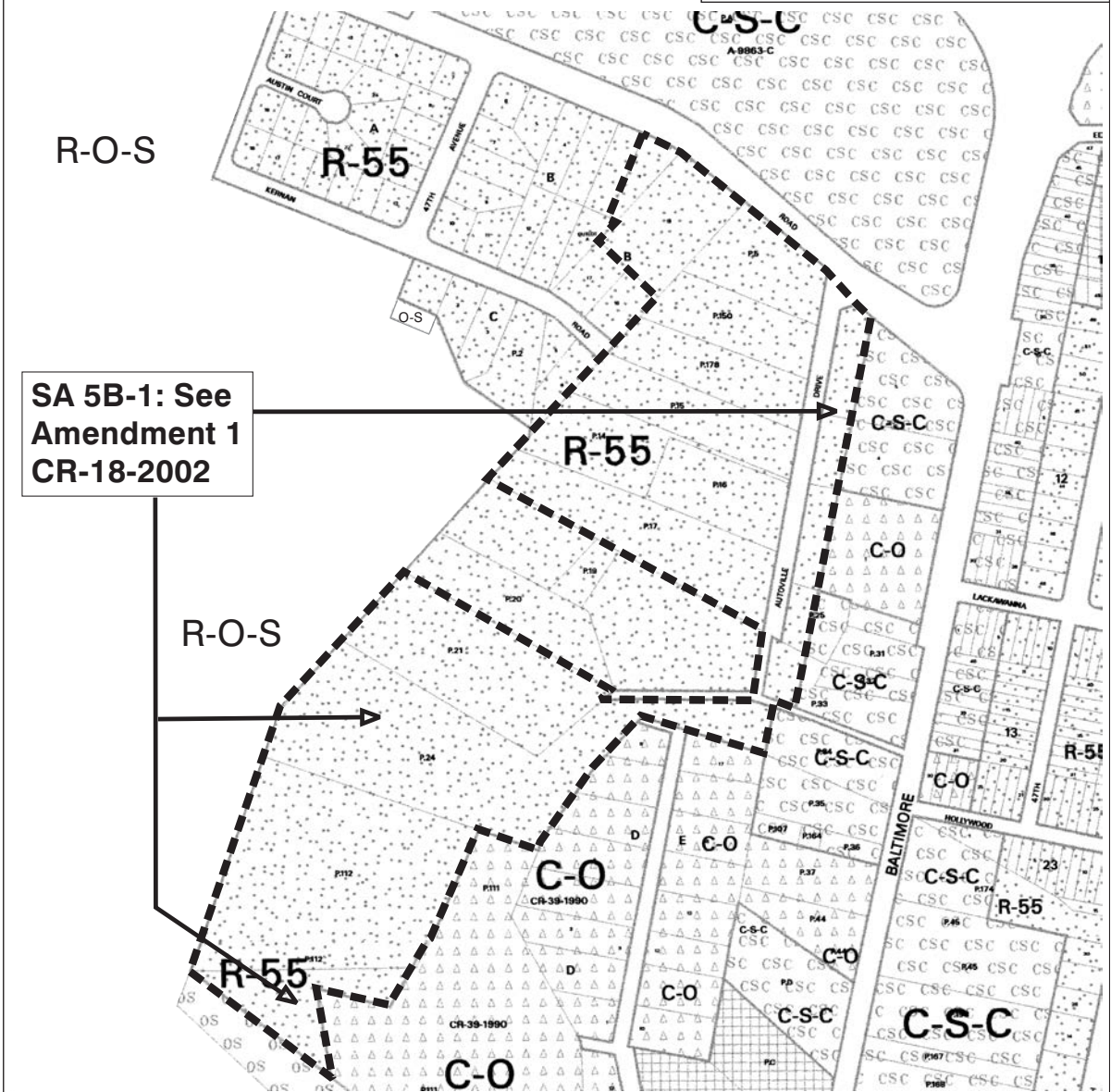
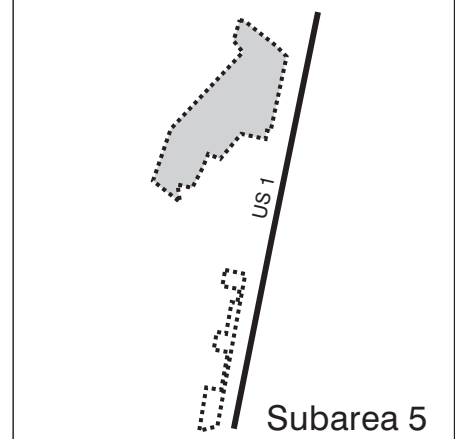


Subarea 5A

Approved Zoning Changes



Subarea 5B (SA 5B)
SA 5B-1 R-55 TO M-U-I



**SA 5B-1: See
 Amendment 1
 CR-18-2002**

Subarea 5B

Approved Zoning Changes



Change Number	Zone Change	Area of Change	Approved SMA/ZAPS/SE Number	Date	200' Scale Index Map
5B-1	R-55 to M-U-I	28.41 ac.	SMA	1990	211NE 4R

Use and Location:

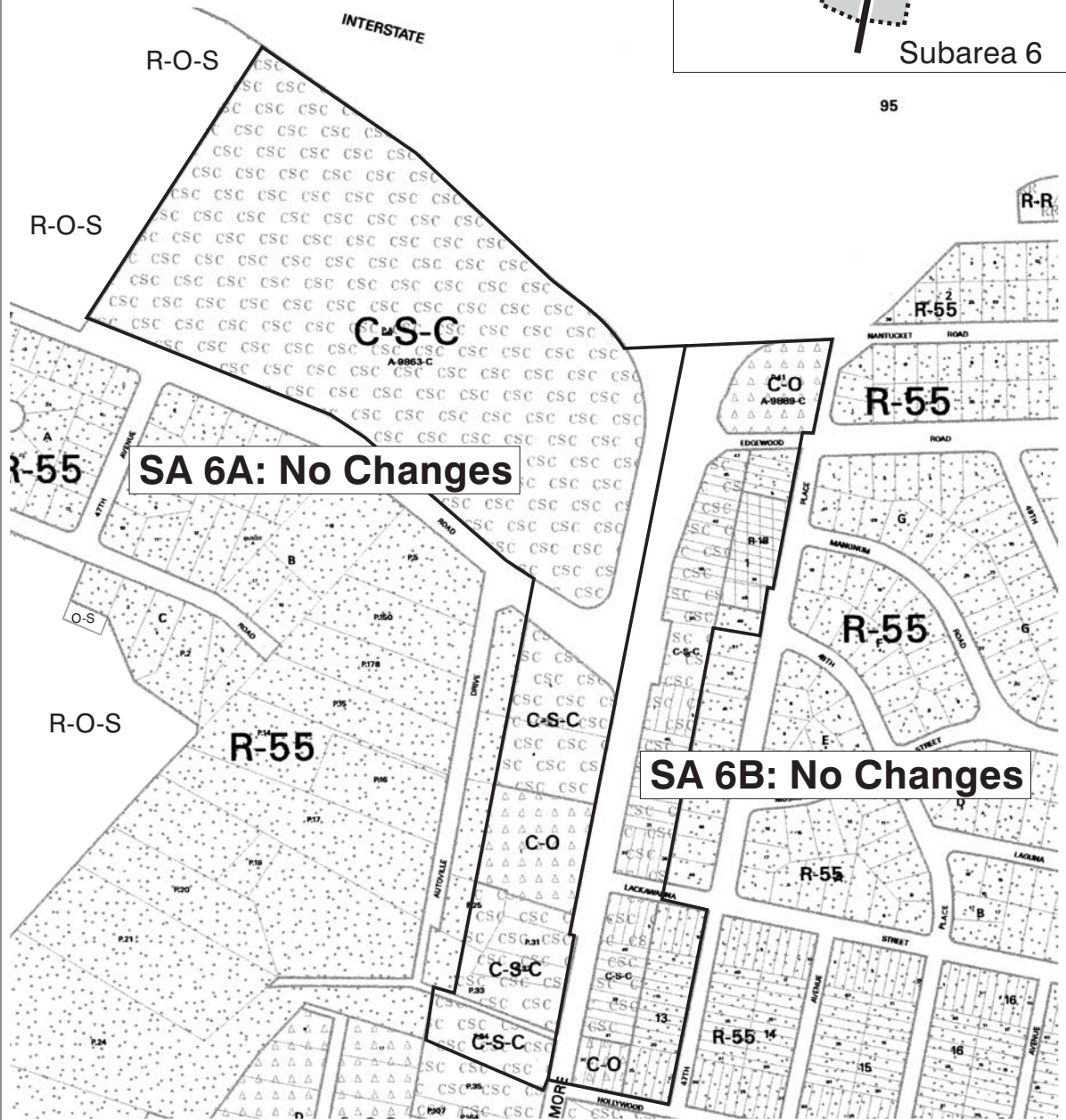
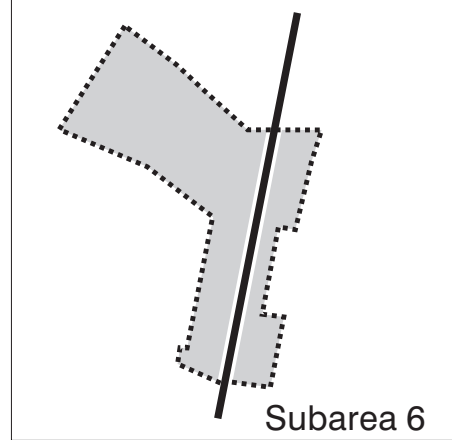
Scattered single-family dwellings on large parcels and a few undeveloped parcels located south of Cherry Hill Road, west of Autoville Drive, and north of, including a portion of, the Mazza property. (TM 25, Grids D-1 and D-2, Cherry Hill Subdivision, Block B, Lot 1; Mozynski Subdivision–resub of Lots 14-16, Block B, Lot 19; Parcels 5, 150, 178, 14-17, 21, 24, 112 and part of 111). Also including a vacant 110±-foot-wide strip of land located along the east side frontage of Autoville Drive North that forms the rear portion of commercially zoned properties that front US 1, including one single-family dwelling. The majority of this strip is maintained as a landscape buffer between commercial uses along US 1 and the existing single-family dwellings in the Autoville Drive North neighborhood. (TM 25, Grids D-2 and E-2, part of Parcels 25 and 33; rear portion of Lots 1, 3, 4 and 5, Plats 21186008 and 21160080)

Discussion:

This relatively large area of scattered single-family homes and undeveloped property, undergoing the consolidation of ownership, can contribute significantly to implementation of mixed-use redevelopment while creating flexible development opportunities consistent with the goals of the plan. The strip of land along the east side of Autoville Drive North is oriented to the M-U-I-zoned area west of Autoville Drive North. Rezoning to M-U-I provides flexible development opportunities consistent with the goals of the plan. All of the lots and parcels described in this change were rezoned via Amendment 1 of CR-18-2002.

Subarea 6A (SA 6A)
No Changes

Subarea 6B (SA 6B)
No Changes



Subarea 6A and Subarea 6B

Approved Zoning Changes



COLLEGE PARK US 1 CORRIDOR DEVELOPMENT DISTRICT STANDARDS

INTRODUCTION

The Development District Overlay Zone (DDOZ) is proposed to be superimposed over the College Park US 1 Corridor, the designated development district, to ensure that the development of land meets the sector plan goals and objectives. The area-wide Development District Standards are specifically intended to address new development and redevelopment proposals in the development district. The standards establish a consistent design framework to ensure quality in future development.

The Development District Standards follow and implement the recommendations in the sector plan and sectional map amendment (SMA) for the College Park US 1 corridor. The standards are organized into three parts, to address public areas, site design and building design for development within the district. Property owners and citizens consulting the standards must also review the sector plan, the Zoning Ordinance and the *Landscape Manual* to have a full understanding of the regulations for property within the district. Tables 13 and 14 summarize the regulatory framework applicable to the six areas within the development district.

SECTOR PLAN SUMMARY

The sector plan sets out goals and objectives and offers a vision for the future development of the corridor. Illustrative concept maps (Map 8-1 to 8-6) show the design intent for each of the six areas of the development district, but the concept maps do not define land use or zoning required for specific properties. Each area has its own specific recommendations to frame land use, circulation, natural features, and urban design issues into a plan for future implementation, revitalization and redevelopment.

The sector plan identifies four primary goals to be implemented through the Development District Standards:

First, to create an attractive and vibrant gateway corridor leading to The University of Maryland and the City of College Park.

Second, to promote quality development by transforming US 1 into a gateway boulevard, main street, and town center in a pedestrian- and bicycle-friendly environment.

Third, to provide a diverse mix of land uses in compact and vertical mixed-use development forms in appropriate locations along the corridor.

Fourth, to encourage multifamily development to reduce the use of the automobile and also expand the opportunity for living, working and studying within the corridor.

The six areas in the development district are (1) Town Center, (2) Open Space Corridor, (3) Main Street, (4) Central Gateway Mixed-Use Area, (5) Autoville Drive Residential Area, and the (6) Gateway North Mixed Commercial Area. Building heights are defined as low-rise (1-3 stories), mid-rise (4-6 stories), and high-rise (7-10 stories). A summary of the development character for each area and subarea from the sector plan is provided below.

TOWN CENTER (AREA 1)—Extends north from Guilford Drive to Paint Branch Parkway and includes Downtown College Park and the East Campus area of The University of Maryland. The vision is to create a pedestrian-oriented, mixed-use town center that is the focus of community activity. A variety of retail/commercial, office, entertainment, residential, and civic uses are encouraged in mid- and high-rise buildings. The street environment will have wide sidewalks with buildings sited close to the street and close to or attached to each other. The primary building entrance will be provided from the street to facilitate convenient pedestrian connections. Vertical mixed-use buildings are encouraged with ground floor retail and either office or multifamily residential located above the first floor. Amenities such as public plazas and

urban open spaces should be integrated into the development of the town center. Parking should be located in lots sited to the side or rear of properties and the use of parking garages and shared parking is strongly encouraged.

- **Subarea 1a (West of Downtown)**—The vision for this subarea is for comprehensive redevelopment including a predominantly residential neighborhood with emphasis on student housing, service retail, and office uses associated with the university.
- **Subarea 1b (West side of US 1)**—The vision for this subarea is for infill and redevelopment including a mix of uses with a retail emphasis fronting on US 1.
- **Subarea 1c (East side of US 1)**—The vision for this subarea is for infill and redevelopment including a mix of uses with a retail emphasis fronting on US 1. Adequate buffers should be provided and building heights should step down to be compatible with the existing surrounding residential neighborhood.
- **Subarea 1d (South of Paint Branch Parkway/East Campus)**—The vision for this subarea is for comprehensive redevelopment with a mix of uses including a hotel/conference center, high tech and general office space, multifamily and student housing, retail, restaurants, and entertainment uses. Direct vehicular and pedestrian connections to Paint Branch Parkway will enhance access and circulation.
- **Subarea 1e (Metro-Related Area)**—The vision for this subarea is for redevelopment with metro-oriented office and high-tech uses, including some convenience retail.

OPEN SPACE CORRIDOR (AREA 2)—Located north of Paint Branch Parkway and west of US 1, including the Paint Branch Stream Valley Park and associated environmental areas. The vision is to preserve this environmentally sensitive greenway except for the introduction of pedestrian trails, boardwalks, stream crossing bridges, and other appropriate passive amenities for a gateway park.

MAIN STREET (AREA 3)—Located on both sides of US 1 between Lakeland and Berwyn Roads. The vision is to create a neighborhood main street district featuring a compact mix of retail, restaurants and offices in low- to mid-rise buildings to meet the demand created by the proximity to the research and engineering facilities of the university. The street environment will have wide sidewalks with buildings sited close to the street and close to or attached to each other. The primary building entrance will be provided from the street to facilitate convenient pedestrian connections. Vertical mixed-use buildings are encouraged, where feasible. Amenities such as public plazas and urban open spaces should be integrated into the development of the main street. The introduction of a one-way service lane with on-street parking adjacent to US 1 is an option to provide for additional short-term retail parking. All other parking should be located in lots sited to the side or rear of properties. Shared parking is strongly encouraged. Floodplain regulations will need to be addressed for development of this area.

- **Subarea 3a (West side of US 1)**—The vision for this subarea is for redevelopment to emphasize office development in proximity to the university. Pedestrian bridges will provide connections to the university over Paint Branch. A rear service road will improve access and circulation throughout this subarea.
- **Subarea 3b (East side of US 1)**—The vision for this subarea is for infill and redevelopment including a mix of uses with a retail emphasis. Adequate buffers should be provided and building heights should step down to be compatible with the existing surrounding residential neighborhood.

CENTRAL GATEWAY MIXED-USE AREA (AREA 4)—Extends north from Berwyn Road to Hollywood Road. The vision is to create a mixed-use area with a variety of retail and office uses, and the introduction of multifamily residential development in mid- and high-rise buildings. Buildings may be sited further from the street and from each other than in the

town center and main street areas. Parking should be located in lots sited to the side or rear of properties. Shared parking is strongly encouraged. Sidewalks set back from the curb edge with trees and landscaping on both sides will create the gateway boulevard envisioned for US 1.

- **Subarea 4a (West side of US 1, south of MD 193/University Boulevard)**—The vision for this subarea is for redevelopment including a mix of retail, office, and residential uses in mid-rise buildings adjacent to the Paint Branch Stream Valley Park.
- **Subarea 4b (East side of US 1, south of Branchville Road)**—The vision for this subarea is for redevelopment including a mix of retail, office, and residential uses in mid-rise buildings. Adequate buffers should be provided and building heights should step down to be compatible with the existing surrounding residential neighborhood. The expansion and improvement of the Veterans Memorial Park is encouraged.
- **Subarea 4c (Southeast corner of US 1/MD 193)**—The vision for this subarea is for redevelopment with primarily multifamily condominiums or apartments in high-rise buildings. Vertical or horizontal mixed-use is encouraged with retail and office uses in addition to residential. Parking structures and shared parking is encouraged.
- **Subarea 4d (West side of US 1, north of MD 193)**—The vision for this subarea is for redevelopment including a mix of retail, office, and residential uses in low-rise buildings. No encroachment of commercial uses is permitted into the Autoville Drive South neighborhood. Primary access to properties will be from US 1. Adequate buffers should be provided and building heights should step down to be compatible with the adjacent existing residential neighborhood.
- **Subarea 4e (East side of US 1, north of MD 193)**—The vision for this subarea is for infill and redevelopment including a mix of retail, office, and residential uses in mid-rise

buildings. Adequate buffers should be provided and building heights should step down to be compatible with the adjacent existing residential neighborhood. Development from Fox Street north to Hollywood Road should be compatible with uses across US 1 in subarea 4f.

- **Subarea 4f (West side of US 1, between Fox Street and Hollywood Road)**—The vision for this subarea anticipates redevelopment as an auto sales and service park, consolidating other uses along the corridor, or alternatively as a mix of commercial uses along US 1 and multifamily residential to the rear, taking advantage of views into the Paint Branch Stream Valley Park. Autoville Drive North is proposed to be extended southward to connect to US 1 at Hollywood Road and Fox Street which will provide additional access to these properties.

AUTOVILLE DRIVE RESIDENTIAL AREA

(AREA 5)—Includes two discontinuous neighborhoods of single-family detached homes. The vision for these two neighborhoods is for residential uses.

- **Subarea 5a (East of Autoville Drive South between Erie Street and University Boulevard)**—The vision for this subarea is for infill housing compatible with the existing single-family detached neighborhood. There is no connection of Autoville Drive South with Autoville Drive North. Adequate buffers should be provided between commercial properties and residential uses. There shall be no expansion of commercial uses into the established single-family residential areas of this subarea.
- **Subarea 5b (South of Cherry Hill Road, west of existing Autoville Drive North)**—The vision for this subarea is for a comprehensively planned residential neighborhood with a mix of single-family attached and multifamily housing types, including limited retail along Cherry Hill Road, and an opportunity to use traditional neighborhood design techniques. A small area of 5b exists as a narrow strip along the east side of Autoville Drive North.

GATEWAY NORTH MIXED-COMMER-CIAL AREA (AREA 6)

–Extends north from Hollywood Road to the Capital Beltway. The vision is to create a mixed-commercial area with a variety of retail, hotel and office uses in mid-rise buildings and a new grade-separated interchange. Buildings may be sited further from the street and from each other than in the town center and main street areas. Parking should be located in lots sited to the side or rear of properties. Shared parking is strongly encouraged. Sidewalks set back from the curb edge with trees and landscaping on both sides will create the gateway boulevard envisioned for US 1.

■ **Subarea 6a (West side of US 1)**–The vision for this subarea is for infill commercial development which complements the College Park Marketplace Shopping Center, the IHOP restaurant, Hampton Inn, and other office uses. The intersection of Autoville Drive North and Cherry Hill Road is proposed to be relocated opposite the existing traffic signal at the shopping center entrance to improve access and circulation.

■ **Subarea 6b (East side of US 1)**–The vision for this subarea is for infill commercial development, if feasible after intersection and road improvements are completed. Adequate buffers should be provided and building heights should step down to be compatible with the existing surrounding residential neighborhood.

APPLICABILITY OF SITE PLAN REQUIREMENTS

New development in the College Park US 1 Corridor Development District is subject to detailed site plan review. New development must show compliance with the Development District Standards in the site plan review process.

Under the Zoning Ordinance, development is any activity that materially affects the condition or use of land or a structure. Redevelopment, rehabilitation, and renovation of existing structures are all forms of development. A change from a lower intensity to a

higher intensity use, as indicated in Table II in Section 4.7 of the *Landscape Manual*, is also a form of development.

The following are exemptions from the Development District Standards and the requirement of site plan review.

1. *Legally existing development.* Until a site plan is submitted, all buildings, structures and uses which were lawful or could be certified as a legal nonconforming use on the date of SMA approval are exempt from the Development District Standards and from site plan review, and are not nonconforming. However, if a permit application is submitted and it is determined that the legally existing building, structure or use has been discontinued for more than 180 days in accordance with Section 27-241(c), it shall comply with all applicable Development District Standards and site plan review.
2. *Legally existing parking and loading.* Until a site plan is submitted, all legally existing parking and loading spaces in the development district which were lawful and not nonconforming on the date of SMA approval are exempt from the DDS standards and site plan review, need not be reduced, and are not nonconforming. The number of parking spaces, loading spaces and landscaping remaining after right-of-way acquisition by the State Highway Administration for the US 1 improvement project will be deemed adequate.
3. *Single-family residential dwellings.* All additions to single-family residential dwellings are exempt from the Development District Standards and site plan review, if the residential use continues.
4. *Multifamily development.* An addition to a multifamily residential structure which was lawful and not nonconforming on the date of SMA approval is exempt from the Development District Standards and site plan review if the addition does not

increase the gross floor area (GFA) by more than 10 percent or 1,000 square feet, whichever is less.

5. *Nonresidential development.* An addition to a nonresidential structure which was lawful and not nonconforming on the date of SMA approval is exempt from the Development District Standards and site plan review, if the addition does not increase the gross floor area (GFA) by more than 20 percent.
6. *Parking facilities.* Resurfacing, restriping, or adding landscaping to parking facilities not required by the standards are exempt from the Development District Standards and site plan review, if the facilities were lawful and not nonconforming on the date of SMA approval, and remain in conformance with all previously applicable regulations.
7. *Nonconforming buildings, structures and uses.*
 - a. Restoration or reconstruction of a nonconforming building or structure, or a certified nonconforming use, is exempt from the Development District Standards and from site plan review if it meets the requirements of Section 27-243(a)(1) of the Zoning Ordinance.
 - b. Except for improvements listed in number 8, a property owner may not expand a certified nonconforming use, or a use or structure which was lawful on the date of SMA approval but does not conform to the Development District Standards, unless a detailed site plan is approved with findings that the expansion is compatible with adjacent uses and meets the goals of the sector plan.
8. *General.* The following are exempt from the Development District Standards and

site plan review if the existing or proposed use is permitted:

- a. Permits for alteration or rehabilitation, with no increase of the existing gross floor area.
 - b. Canopies
 - c. Fences
 - d. Decks
 - e. Ordinary maintenance
 - f. Changes in occupancy
 - g. Changes in ownership
9. *Signs.*
- a. Signs for development that does not otherwise require a detailed site plan will be reviewed in the permit review process for compliance with Development District Standards. Departures for signs which do not comply with the Development District Standards and which do not otherwise require a detailed site plan may be processed by the City of College Park. Signs in a development requiring a detailed site plan will be reviewed in the site plan process.
 - b. New and replacement signs are subject to the Development District Standards.
 - c. Refacing of an existing sign is exempt from the Development District Standards.

SITE PLAN SUBMITTAL REQUIREMENTS

The site plan submittal requirements for the College Park US 1 Corridor Development District are the same as those required by Part 3, Division 9, of the Zoning Ordinance. All site plan applications must include architectural elevations which demonstrate compliance with building design standards. In addition, all site plan applications shall address the area and subarea land use and

urban design recommendations of the College Park US 1 Corridor Sector Plan.

USES

The uses allowed on property in the College Park US 1 Corridor Development District are the same as those allowed in the underlying zone in which the property is classified (R-O-S, O-S, R-55, R-T, R-18, R-10, C-S-C and C-O), except as modified by the use tables contained in the Development District Standards. In addition, the Mixed-Use Infill (M-U-I) Zone permits a mix of residential and commercial uses. Readers consulting the use tables in the Zoning Ordinance will find that the use tables in the Development District Standards section (Tables 17 and 18) modify the Zoning Ordinance use tables by restricting uses, normally by prohibiting a use the Zoning Ordinance permits and by eliminating the need for special exceptions.

DEVELOPMENT DISTRICT STANDARDS

The area-wide standards are organized into three main categories and related topics which are described with text. Each category has a defined objective followed by the required standards. Corresponding illustrations are provided to demonstrate the intent of the standards. There are two types of standards—regulatory standards and performance standards. The regulatory standards are quantitative and modify existing regulations generally contained in the Zoning Ordinance and Landscape Manual. These standards define the character of new development and redevelopment in all six areas within the development district. The performance standards support the regulatory standards and establish a consistent design framework for quality site and building construction.

Modification of the Development District Standards is permitted through the process described in Section 27-548.25(c). "If the applicant so requests, the Planning Board may apply development standards which differ from the approved Development District Standards, unless the Sectional Map Amendment provides otherwise. The Planning Board shall find that the alternate Development District Standards will benefit the development and the development district and will not substantially impair implementation of the Master Plan, Master Plan Amendment, or sector plan." There are three types of amendments which are required to be heard by the District Council: changes to the boundary of the D-D-O Zone; changes to the underlying zones and to the list of permitted uses; and changes to any other specifically designated standard. These amendments may be in the form of a Conceptual or Detailed Site Plan.

As set forth in Section 27-108.01 of the Zoning Ordinance, "The words 'shall,' 'must,' 'may only' or 'may not' are always mandatory and not discretionary. The word 'may' is permissive." "The words 'including' and 'such as' do not limit a term to the specified examples, but are intended to extend its meaning to all other instances or circumstances of like kind or character." The word 'should' is a directive but not necessarily mandatory.

PUBLIC AREAS

- P1. Road Network
- P2. Sidewalks, Bikeways, Trails and Crosswalks
- P3. Street Furniture
- P4. Street Trees
- P5. Lighting
- P6. Utilities

SITE DESIGN

- S1. Vehicular Access
- S2. Parking Areas
- S3. Building Siting and Setbacks
- S4. Buffers and Screening
- S5. Freestanding Signs
- S6. Trees, Plantings and Open Space
- S7. Stormwater Management
- S8. Noise

BUILDING DESIGN

- B1. Height, Scale, Massing and Size
- B2. Roofs
- B3. Architectural Features (Architectural Materials and Details, Window and Door Openings, Awnings, Building Amenities)
- B4. Lighting
- B5. Signs (Attached to a Building or Canopy)
- B6. Building Services
- B7. Former Residential Buildings in Commercial Use

Table 13
Summary of Site Regulations

Note: Users of this document must refer to Development District Standards for an explanation of the summary of information provided within this chart.

SITE REGULATIONS	AREAS					
	1	2	3	4	5	6
Landscape Strip per Landscape Manual	No	N/A	No	Yes	Yes	Yes
Bufferyards per Landscape Manual	May be reduced by 50% with a 6' high wall	N/A	May be reduced by 50% with a 6' high wall	May be reduced by 50% with a 6' high wall	May be reduced by 50% with a 6' high wall	May be reduced by 50% with a 6' high wall
Max. Lot Coverage:						
Single-family detached	60%	N/A	60%	60%	60%	N/A
Single-family attached	50% of net tract area		50% of net tract area	50% of net tract area	50% of net tract area	
Multifamily(less than 4 stories)	60%		60%	60%	60%	
Multifamily(4 or more stories)	70%		70%	70%	70%	
Parking	Min. and max. established. ¹	N/A	Min. and max. established. ¹	Min. and max. established. ¹	Min. and max. established. ¹	Min. and max. established. ¹
Sidewalk Widths	From curb edge to building face ²	N/A	From curb edge to building face ²	5' wide	5' wide	5' wide
Freestanding Sign:						
Max. area	100 S.F.	N/A	100 S.F.	100 S.F.	100 S.F.	100 S.F.
Max. height	8'		8'	12'	12'	12'
Pole-mounted	not permitted		not permitted	not permitted	not permitted	not permitted

¹ The maximum off-street parking spaces permitted is equal to the minimum requirements in the Zoning Ordinance. The minimum off-street parking spaces permitted is reduced 10 percent from the maximum.

² Streets with single-family detached/attached dwellings have a five-foot-wide sidewalk.

Table 14
Summary of Building Regulations

BUILDING REGULATIONS	AREAS					
	1	2	3	4	5	6
Front Build-to Line	12"-12' behind R-O-W line	N/A	12"-12' behind R-O-W line	10'-20' behind R-O-W line	10'-20' behind R-O-W line	10'-20' behind R-O-W line
Building Facade along US 1 Street Frontage	70% min. ¹	N/A	70% min.	50% min.	50% min.	50% min.
Building Height	4 - 8 stories ²	N/A	5 stories	3 - 8 stories ²	3 - 5 stories ²	5 stories
Architecture/Min. Size:		N/A				N/A
Single-family detached	2,200 S.F.		2,200 S.F.	2,200 S.F.	2,200 S.F.	
Single-family attached	1,600 S.F.		1,600 S.F.	1,600 S.F.	1,600 S.F.	
Multifamily	min. average size		min. average size	min. average size	min. average size	
Display Windows for Retail Storefronts	40-80% of a building's street frontage. ¹	N/A	40-80% of a building's street frontage	N/A	N/A	N/A
Window Signs	25% max. of area	N/A	25% max. of area	25% max. of area	25% max. of area	25% max. of area

¹ In subareas 1b, 1c and 1d only.

² Heights vary by subarea. See B1. Height, Scale, Massing and Size., and Building Heights map

PUBLIC AREAS PI. ROAD NETWORK

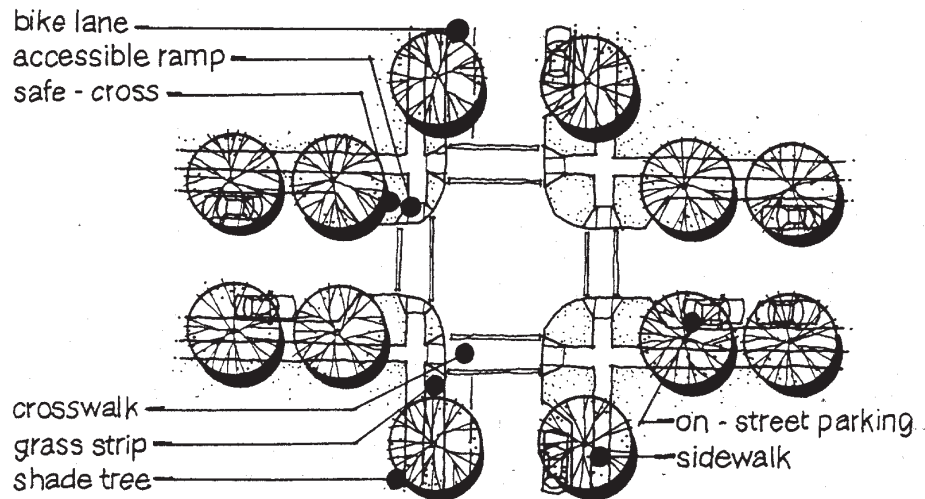
OBJECTIVE

To provide a multimodal circulation system within the development district which will stimulate development, the use of Metro and other transit within a network of interconnected streets which are user-friendly for pedestrians and bicyclists and also accommodate motorists.

DESIGN STANDARDS

- A. Development should, where possible, provide for on-street parking.¹
- B. Cul-de-sacs as the terminus to streets shall be avoided. Cul-de-sacs may be used when developable land is surrounded by environmentally sensitive features.
- C. Intersections in areas of new development within the development district should employ "safe-crosses" on streets which provide on-street parking. This treatment enhances pedestrian safety by expanding the sidewalk area in the unused portion of the on-street parking lane adjacent to the intersection.
- D. Vehicular access to commercial properties from proposed Autoville Drive North - Extended (60-foot right-of-way) shall be permitted to provide alternative access and minimize turning movements onto US 1. Safe crosses and other traffic calming techniques should be incorporated in the construction of this roadway.¹

INTERSECTION WITH SAFE-CROSS TREATMENT

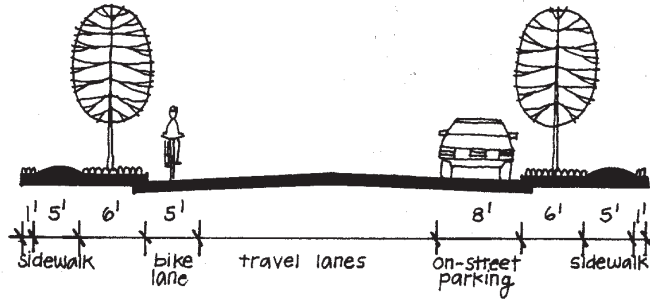


¹ Requirements shall be coordinated with appropriate public agencies.

PUBLIC AREAS PI. ROAD NETWORK

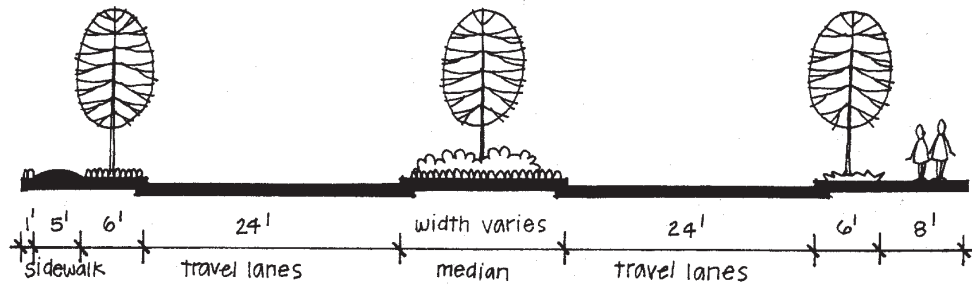
STREET SECTIONS

AUTOVILLE DRIVE NORTH - EXTENDED 60' R-O-W



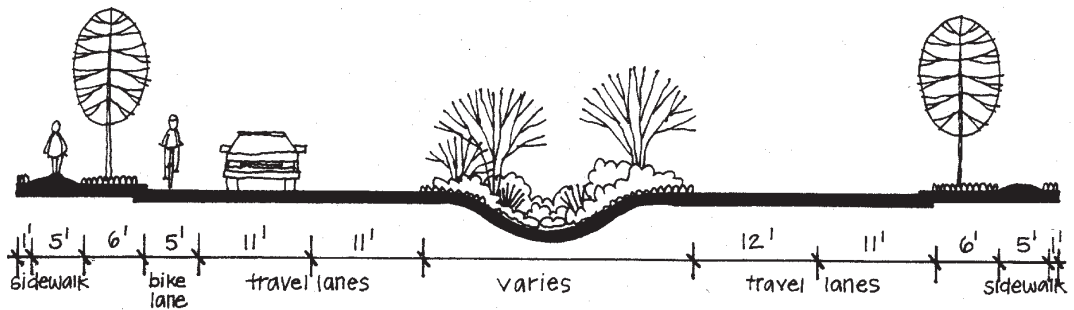
shade trees located in 6-foot-wide grass strip, typ.

PAINT BRANCH PARKWAY 90' R-O-W



shade trees located in 6-foot-wide by 8 foot-long area

GUILFORD DRIVE 100' R-O-W



Note: See Street Edge Types Table for tree planting and sidewalk treatments.

PUBLIC AREAS

P2. SIDEWALKS, BIKEWAYS, TRAILS AND CROSSWALKS

OBJECTIVE

To encourage alternative modes of transportation to the automobile by creating safe opportunities for walking and bicycling. To provide a continuous system of sidewalks and crosswalks with convenient trail connections. To establish a comfortable and inviting pedestrian-oriented environment within the development district.

DESIGN STANDARDS

- A. All roads within the development district shall have a continuous system of sidewalks on both sides of the street. Refer to the Street Edge table (Table 16) in S3. Building Siting and Setbacks for the width of new sidewalks in the development district.²
- B. Sidewalks along Baltimore Avenue shall be compatible in materials to provide a consistent appearance throughout the entire US 1 corridor. Brick, concrete pavers, poured-in-place concrete or other similar materials should be utilized along US 1 and other streets within the development district. Asphalt shall not be used as a paving material for sidewalks.²
- C. All sidewalks shall have accessible ramps and comply with Americans with Disabilities Act (ADA) regulations.²
- D. A five-foot-wide sidewalk adjacent to a private access or service road shall be furnished if no other alternative pedestrian connections are provided.
- E. Crosswalks shall be provided at all intersections along US 1 and Paint Branch Parkway within the development district. Crosswalks at primary intersections shall be constructed of interlocking concrete pavers. Crosswalks at secondary intersections shall have striped markings in the pavement. Crosswalk materials for primary intersections shall be consistent along Baltimore Avenue and Paint Branch Parkway. Primary intersections are all intersections with existing and proposed traffic signals on Baltimore Avenue and Paint Branch Parkway. All other intersections are secondary.² All signalized intersections shall have pedestrian crossing signals.
- F. Connections to the trail network shall be provided from the sidewalk system at designated locations throughout the development district. These locations shall be identified with appropriate signs. (See Pedestrian Circulation Map 10.)²
- G. Internal pedestrian ways shall be clearly marked and distinguished from vehicular travelways and shall also connect to the external sidewalk system on US 1.
- H. In naturally vegetated areas, underbrush and other vegetation shall be kept clear of trails and bikeways to provide comfortable sight distances and allow ample time to perceive any hazards. Landscaping that creates hiding places near trails and bikeways shall be avoided.

² Requirements shall be coordinated with appropriate public agencies.

PUBLIC AREAS

P2. SIDEWALKS, BIKEWAYS, TRAILS AND CROSSWALKS

An alternate, more visible route should be provided when trails and bikeways lead into wooded areas.

- I. Bicycle parking facilities shall be located in highly visible and well-lit areas.
- J. The location and number of bicycle racks, lockers and other features shall be determined at site plan review.
- K. All new retail and office development shall provide a minimum of two bicycle parking spaces per 10,000 square feet of gross floor area. Covered (open-air) bicycle parking spaces should be provided for mixed-use development where feasible.

PUBLIC AREAS

P3. STREET FURNITURE

OBJECTIVE

To create a positive identity and a pedestrian-friendly street environment, a compatible group of street furniture should be located along Baltimore Avenue and other appropriate locations within the development district. This furniture includes, but is not limited to, benches, trash receptacles, bicycle racks, light fixtures, banners, bus shelters, kiosks, planters and bollards.

DESIGN STANDARDS

- A. Street furniture shall be constructed of durable materials and require minimal maintenance.
- B. Development should provide benches, trash receptacles and bicycle racks in strategic locations, such as public plazas and within retail/commercial activity areas.
- C. Benches, trash receptacles and bicycle racks should be provided along trails, in high pedestrian activity areas, urban parks and bus stops along US 1.³

³ Requirements shall be coordinated with appropriate public agencies.

PUBLIC AREAS

P4. STREET TREES

OBJECTIVE

To enhance the visual character of the exterior environment with the introduction of street trees along the major road corridors and on individual parcels in the development district. Trees serve several aesthetic functions, such as defining edges and spaces, directing views, strengthening vistas and providing buffers between incompatible uses. Environmental functions served by trees include reduction in ambient air and surface temperatures, reductions in ground-level ozone, and improvements to air quality.

DESIGN STANDARDS

- A. Street trees shall be used along both sides of all roadways within the development district to define the street edge, provide a shaded overhead canopy and establish an unifying element to the street environment.⁴
- B. Medium to large deciduous shade trees shall be utilized for street trees, and shall be planted between 30 and 40 feet on center. Street trees shall be installed at a minimum height of 12 feet and 2½ inch caliper.⁴
- C. Ornamental and flowering trees shall be planted instead of deciduous shade trees (street trees) in areas with existing or proposed overhead obstructions. Ornamental and flowering trees shall be installed at a minimum height of eight feet and with an ultimate height of between 20 and 40 feet.⁴
- D. A limited tree palette shall be selected for each roadway to provide consistency, uniformity and a distinct identity to the major roads within the development district.⁴
- E. Plant selection for street trees shall consider the following characteristics: shape of canopy, sun/shade tolerance, overhead utility lines, drought tolerance, maintenance requirements and tolerance of adverse urban conditions. Native tree species are strongly recommended.⁴
- F. The minimum planting area for street trees shall be six feet in width, eight feet in length and four feet deep. Wherever possible, the tree planting areas below the sidewalk paving should be connected so that root zone space for trees can be shared.⁴

⁴ Requirements shall be coordinated with appropriate public agencies.

PUBLIC AREAS

P5. LIGHTING

OBJECTIVE

To assist in creating a distinct identity along the US 1 corridor by introducing the use of ornamental street lighting. Exterior lighting should enhance the visual appearance, as well as contribute to user safety and improved nighttime visibility. New and existing light fixtures should use energy-efficient lamps. The use of cut-off shields and timers is also encouraged to reduce light spill-over and usage.

DESIGN STANDARDS

- A. Pole-mounted light fixtures shall effectively illuminate all streets and sidewalks within the development district.⁵
- B. One consistent type of ornamental pole and luminaire should be used along US 1 and Paint Branch Parkway.⁵
- C. At the time of the first detailed site plan submission in the main street (3a and 3b) and town center (1a, 1b, 1c, 1d and 1e) subareas, the applicant shall consult with the City of College Park and Planning Department staff and propose for Planning Board approval one type of ornamental pole and luminaire for all new development projects within these areas.⁵
- D. Light levels from nonresidential uses shall not negatively impact adjacent residential properties and neighborhoods. Within the development district, light fixtures shall incorporate internal cut-off shields to direct light to intended areas and reduce light spill-over to adjacent properties.

⁵ Requirements shall be coordinated with appropriate public agencies.

PUBLIC AREAS P6. UTILITIES

OBJECTIVE

To reduce the visual impact of existing overhead utility lines and associated poles along Baltimore Avenue within the development district by consolidating utility pole usage, relocating utility poles, or placing existing utility lines underground.

DESIGN STANDARDS

- A. All new development within the development district shall place utility lines underground. Utilities shall include, but are not limited to, electric, natural gas, fiber optic, cable television, telephone, water and sewer service.

SITE DESIGN

SI. VEHICULAR CIRCULATION/ACCESS

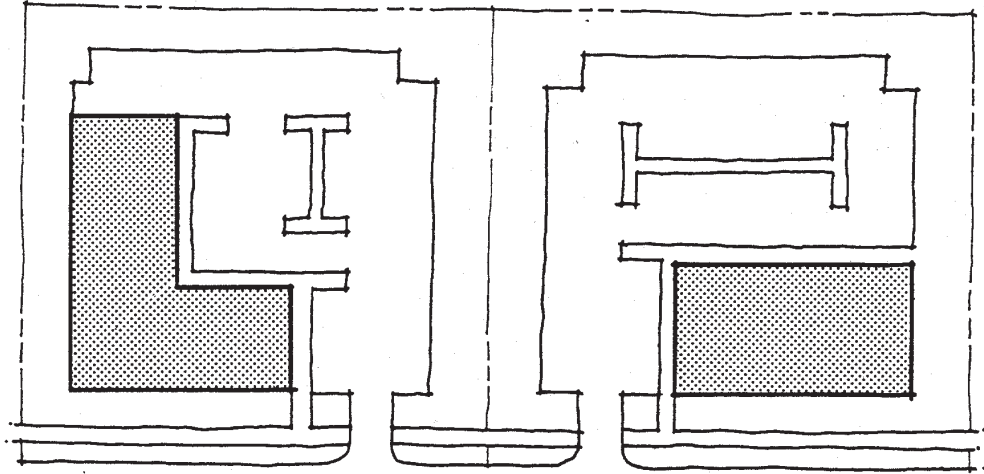
OBJECTIVE

To provide access to businesses/properties that is clearly defined and safe for motorists and pedestrians. The circulation pattern within parking lots shall be designed to facilitate clear vehicular movement and to ensure safe and convenient pedestrian access from parked cars to building entrance(s).

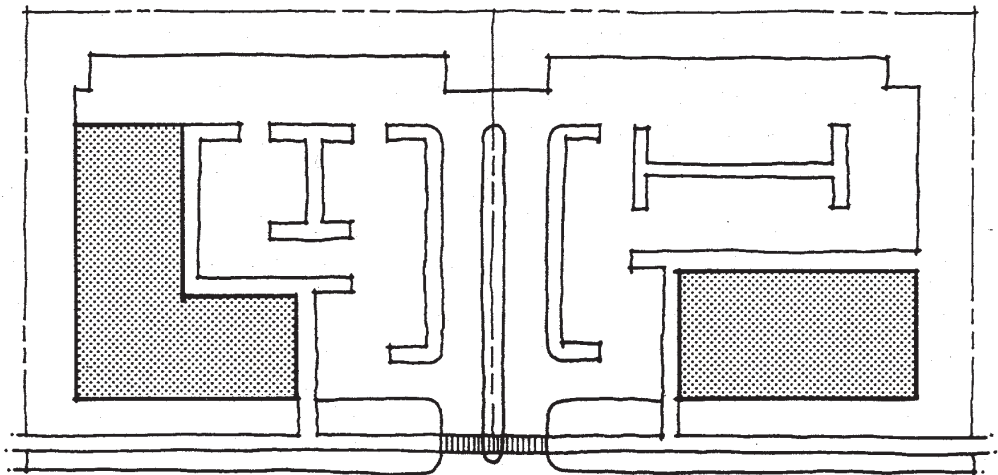
DESIGN STANDARDS

- A. Common, shared entrances (curb cuts) shall be utilized for access to nonresidential property, wherever feasible, instead of individual entry points to each property. The amount of curb cuts used for all new development shall be minimized throughout the development district. The need for more than one curb cut from US 1 shall be demonstrated for each new development proposal. Vehicular access from side streets (perpendicular to US 1) should be utilized.
- B. Vehicular entrance drives shall permit safe and clear pedestrian crossings. Sidewalk material(s) should continue across driveway aprons.
- C. The width of entrance drives should be visually minimized, where appropriate, by the provision of a planted median of at least six feet in width separating incoming and outgoing traffic, especially if two or more lanes are provided in each direction.
- D. In an attached row or group of buildings in a block, the number of vehicular connections from the front to the rear of the property shall be minimized.
- E. All new drive-in or drive-through windows for any use are prohibited in Subareas 1 and 3. However, any existing drive-in or drive-through windows in Subarea 3 that must be removed because of governmental action shall be considered a permitted use and shall be exempt from the Development District Standards and site plan review.

SITE DESIGN
SI. VEHICULAR CIRCULATION/ACCESS



DON'T use multiple or separate entrances.



- DO** use shared or common entrances.
- DO** provide a pedestrian connection across entry drives.
- DO** separate traffic flow with a planted median.

SITE DESIGN

S2. PARKING AREAS

OBJECTIVE

To reduce the visual impact of parked cars in parking lots adjacent to all roadways with plantings and walls. To provide parking credits and incentives which will encourage the use of shared parking facilities for two or more adjacent land uses. To promote alternative modes of transportation other than single occupancy vehicles which will reduce the number of cars and parking spaces on the US 1 corridor. The use of parking garages is also strongly encouraged in the main street and town center areas of the development district. The use of pervious pavement for parking areas should be explored to reduce the amount of surface run-off.

DESIGN STANDARDS

Parking Lots

- A. Surface parking lots shall be located to the side or rear of buildings to reduce the visual impact of parked cars and large expanses of asphalt adjacent to public streets. The number of parking spaces located between buildings and the street frontage of roadways shall be minimized.
- B. Overflow parking areas and areas required for emergency access only shall be constructed with pervious pavement.
- C. Landscaping, screening and buffering of all parking lots and parking garages within the development district shall comply with the provisions of the *Landscape Manual*, except as modified by the standards for the development district.
- D. In the main street (3a and 3b) and town center (1a,1b,1c,1d and 1e) subareas on-site parking lots/spaces which are located adjacent to the right-of-way line or curb edge due to site constraints shall be screened from adjacent roadways with a continuous, low masonry wall.

A minimum of a six-foot-wide landscape strip shall be provided between the right-of-way line and the parking lot. The wall should be between 36 and 42 inches in height and be faced on both sides with a masonry veneer. A masonry veneer may be constructed of brick, stone, precast concrete panels, split-face concrete masonry units or an equivalent material. Unfinished concrete block or poured-in place concrete are not acceptable materials. The low masonry wall shall be compatible in materials and design with associated buildings.

One shade tree per 35 linear feet of frontage, excluding driveway openings, shall also be provided. The landscape strip shall also contain a mixture of evergreen ground cover and low shrubs planted between the shade trees. Shrubs shall be installed at a minimum of 12 inches in height. Two ornamental trees may be substituted for the one shade tree in areas with overhead obstructions only.

- E. In areas 4, 5 and 6, Parking Lot Landscape Strip requirements shall comply with either Option 1, Option 3 or Option 4 (as modified by the standards for the development district) in the *Landscape Manual*.

SITE DESIGN

S2. PARKING AREAS

- F. Parking lots utilizing berms are not permitted, as illustrated in Parking Lot Landscape Strip, Option 2 of the *Landscape Manual*.
- G. Parking lots shall provide uniform light distribution, without any dark areas. Internal pedestrian ways shall be well-illuminated and clearly delineated within parking lots.
- H. Single, large surface parking lots are not permitted. Instead, parking should be provided in smaller defined areas separated by planted medians. The arrangement and design of long, straight aisles within parking areas should be avoided to reduce the opportunities for increased speed of vehicles.
- I. The placement of parking lots shall avoid creating isolated and remote areas. Parking areas should be clearly visible from adjacent windows, doorways and walkways to maximize natural surveillance.
- J. Wheel stops shall be provided to prevent damage to buildings, landscaping and overhangs of vehicles blocking pedestrian or vehicular access. Materials for wheel stops include concrete or recycled materials; timber wheel stops are not permitted.
- K. All parking lots shall be in compliance with the Americans with Disabilities Act (ADA).
- L. Innovative solutions for parking of retail, office and multifamily housing should be explored to reduce the amount of land area needed for surface parking lots. These options include parking at grade underneath the building, and/or parking a half level or one full level below grade.

Parking Garages

- M. Parking garages shall not dominate the street edge and shall incorporate architectural design or landscape features to screen parked vehicles from passing pedestrians and motorists.
- N. Parking garages shall be an integral component with the buildings and structures located on a parcel or property and shall incorporate similar high-quality building materials, color(s) and massing. The height of the parking garage shall not exceed the height of the adjacent buildings on the property. Whenever possible, the parking structure should be located in the interior of the parcel.
- O. The facade of a parking garage should not be sited directly on US 1 throughout the development district. If no other location is feasible, the facade(s) sited on US 1 shall be mitigated through innovative architectural facade treatments which enhance the pedestrian environment.
- P. Convenient, well-marked and illuminated pedestrian connections shall be provided between parking garages and adjacent buildings/destinations.

SITE DESIGN

S2. PARKING AREAS

- Q. The use of parking garages should be explored in the main street (3a and 3b) and town center (1a, 1b, 1c, 1d and 1e) subareas. Parking garages with compatible uses should be considered as part of a mixed-use development.

Adequacy of Transportation Facilities

- R. The transportation facilities adequacy standard shall be Level-of-Service E, based on the average peak period levels of service for all signalized intersections in the three designated segments of the US 1 corridor. These segments are (1) Capital Beltway to MD 193, (2) MD 193 to Paint Branch Parkway/Campus Drive, and (3) Paint Branch Parkway/Campus Drive to Guilford Drive.

Off-Street Surface Parking Requirements for all Development (except Mixed-Use Development Projects)

- S. The maximum number of off-street surface parking spaces permitted for each land use type shall be equal to the minimum number of required off-street parking spaces in accordance with Section 27-568(a) of the Zoning Ordinance.
- T. The minimum number of off-street surface parking spaces permitted for each land use type shall be reduced 10 percent from the minimum number of required off-street parking spaces in accordance with Section 27-568(a) of the Zoning Ordinance.

Off-Street Parking Requirements for Mixed-Use Development Projects

- U. The minimum and/or maximum number of parking spaces required for a mixed-use development project which contains a minimum of five adjoining gross acres and two or more uses shall be calculated by the applicant in accordance with the requirements of Section 27-574 of the Zoning Ordinance and submitted with a detailed site plan.

Parking Credits

Reductions to the required minimum number of off-street parking spaces as described in (T) above, may be approved if such reductions are in accordance with the requirements described in the following text and Table 15. An applicant may be granted one or more credits. Where multiple credits are granted, after an initial reduction is applied for the first credit, additional reductions for each successive credit shall apply to the balance of the off-street parking spaces required.

- V. *Parking Credits for Shared Parking*—To facilitate shared parking within the development district, Section 27-570, Multiple Uses, and Section 27-572, Joint Use of a Parking Lot, shall be waived.
1. *Single Ownership*. When any land and/or building is under the same ownership and used for two or more uses, the number of parking spaces shall be computed by multiplying the minimum amount of parking required for each land use, as stated under section (T) above, by the appropriate percentage as shown in the shared parking requirements by time period (Table 15). The number of parking spaces required for the development is then determined by adding the results in each column. The column

SITE DESIGN
S2. PARKING AREAS

totaling the highest number of parking spaces becomes the minimum off-street parking requirement.

2. Multiple Ownership. The off-street parking requirements for two or more uses with different ownership may be satisfied by providing a joint parking facility, and the minimum parking requirements may be reduced in accordance with the procedure outlined above for shared parking for Single Ownership, provided:
 - a. The Planning Board shall determine that shared parking is appropriate for the proposed uses and location.
 - b. The shared parking facility is within 500 linear feet, measured along the most appropriate walking routes between the shared parking facility and the entrances to all establishments being served.
 - c. The applicant provides a recorded shared-use parking agreement signed by all owners involved which ensures the shared parking facility will be permanently available to all current and future uses and also contains a provision for parking facility maintenance.

Table 15
Shared Parking Requirements by Time Period

Type of Use	Weekday		Weekend		Night-time
	Daytime 6 a.m.– 6 p.m.	Evening 6 p.m. -midnight	Daytime 6 a.m. - 6 p.m.	Evening 6 p.m. -midnight	midnight–6 a.m.
Office/Industrial/ Manufacturing /Storage	100%	10%	10%	5%	5%
Commercial Trade	60%	90%	100%	70%	5%
Restaurant	50%	100%	100%	100%	10%
Lodging	70%	100%	70%	100%	70%
Recreational/Entertainment/ Social/ Cultural	40%	100%	80%	100%	10%
Residential	60%	90%	80%	90%	100%
Other Uses	100%	100%	100%	100%	100%

Source: Table based on Shared Parking, a publication from the Urban Land Institute, Washington, D.C., 1983.

W. Parking Credits For Use of Alternative Modes of Transportation

1. Applicants may request from the Planning Board during the site plan review process a reduction in the minimum off-street parking requirements if they provide incentives to encourage use of alternative modes of transportation other than single occupancy vehicles. These alternatives include contributing to the county and/or city ride sharing program, providing private incentives for car- and vanpooling, participating in usage

SITE DESIGN

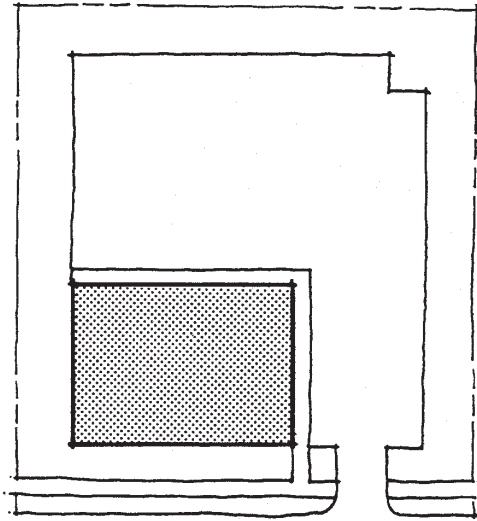
S2. PARKING AREAS

of public transportation programs such as WMATA's *Metrochek* and MTA's *TransitPlus 2000*, or provision of private shuttle bus service. Verifiable data must be produced that supports the desired reductions in the minimum off-street parking. The reduction shall range between 5 and 20 percent.

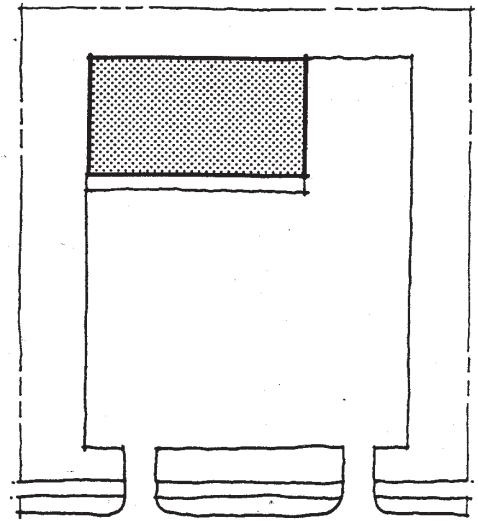
Parking Requirements for all Development Within the Main Street and Town Center

- X. To encourage the construction of off-street structured parking facilities in the main street and town center (areas 1 and 3) and to promote economic development, the Planning Board during the site plan review process or the Planning Department during a permit review process may waive the minimum off-street parking requirements provided that:
 - 1. A Parking District is established for the area in accordance with requirements of Subtitle 2, Division 27, Sections 2-399 to 2-413 of the Prince George's County Code.
 - 2. The applicant agrees to pay a fee-in-lieu for the required number of off-street parking spaces that are to be waived and/or a special assessment as defined by the Parking District.
 - 3. The developer has a written agreement with the university that student tenants in the building will be permitted to purchase parking permits to park on The University of Maryland campus under current student parking policy as such as may be changed from time to time.

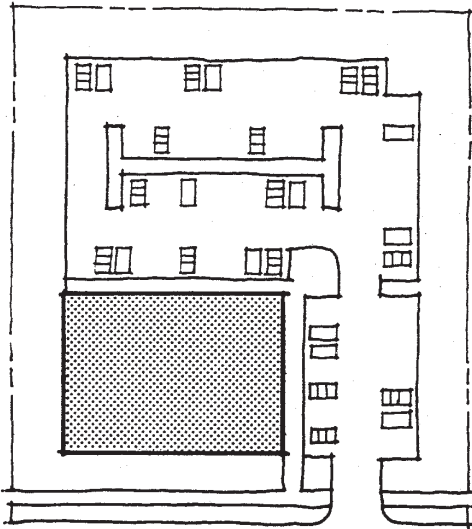
SITE DESIGN S2. PARKING AREAS



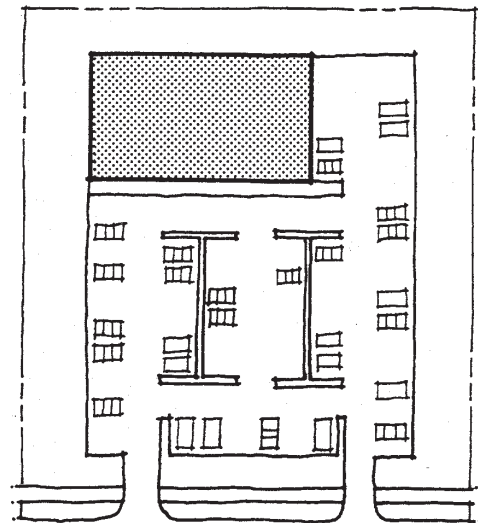
DO locate parking lots to the side or rear of a building.



DON'T locate parking lots between a building and the street.

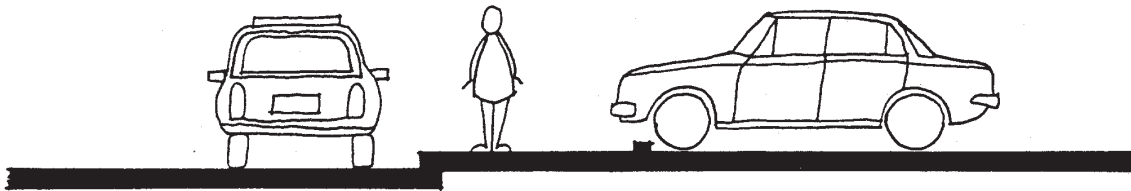


DO separate parking lots with planted medians and islands.

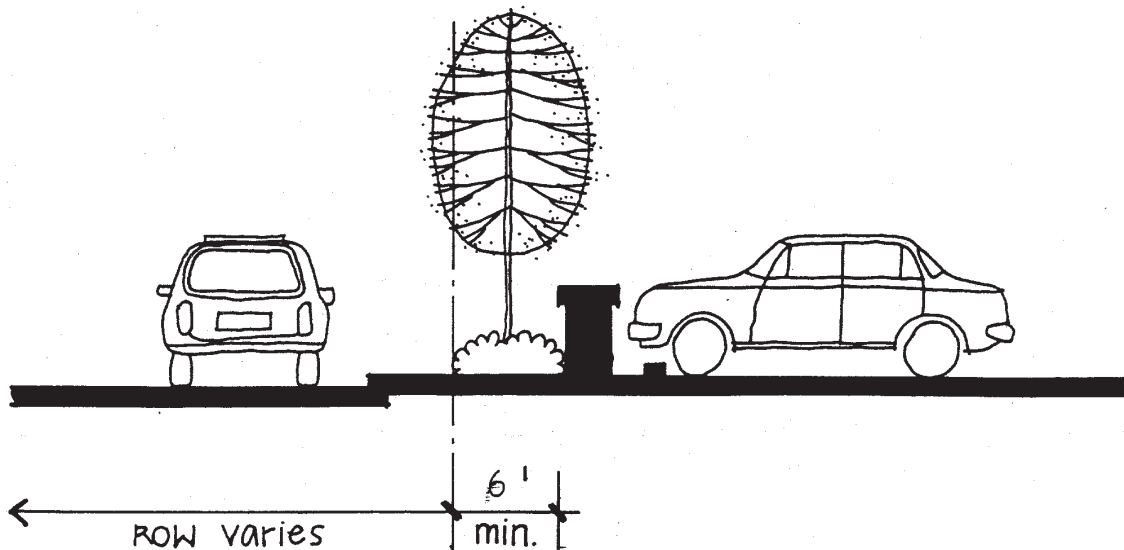


DON'T use one large parking lot.

SITE DESIGN S2. PARKING AREAS



DON'T locate parking spaces adjacent to roadways and public areas without adequate buffers.



DO screen parked cars from roadways and public areas with a landscape strip containing a low masonry wall.

SITE DESIGN

S3. BUILDING SITING AND SETBACKS

OBJECTIVE

To provide a consistent setback close to the right-of-way line or street edge within an attached row or block of mixed-use, multifamily or commercial buildings. Setbacks should maintain a continuous building edge to define the public zone of the street. This defined and close edge enlivens commercial areas by encouraging window shopping and streetside activity. Residential building setbacks from public streets should be minimized to encourage a more active street environment.

DESIGN STANDARDS

A. Buildings shall be oriented to and face the street edge along the entire US 1 frontage. Buildings should also be oriented to Paint Branch Parkway in subareas 1d and 1e. The primary entrance to a building shall be clearly visible from the street. Prominent entrances are encouraged for architectural interest and as an element of scale and orientation. The primary building entrance accessed from interior-facing parking lots should be avoided. Secondary rear and/or side building entrances shall be provided if served by an adjacent parking area. These entrances shall be inviting, well-lit and clearly articulated.

B. A front build-to line between 12 inches and 12 feet from the ultimate right-of-way shall be established for all buildings or storefronts which are located in the main street (3a and 3b) and town center (1a, 1b, 1c, 1d and 1e) subareas (see Table 16). The build-to line shall be compatible with all storefronts or buildings within an attached row or a group of buildings in a block. The Commercial and Industrial Landscape Strip requirements (Section 4.2) of the *Landscape Manual* shall not apply. See Type I Street Edge.

Existing building setbacks which are compatible with the front build-to line standards (Type I Street Edge) shall be utilized along US 1 in locations where infill redevelopment is proposed in the town center (1b and 1c) subareas.

C. A front build-to line between 10 and 20 feet from the ultimate right-of-way shall be established for all buildings in areas 4, 5 and 6. See Type II Street Edge.

D. Building facades shall occupy a minimum of 70 percent of a property's street-facing frontage on US 1 in the main street (3a and 3b) and town center (1b, 1c and 1d) subareas (measured in linear feet). Building facades in the remainder of the development district shall occupy a minimum of 50 percent of a property's street-facing frontage on US 1. Parking may be provided in front of the building facade only if it is one parking row wide for either parallel or angle-in, short-term parking.

E. To achieve a continuous building edge in the main street (3a and 3b) and town center (1b, 1c and 1d) subareas along US 1, side yards between adjoining nonresidential development shall be minimized to the greatest extent possible.

F. Streets which intersect with US 1 within the development district should have compatible street edge treatments.

G. Residential buildings shall front onto public streets, whenever possible.

SITE DESIGN
S3. BUILDING SITING AND SETBACKS

- H. Single-family detached residential garages shall be sited to reduce their visual impact on the street. Alternatives should be pursued which locate the garage toward the side or rear of a lot, or at a minimum recess the garage at least six feet from the front building facade.
- I. The maximum lot coverage for single-family detached dwelling units shall be 60 percent.
- J. The maximum building coverage for single-family attached dwelling units shall be 50 percent of the overall net tract area.
- K. The maximum lot coverage for multifamily dwellings having less than 4 stories shall be 60 percent of the overall net lot area.
- L. The maximum lot coverage for multifamily dwellings having 4 or more stories shall be 70 percent of the overall net lot area.

Table 16
Street Edge Types

	Location	Front Build-to Line (behind ultimate R-O-W)	Compliance with Landscape Strip Requirements (Section 4.2 of the Landscape Manual)	Tree Planting and Sidewalk Treatment
Type I Street Edge	Main street (3a and 3b) and town center (1a, 1b, 1c, 1d and 1e) ^{1,2,3}	12 inches to 12 feet behind R-O-W	No	6' wide x 8' long x 4' deep area for trees located at back of curb. Consistent paving material from curb edge to building face.
Type II Street Edge	Areas 4, 5 and 6	10 to 20 feet behind R-O-W	Yes	6' wide grass strip for trees. 5' wide sidewalk.

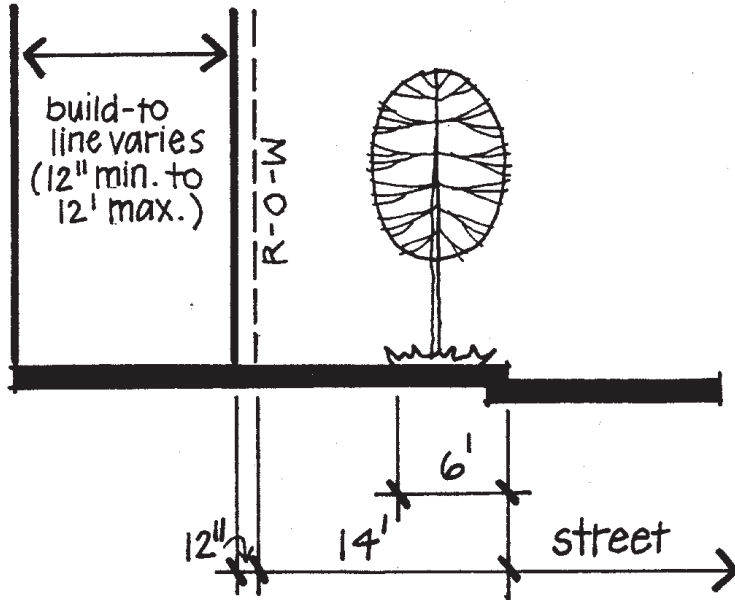
¹Guilford Drive has a Type II Street Edge.

²Paint Branch Parkway has a Type I Street Edge on the south side of the street and a Type II Street Edge on the north side of the street.

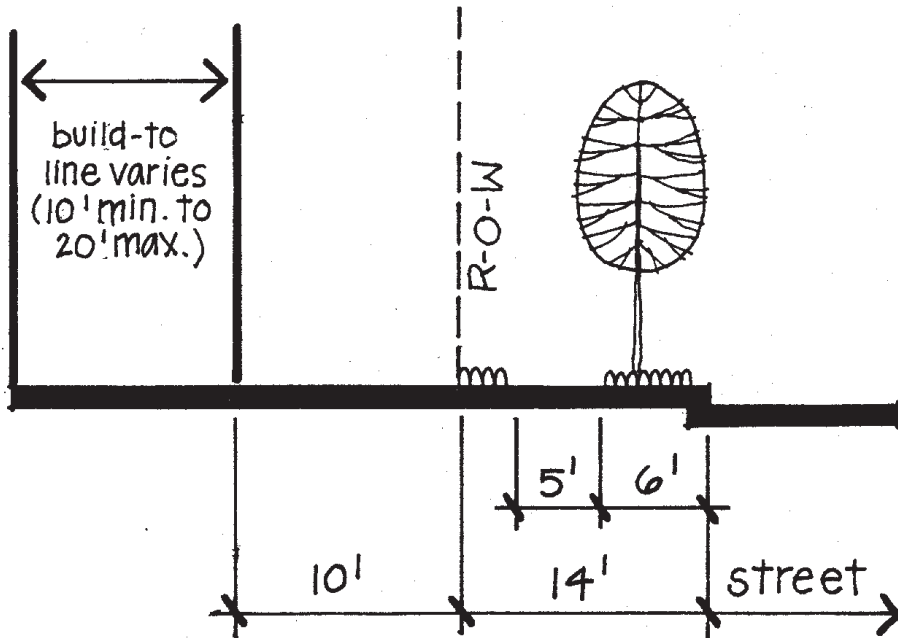
³Streets with single-family detached/attached dwellings have a Type II Street Edge.

SITE DESIGN
S3. BUILDING SITING AND SETBACKS

TYPE I STREET EDGE



TYPE II STREET EDGE

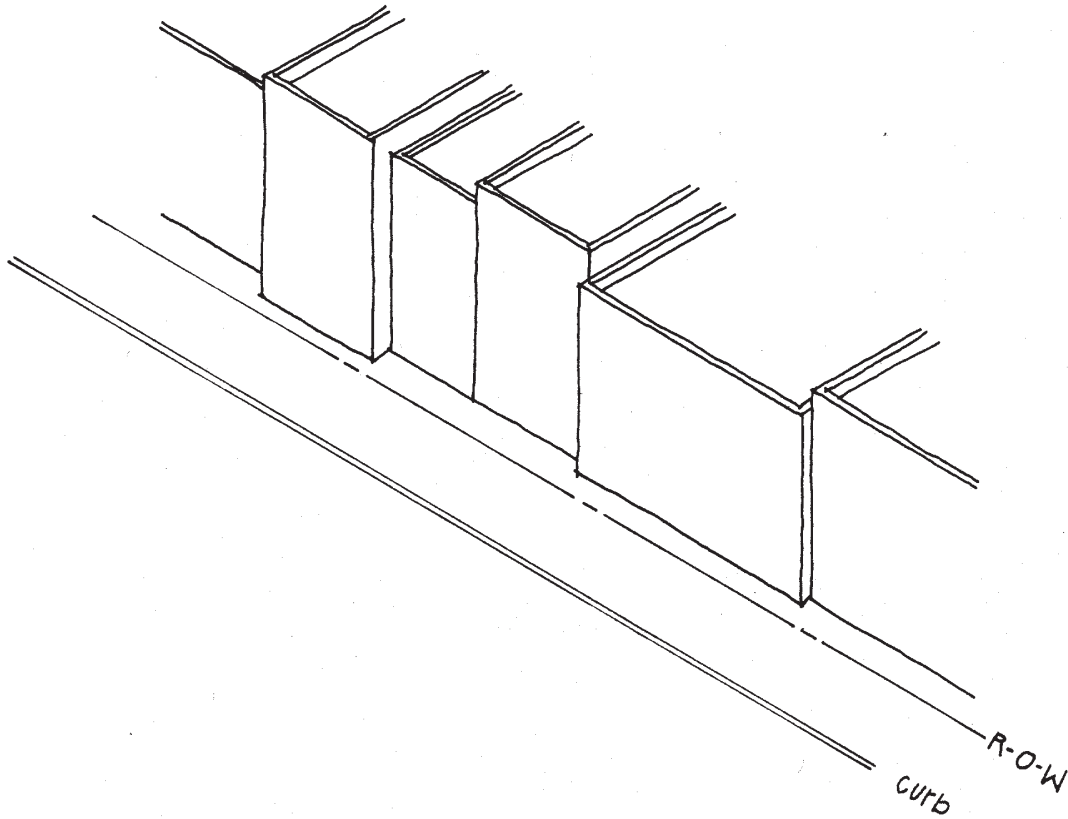


SITE DESIGN

S3. BUILDING SITING AND SETBACKS

BUILD-TO LINE

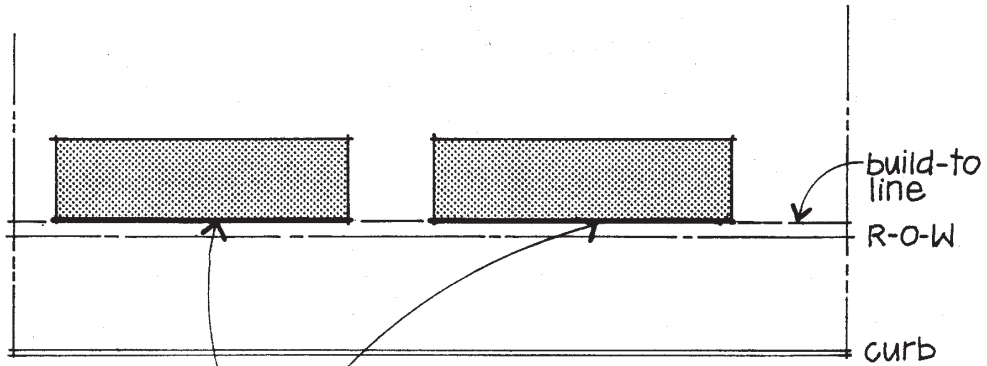
DO maintain a consistent front build-to line with all storefronts or buildings within an attached row or group of buildings in a block.



SITE DESIGN
S3. BUILDING SITING AND SETBACKS

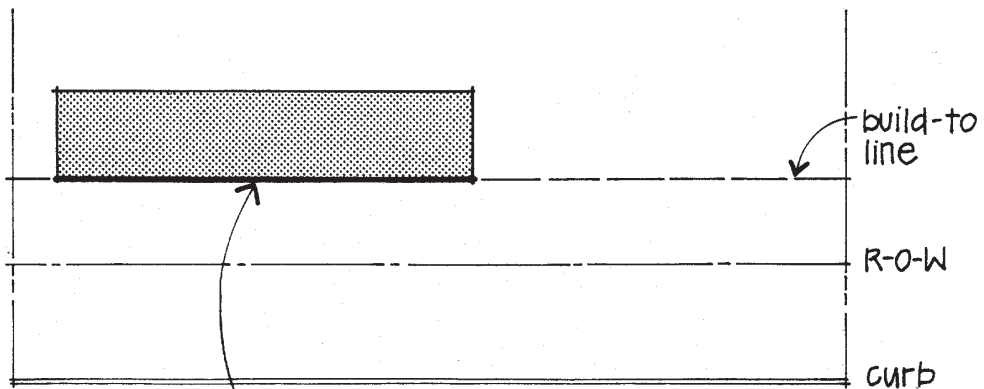
STREET-FACING FRONTAGE ON US 1

In Main street (3a and 3b) and Town Center (1b, 1c and 1d) subareas:



building facades shall occupy a minimum of 70 percent of a property's street-facing frontage on US 1

In the remainder of the development district:



building facades shall occupy a minimum of 50 percent of a property's street-facing frontage on US 1

SITE DESIGN

S4. BUFFERS AND SCREENING

OBJECTIVE

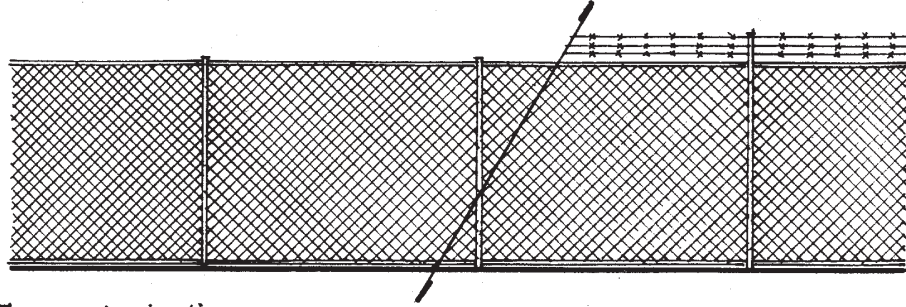
To provide an attractive, positive image of the community by screening unsightly views and providing adequate buffers between incompatible uses. Materials selected for buffers shall be of high quality and enhance the character of the built environment.

DESIGN STANDARDS

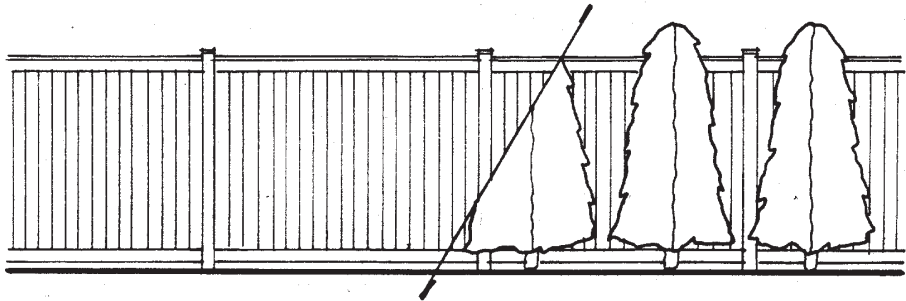
- A. All mechanical equipment, dumpsters, storage, service, loading and delivery areas shall be screened from public view, adjacent residential property and rights-of-way with an appropriate buffer consisting of plantings, walls or fences in compliance with the Screening Requirements of the *Landscape Manual*.
- B. Chain-link fencing (of any type), corrugated metal, corrugated fiberglass, sheet metal or wire mesh shall not be used as a screening material. The use of barbed/razor wire is not permitted.
- C. Appropriate elements for a buffer include continuous solid, opaque fences and masonry walls. Evergreen plant material may also be used in combination with metal picket-type/rail fencing. Plant material shall be of an appropriate species, size and quantity to provide an effective, immediate year-round buffer.
- D. Above-ground utility structures shall be screened with walls that have architecturally compatible materials and design features with the associated building.
- E. The bufferyard requirements within the development district may be reduced to facilitate a compact form of development compatible with the urban character of the US 1 corridor. The minimum bufferyard requirements (landscape yard) for incompatible uses in the *Landscape Manual* (Section 4.7) may be reduced by 50 percent. The plant units required per 100 linear feet of property line or right-of-way shall also be reduced by 50 percent. Alternative Compliance shall not be required for these reductions.

A six-foot-high, opaque masonry wall or other opaque screening treatment shall be provided in conjunction with the reduced width of the bufferyard between office/retail/commercial uses and residential uses.
- F. Bufferyards between any uses contained within a property of a mixed-use development in the main street (3a and 3b) and town center (1a, 1b, 1c, 1d and 1e) subareas shall not be required.
- G. Residential uses within the development district shall comply with the Residential Planting Requirements of the *Landscape Manual*.
- H. Walls, fences and plantings should be used to define boundaries, provide access control and also distinguish private and public areas.
- I. The placement of walls, fences and plantings shall not create blind spots or hiding places.

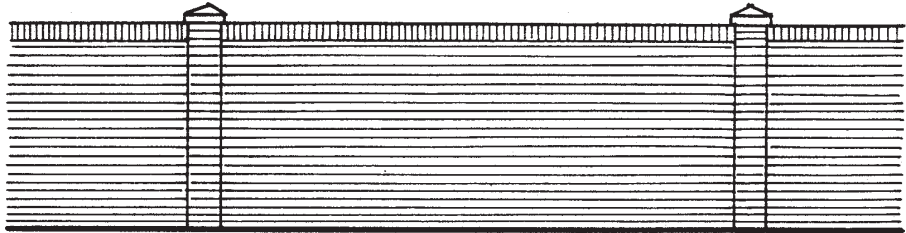
SITE DESIGN
S4. BUFFERS AND SCREENING



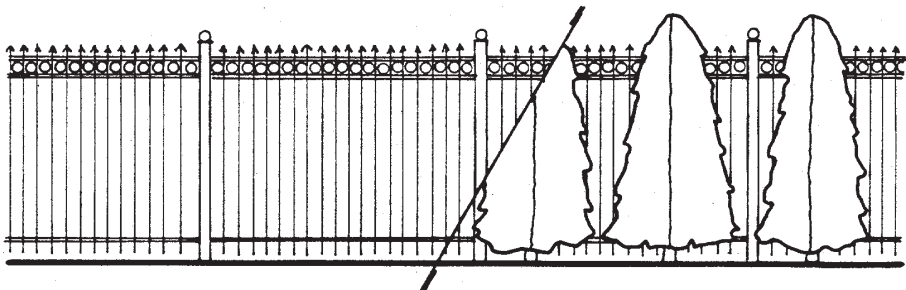
DON'T use chain-link fence as a buffer.



DO use a solid, opaque wood fence as a buffer.



DO use a solid masonry wall as a buffer.



DO use a metal picket fence in combination with evergreen/ coniferous trees as a buffer.

SITE DESIGN

S5. FREESTANDING SIGNS

OBJECTIVE

To create a positive, attractive appearance for businesses within the development district. All freestanding signs shall be compatible with their surroundings in terms of size, height, materials and design. Signs shall also be constructed of quality materials and complement the architectural character of associated buildings.

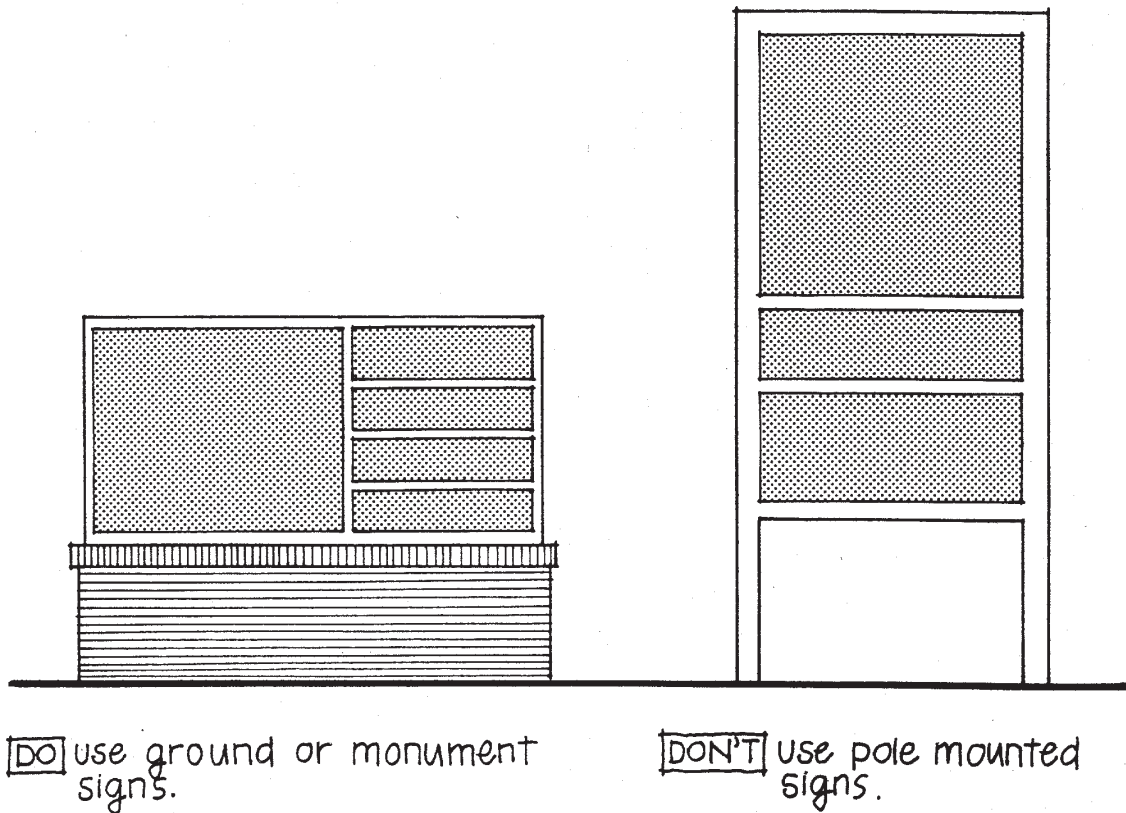
DESIGN STANDARDS

- A. The location of freestanding signs shall not be located closer than 10 feet behind the ultimate right-of-way as modified by Section 27-614(a), Freestanding Signs, in Part 12 of the Zoning Ordinance. In the main street (3a and 3b) and town center (1a, 1b, 1c, 1d and 1e) subareas, freestanding signs are discouraged and building signs should be used instead wherever possible. Placement of freestanding signs shall not hinder vision or obscure site lines for motorists.
- B. The maximum height of freestanding signs shall be eight feet in the main street (3a and 3b) and town center (1a, 1b, 1c, 1d and 1e) subareas and 12 feet elsewhere in the development district as measured from the finished grade at the base of the sign to the top of the sign for all commercial zones, as modified from Section 27-614(b) of the Zoning Ordinance.
- C. The area of the freestanding sign shall not exceed 1 square foot for each 2 linear feet of street frontage, to a maximum of 100 square feet for each sign for building(s) located in an integrated shopping center, other commercial center with 3 or more businesses served by common and immediate off-street parking and loading facilities, or an office building complex, as modified from Section 27-614(c). The street frontage shall be measured on the property occupied by the center or complex associated with the sign.
- D. The area of the freestanding sign shall not exceed 1 square foot for each 4 linear feet of street frontage, to a maximum of 100 square feet per sign for building(s) not located in an integrated shopping center, other commercial center with 3 or more businesses served by common and immediate off-street parking and loading facilities, or an office building complex, Section 27-614(c). The street frontage shall be measured on the property occupied by the use associated with the sign.
- E. The quantity of freestanding signs shall be equal to or less than the amount required by Section 27-614(d), Freestanding Signs, in Part 12 of the Zoning Ordinance.
- F. Ground or monument signs (signs mounted directly on a solid base) shall be used throughout the entire development district. New pole-mounted signs are not permitted.
- G. Freestanding signs that are portable, movable or have flashing components are not permitted.
- H. Freestanding signs shall primarily serve to identify the name and type of business establishment only, including a business logo.

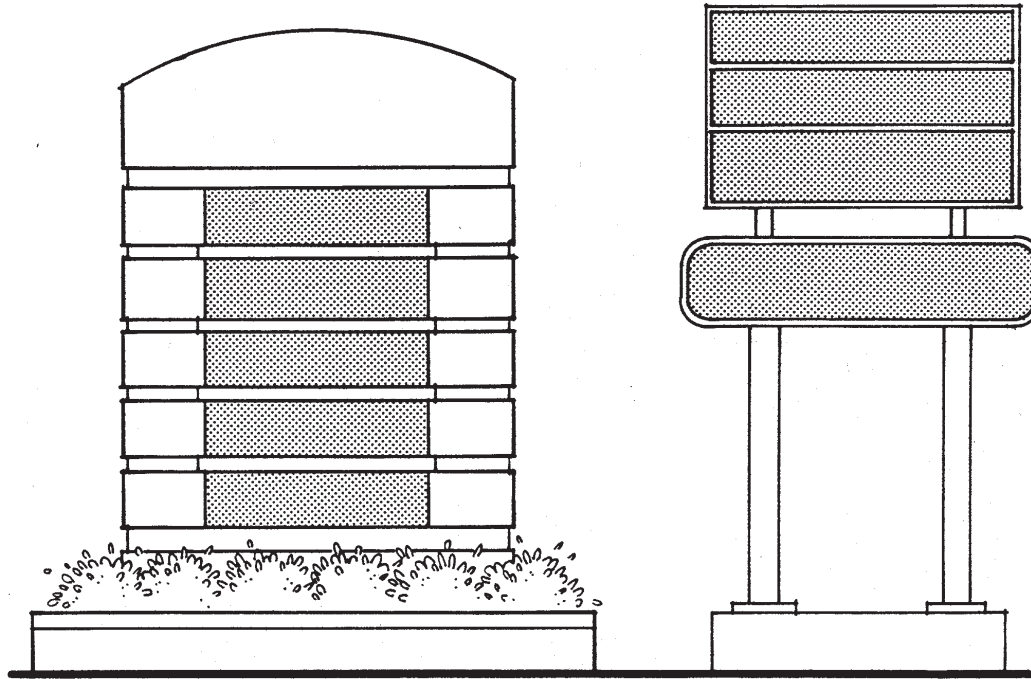
SITE DESIGN

S5. FREESTANDING SIGNS

- I. Freestanding signs should be compatible in design, color and materials with the overall architectural character of associated buildings on the parcel or property. Plantings may be incorporated around the base of signs to soften and integrate their appearance into the landscape.
- J. Freestanding signs should be externally lit and lighting should be directed to illuminate the sign face only. Light fixtures should be discretely placed so the light source and associated glare is not visible to motorists or pedestrians.
- K. All new institutional, office and retail/commercial buildings shall provide a common sign plan when there is more than one principal building or multitenant (three or more businesses) building on a single parcel or a combination of parcels under common ownership. Common sign plans shall specify standards including lighting, colors, lettering style, size, height, quantity and location within the site and on the building. Requests for major exterior renovation (50 percent or more based on front facade in linear feet) or major rehabilitation (50 percent or more increase in GFA) shall also submit a common sign plan.



SITE DESIGN
S5. FREESTANDING SIGNS



DO use signs which utilize quality building materials and integrate architectural design elements from adjacent buildings.

DONT use signs which utilize poor quality building materials and design elements.

SITE DESIGN

S6. TREES, PLANTINGS AND OPEN SPACE

OBJECTIVE

To enhance the visual character of the exterior environment with the introduction of trees and plantings as green infrastructure elements that reduce ambient air temperatures and ground-level ozone, and improve air quality and community aesthetics.

DESIGN STANDARDS

Trees and Plantings

- A. All design standards for planting street trees shall also apply to the planting of landscape trees and plants on sites proposed for new development and/or redevelopment. (See P4. Street Trees.)
- B. The planting of trees on sites proposed for new development and/or redevelopment shall be counted toward meeting the Woodland Conservation Ordinance requirements. Street trees planted on abutting road rights-of-way may also be counted toward meeting the requirement.
- C. Afforestation shall be accomplished through the provision of shade and ornamental trees. Tree cover shall be provided for a minimum of 10 percent of the gross site area and shall be measured by the amount of cover provided by a tree species in 10 years. Street trees planted along abutting rights-of-way may be counted toward meeting this standard. Exceptions to this standard shall be granted on redevelopment sites where provision of 10 percent tree cover is not feasible due to existing buildings and site features.
- D. Plant materials shall be selected that will maintain an appropriate height and shall not obscure sight lines.

Open Space

- E. Parks, open space, recreational facilities and playgrounds shall be located to maximize natural surveillance from adjacent buildings, roadways, sidewalks and trails.
- F. The boundaries and access routes to parks, open space, recreational facilities and playgrounds shall be clearly delineated through grading, path locations, landscaping, fences or woodland edges.
- G. In residential developments, parks, open space, recreational facilities and playgrounds shall be centrally located to encourage use and serve as the focus for the development.

SITE DESIGN

S7. STORMWATER MANAGEMENT

OBJECTIVE

To protect and enhance the natural stream system through the treatment of stormwater to improve quality and to reduce volume and velocity of stormwater entering receiving streams.

DESIGN STANDARDS

- A. Low impact development techniques, as contained in the current version of the design manual "Low-Impact Development Design Strategies An Integrated Design Approach," as published by the Department of Environmental Resources, shall be used on all sites as either the primary or secondary method of collecting and/or treating stormwater.
- B. Existing stormwater management facilities which are to be used to meet stormwater management requirements shall be retrofitted to the maximum extent possible to incorporate low-impact development techniques.
- C. If the construction of stormwater management facilities results in the removal of trees or existing woodland, the area should be replaced within the same site. Wherever possible, bio-engineering techniques should be used to reestablish the woodland lost.
- D. The use of underground retention facilities shall be considered throughout the development district, especially in the main street (3a and 3b) and town center (1a, 1b, 1c, 1d and 1e) subareas.
- E. Stormwater management facilities should be designed as visual amenities that are visible from a building or a street, rather than located in isolated areas. Openings in any screening treatments shall be provided to facilitate observation of the area.

SITE DESIGN
S8. NOISE

OBJECTIVE

To mitigate the impacts of unwanted noise to an established level.

DESIGN STANDARDS

A. Noise impacts from the proposed use:

Each site plan shall show the location of the projected noise contours for the proposed use based on the zoning of the adjacent properties as stated in the table below. When the noise contours for the proposed use are located beyond the property lines for that use, each site plan shall provide a noise study that contains appropriate mitigation measures to reduce the projected noise levels at or below the maximum noise levels listed below. If a noise study is required, it shall be reviewed and approved by staff prior to Planning Board approval of the site plan. If it is clear, based on the use of the property and its location, that noise levels will be well below the projected noise contours stated above, the site plan shall contain a note providing justification for not providing noise information for the proposed use.

Adjacent Zoning	Contour to be Shown	Measure of Noise
Industrial	75 dBA	L_{eq} (24)
Commercial	67 dBA	L_{dn}
Residential	65 dBA	L_{dn}

B. Noise impacts from transportation-generated noise:

Transportation-generated noise shall be measured for residential sites located along arterials, expressways and freeways. Each site plan for residential development shall show the location of the 65 dBA (L_{dn}) noise contour based on projected average daily traffic volumes on the subject roadway. Upon plan submission, the Planning Department shall determine if a noise study is required based on the delineation of the noise contour.

If it is determined that a noise study is required, it shall be reviewed and approved prior to approval of any site plan. The study shall include appropriate mitigation measures and the use of acoustical design techniques to obtain exterior noise levels of 65 dBA (L_{dn}) and interior noise levels of 45 dBA (L_{dn}) or less. A typical cross-section profile shall be provided for noise emissions from the road to the nearest habitable structure.

BUILDING DESIGN

BI. HEIGHT, SCALE, MASSING AND SIZE

OBJECTIVE

To ensure proposed buildings are an appropriate height, scale and massing for their intended function(s) and location along the US 1 corridor.

DESIGN STANDARDS

Height

- A. Retail/commercial, office and institutional buildings within an attached row in a block shall be similar in height and should not vary more than 15 percent relative to the average height in the row or block in the main street (3a and 3b) and town center (1a, 1b, 1c, 1d and 1e) subareas.
- B. Buildings located at prominent intersections should articulate the corner location with appropriate building forms and vertical emphasis in the main street (3a and 3b) and town center (1b, 1c and 1d) subareas along US 1 and Paint Branch Parkway.
- C. Buildings on parcels or properties, one or more of whose boundaries coincide with the Height Transition Line, shall step down to be compatible with buildings in adjacent existing residential neighborhoods. Any differences in topography shall be considered when determining the height of proposed buildings. (For building heights and Heights Transition Line, see Building Heights map.)

HEIGHT

DON'T vary the building height more than 15% relative to the average height of adjacent buildings.

DO construct buildings within an attached row which are similar in height.



BUILDING DESIGN

BI. HEIGHT, SCALE, MASSING AND SIZE

Scale

- D. Individual buildings shall utilize human-scaled architectural elements. Oversized/exaggerated elements or large monolithic box-like structures shall be avoided.
- E. Nonresidential and multifamily buildings shall articulate the first story and primary entrances with pedestrian-scaled architectural features and a basic three-part organizational structure of ground level, middle stories and roof.

Massing

- F. Buildings shall be articulated with wall plane projections, recesses or offsets on facades greater than 100 feet in horizontal length (measured in linear feet) and which face onto public streets.
- G. Architectural components should be designed as integral elements of the building and should not appear to be attached or applied onto the building facade.
- H. At least 60 percent of single-family detached residential dwellings in a development project shall incorporate street-facing porches to create a more active street environment and provide natural surveillance.
- I. All multifamily buildings should provide a balcony for each dwelling unit above the ground floor to articulate the building facade and to increase natural surveillance of the surrounding area.
- J. Service areas shall be architecturally integrated into the overall design of buildings.


Size

- K. Within an attached row or group of buildings in a block, infill buildings should maintain and reinforce the existing pattern of development. The height, scale, massing, architectural features and roof form shall be compatible with adjacent buildings.
- L. The minimum size for single-family detached dwellings units shall be 2,200 square feet, not to include garages and unfinished basements. The minimum size for single-family attached dwelling units shall be 1,600 square feet, not to include garages and unfinished basements.
- M. The average size of all multifamily dwelling units in a development project shall be a minimum of:
 - 750 square feet for a 1-bedroom/1-bath unit.
 - 1,050 square feet for a 2-bedroom/2-bath unit.
 - 1,275 square feet for a 3-bedroom/2-bath unit.

Bedroom Percentages

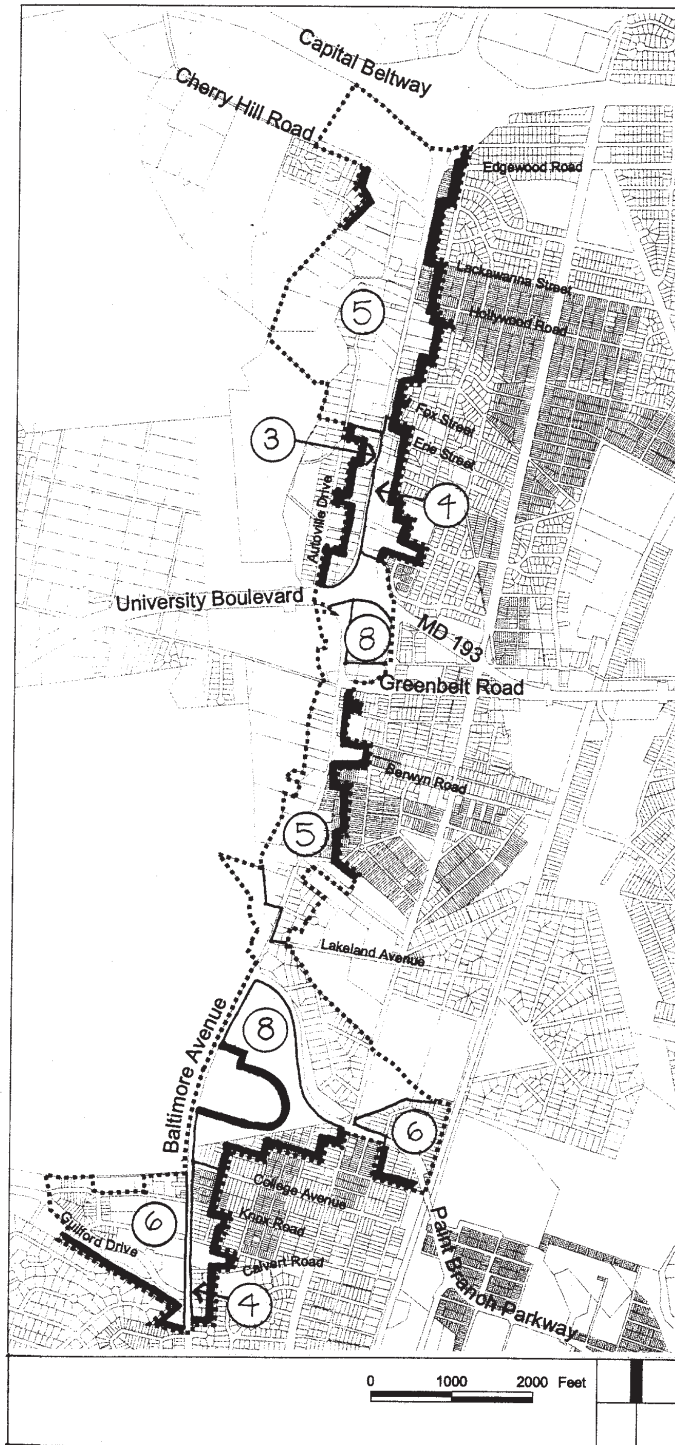
- N. Bedroom percentages for multifamily dwellings may be modified from section 27-419 of the Zoning Ordinance, if new development or redevelopment for student housing is proposed and the density is not increased above that permitted in the underlying zone.

LEGEND

- ③ 3 stories *
(subareas 4d & 5a)
- ④ 4 stories *
(subareas 4e & 1c)
- ⑤ 5 stories *
(subareas 3a, 3b, 4a,
4b, 4f, 5b, 6a & 6b)
- ⑥ 6 stories *
(subareas 1a, 1b, & 1e)
- ⑧ 8 stories *
(subareas 1d & 4c)
-  Height Transition Line

* Maximum height in general.

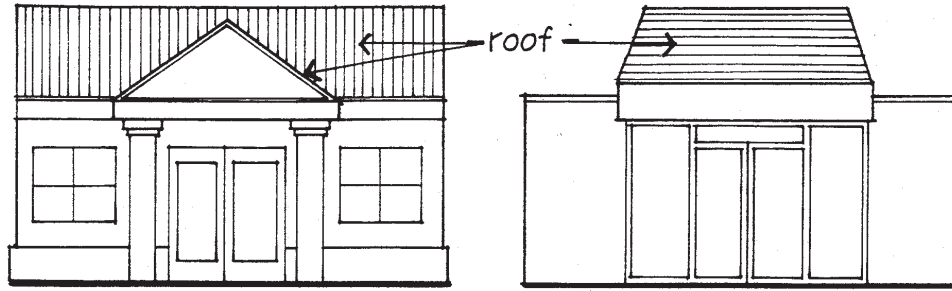
Upon demonstration by applicant that market and design considerations justify additional height, additional stories may be approved.



Building Heights

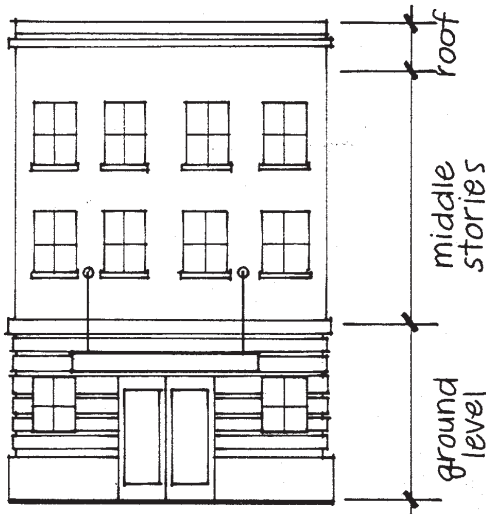
BUILDING DESIGN
BI. HEIGHT, SCALE, MASSING AND SIZE

SCALE AND MASSING

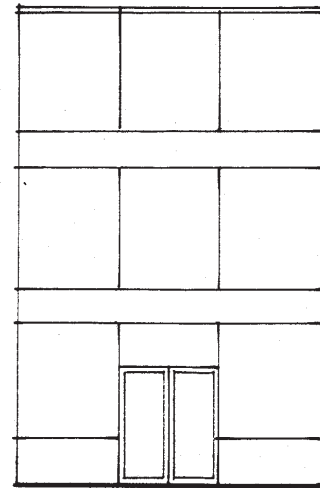


DO utilize architectural features which are integral components to a building.

DON'T utilize architectural features which appear attached or applied to the building facade.



DO utilize a basic three part organizational structure which promotes a sense of human scale.



DON'T utilize architectural features which do not convey a sense of human scale, such as unarticulated entries, flat or blank walls, large expanses of glass or opaque building materials.

BUILDING DESIGN
BI. HEIGHT, SCALE, MASSING AND SIZE

INFILL DEVELOPMENT IN AN ATTACHED ROW



compatible
infill development



incompatible
infill development

incompatible
infill development

BUILDING DESIGN

B2. ROOFS

OBJECTIVE

To utilize an appropriate roof form for the height, size, type and function(s) of a proposed building within the development district.

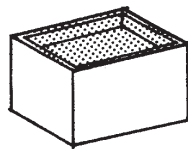
DESIGN STANDARDS

- A. Commercial buildings should employ flat roofs located behind parapet walls. Parapet walls should provide a three dimensional cornice treatment. Simple gable or hipped roofs may also be integrated into the roof design of commercial buildings.
- B. Residential buildings should employ simple gable or hipped roofs.
- C. Overly complex roof forms, as well as gambrel and mansard roofs, shall be avoided.

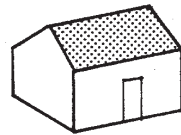
APPROPRIATE ROOF FORMS

commercial :

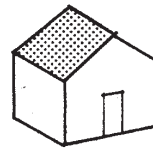
Residential :



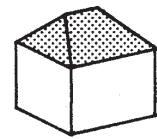
Flat with parapet walls



side-gabled



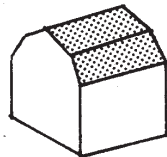
Front-gabled



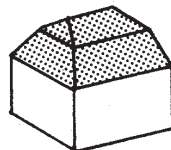
Hipped

INCOMPATIBLE ROOF FORMS

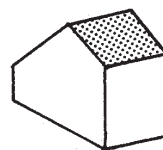
commercial and Residential :



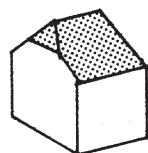
Gambrel



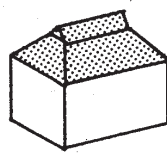
Mansard



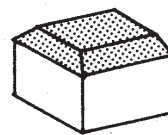
salt-box



Hip-on-gable



Gable-on-hip



Deck

BUILDING DESIGN

B3. ARCHITECTURAL FEATURES

OBJECTIVE

To select materials and architectural details which are appropriate to a building's use, location and surrounding context. Windows and doors are major elements which express the design character of a building's exterior appearance and their appropriate selection should establish scale, proportion and rhythm for a building's intended use and location.

The treatment of commercial storefronts should vary in architectural character but remain harmonious in overall appearance by utilizing compatible materials and details. The use of arcades, bays and other architectural features at street level should draw in pedestrians and reinforce street activity. Awnings on storefronts provide pedestrians with protection from the weather, shade window displays and articulate the three-dimensional quality of building facades which will enhance the pedestrian-focus within the development district. Fabric awnings, such as a canvas-type, are highly recommended. Metal awnings should generally be avoided.

DESIGN STANDARDS

Architectural Materials and Details

- A. High quality materials which are durable and attractive shall be used on all proposed non-residential and vertical mixed-use buildings within the development district. Exterior building materials such as precast concrete, brick, tile, wood and stone are recommended. Tilt-up concrete panels, smooth-faced concrete masonry units and prefabricated metal panels are not permitted.
- B. Single-family (detached/attached) residential building types shall have masonry facades (brick, stone or approved equal) on at least 60 percent (excluding gables, windows, trim and doors) of three sides of a dwelling or building within a development project.
- C. All multifamily building types in a development shall have a minimum of 75 percent of the exterior facades in brick, stone or approved equal (excluding windows, trim and doors).
- D. Above-grade foundation walls shall be clad with finish materials compatible with the primary facade material. Exposed foundation walls of unclad or unfinished concrete are prohibited. Exterior facade materials shall be extended down to 12 inches of finished grade, avoiding exposed unfinished concrete or concrete masonry unit (CMU) basement walls.
- E. All residential detached/attached building types where a chimney is provided shall incorporate masonry on the exterior of the chimney. Masonry material shall be brick or stone.
- F. Building facades which are composed of highly reflective or mirrored glass are not permitted. These materials do not convey a sense of human scale and are not compatible with a pedestrian-focused environment.
- G. Imitation or synthetic exterior building materials which simulate the appearance of stone or brick should be avoided.
- H. Buildings which are composed of "ribbons or bands" of glass and architectural precast panels shall be avoided.

BUILDING DESIGN

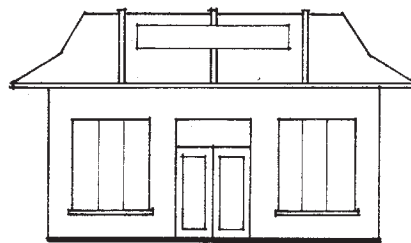
B3. ARCHITECTURAL FEATURES

- I. The exterior appearance of building facades within a residential development shall avoid the use of repetitive architectural elements and building forms. Residential dwellings shall employ a variety of architectural building designs incorporating features such as roofline, dormers, window and door treatments, porches, balconies, color and materials.
- J. Trademark buildings with typical franchise architecture are not permitted unless their exterior design is modified to relate to both the specific site and local building traditions, particularly regarding the building's siting, form, scale, detailing, color and construction materials.

TRADEMARK BUILDINGS



DO use an architectural vocabulary which relates to local building traditions and respects the integrity and character of the context in which the building is located.



DON'T use a standard trademark building, which is not compatible with the design intent and vision for the US 1 corridor.

Window and Door Openings

- K. Nonresidential buildings shall have clearly defined and highly visible customer entrances and shall be recessed or framed by a sheltering element such as an overhang, arcade, portico or other roof form.
- L. Individual "punched" or framed windows shall be provided instead of horizontal "ribbon or band" type windows. Glass curtain walls and other continuous, floor-to-ceiling windows shall be avoided.
- M. Large display windows shall be provided for storefronts with retail uses at street level throughout the US 1 corridor. Display windows within the main street (3a and 3b) and town center (1b, 1c and 1d) subareas shall encompass a minimum of 40 percent and a maximum of 80 percent of a storefront's frontage (measured in linear feet).

BUILDING DESIGN

B3. ARCHITECTURAL FEATURES

- N. Patterns of window openings or articulation of bays should be used to create a sense of scale and add visual interest to building facades.
- O. Large, blank building walls shall not permitted when facing public areas such as streets, parking lots or zones of pedestrian activity.
- P. Overly small or large windows which convey a distorted sense of scale shall be avoided.
- Q. Existing windows shall not be blocked in and replaced with a smaller or incompatible window. Replacement windows shall match the existing window in design, materials and size as closely as possible.
- R. Exterior burglar bars on windows and doors shall not permitted within the development district. Burglar bars convey a negative image, and other less visually obtrusive security methods should be employed instead.
- S. Merchandise shall not be displayed in front of or leaning against the exterior facade(s) of a building.

Awnings

- T. The design of awnings, including the selection of a material and color, shall complement the architectural style and character of a building.
- U. Large buildings with several storefronts shall have compatible, though not necessarily identical, awnings. Awnings should be the same general style, material and proportion, although awnings may employ different but harmonious colors and patterns.
- V. Awnings (canopies) may project up to three feet beyond the build-to line into the public right-of-way and the bottom of the canopy shall be at a minimum of eight feet above the sidewalk.

Building Amenities

- W. All multifamily buildings and mixed-use developments with residential units having four or more stories should provide amenities including, but not limited to:
 - 1. A furnished lobby, security system for building access, a fitness center and a porte-cochere at the building's primary entrance.
 - 2. A washer/dryer and a security system in each dwelling unit.
- X. All multifamily buildings having less than four stories should provide amenities including, but not limited to:
 - 1. A fitness center for a development project with 100 or more dwelling units.
 - 2. A washer/dryer and a security system in each dwelling unit.

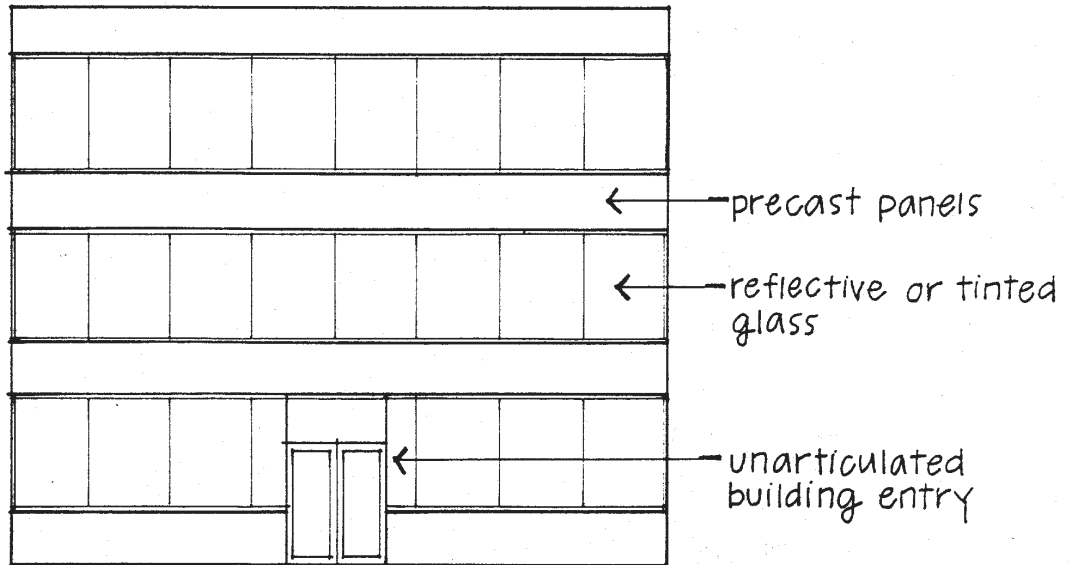
BUILDING DESIGN

B3. ARCHITECTURAL FEATURES

MATERIALS AND DETAILS



DO articulate the first story and primary entrances of a building with pedestrian-scaled architectural features.



DON'T use continuous "ribbons or bands" of reflective/tinted glass and architectural precast panels.

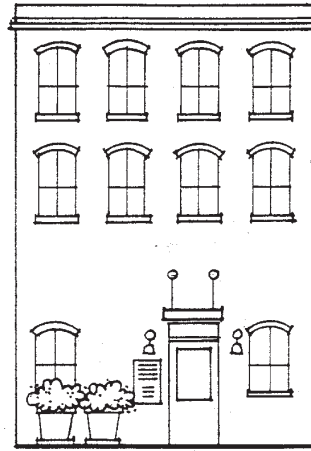
BUILDING DESIGN

B3. ARCHITECTURAL FEATURES

APPROPRIATE STOREFRONT TREATMENTS



DO articulate storefronts with display windows, recessed entry door(s), awnings, signs and lighting.



DO articulate rear building entrances with awnings, signs, lighting and plantings.

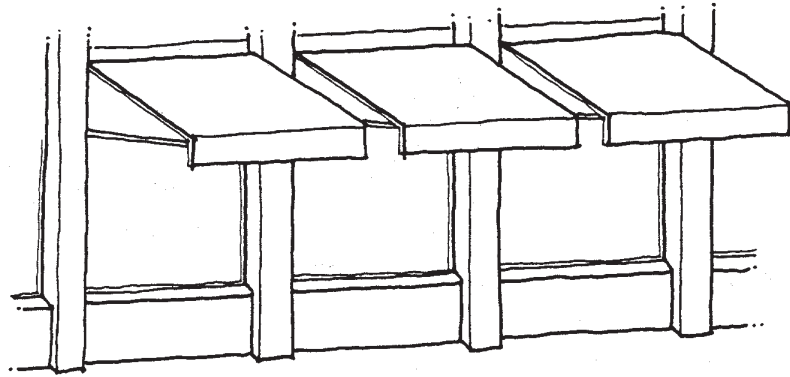
INCOMPATIBLE STOREFRONT TREATMENT



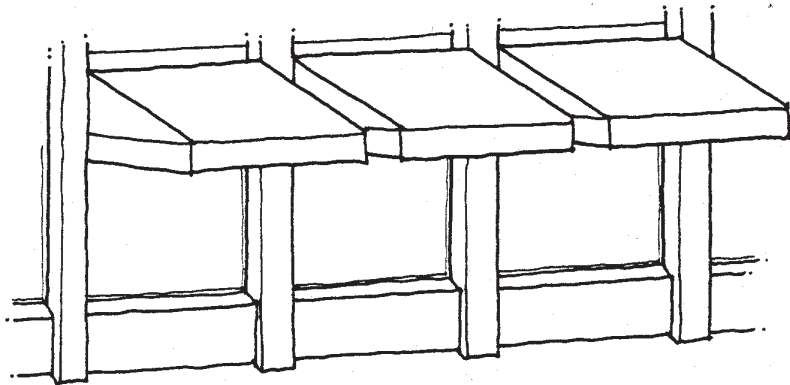
DON'T vary an individual storefront with incompatible materials and design features within a multitenant building.

BUILDING DESIGN
B3. ARCHITECTURAL FEATURES

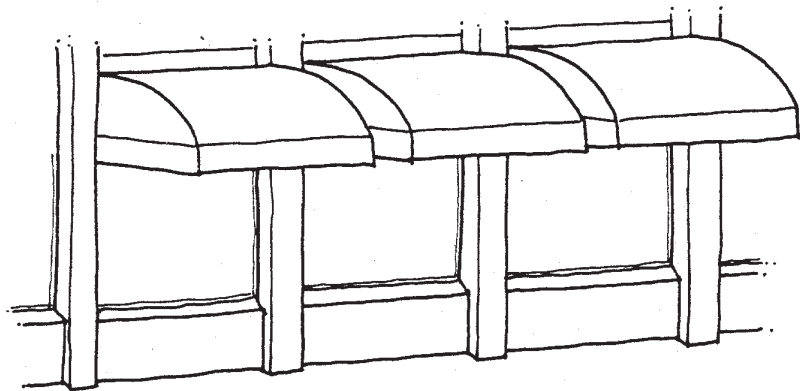
Appropriate types of building awnings



slanted-type awning (with open sides)



slanted-type awning (with closed sides)

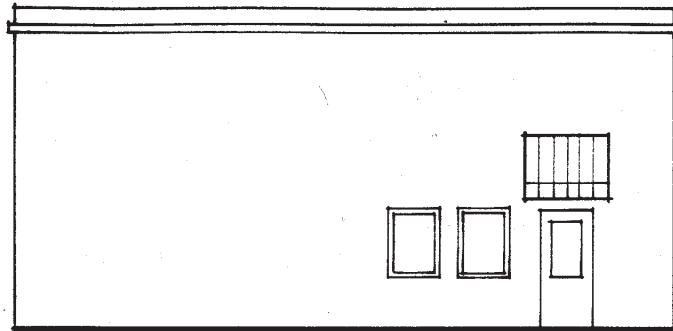


quarter-circular type awning (with closed sides)

BUILDING DESIGN

B3. ARCHITECTURAL FEATURES

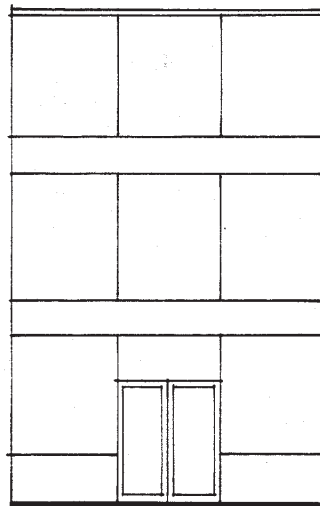
WINDOW AND DOOR OPENINGS



DON'T locate large, blank walls which face public areas such as streets, parking lots or zones of pedestrian activity.



DO use individual "punched" or framed windows and large display windows at street level.



DON'T use large, continuous floor to ceiling windows.

BUILDING DESIGN

B4. LIGHTING

OBJECTIVE

To incorporate lighting as a distinctive and character-defining element on building facades within the development district.

DESIGN STANDARDS

- A. Lighting shall be an integral component in the overall architectural design of all buildings within the development district.
- B. Lighting shall provide adequate safety and visibility around building entrance(s) and the building's perimeter. Building light fixtures shall be placed to avoid blind spots, glare areas, and shadows.
- C. High intensity light fixtures that are mounted to the exterior of a building shall direct glare away from adjoining properties and public rights-of-way.
- D. Building lighting shall be coordinated with site lighting, when appropriate.

BUILDING DESIGN

B5. SIGNS (ATTACHED TO A BUILDING OR CANOPY)

OBJECTIVE

To create a positive image with attractive and well-maintained signs along the US 1 corridor. Signs should also enhance and contribute to the architectural character of the buildings within the development district.

DESIGN STANDARDS

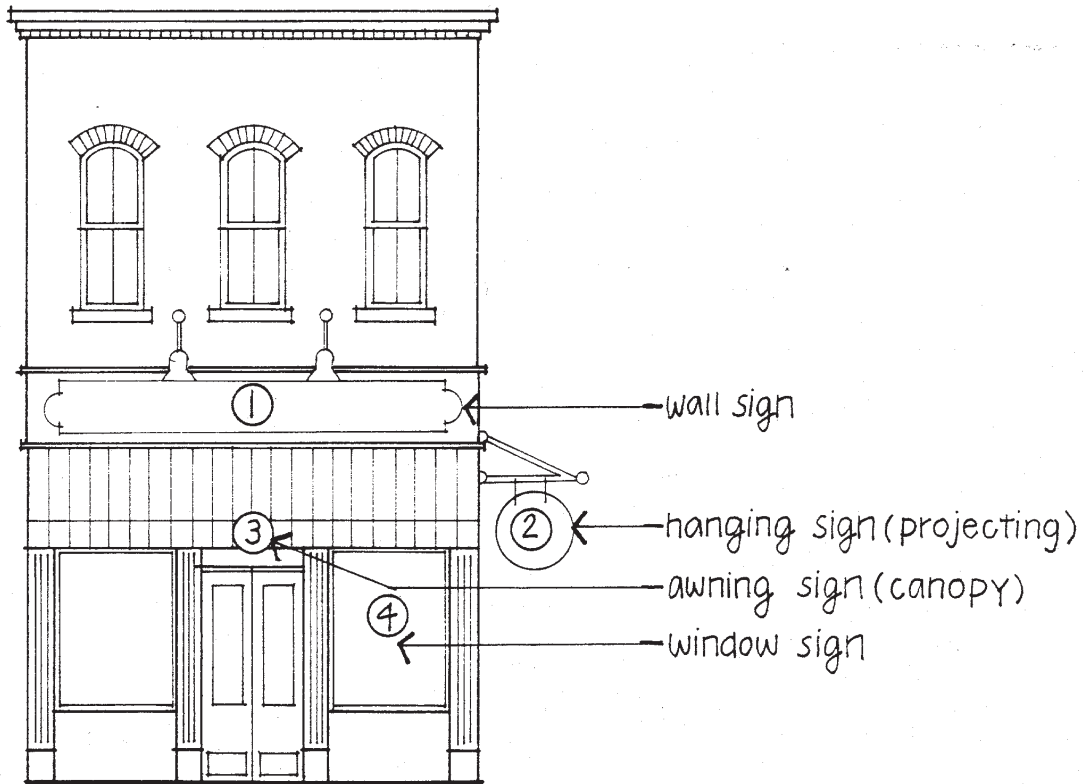
- A. The location of a sign attached to a building in the main street (3a and 3b) and town center (1a, 1b, 1c, 1d and 1e) subareas may not extend beyond the street line or ultimate right-of-way as modified from Section 27-613(a) of the Zoning Ordinance. Section 27-613(a) applies in the remaining areas of the development district.
- B. Signs located above or projecting from the roof line or parapet wall are not permitted throughout the development district as modified from Section 27-613(b) of the Zoning Ordinance.
- C. Projecting signs shall not interfere with the vehicular or pedestrian traffic adjacent to the sign.
- D. Signs shall be simply designed, contain a minimum amount of information, and shall primarily serve to identify the name, type, logo and street address of a business establishment.
- E. Signs that have flashing or blinking components are not permitted.
- F. Building signs shall be constructed of permanent, quality materials. Temporary signs which are attached to the building facade are not permitted.
- G. The placement of a sign shall be integrated into the overall architectural design of a building. The materials, colors, type, style and size of signs shall be compatible with other architectural features of a building.
- H. Signs that are externally lit are recommended and shall be directed to illuminate the sign face only. Entire sign faces (box signs) that are internally lit are not recommended. Individual letters or characters should be lit instead of the entire sign face.
- I. Window signs (attached to the inside or outside of a window) shall not obscure the interior view of a business/retail establishment and shall not occupy more than 25 percent of the total area of the window in which the sign is located.
- J. Canopy (awning) signs may contain the name of a business, logo and business street number.
- K. Signs for multitenant buildings shall be consistent and coordinated in terms of design, placement, size, materials and color.
- L. Flags and banners attached to a building facade in a commercial zone shall be included in the calculations for a building sign.

BUILDING DESIGN

B5. SIGNS (ATTACHED TO A BUILDING OR CANOPY)

- M. Pennants, pinwheels and similar circus- or carnival-type attractors which are attached to a building, other site features or strung between buildings and other site features are not permitted.
- N. All new institutional, office and retail/commercial buildings shall provide a common sign plan when there is more than one principal building or multitenant (three or more businesses) building on a single parcel or a combination of parcels under common ownership. Common sign plans shall specify standards including lighting, colors, lettering style, size, height, quantity and location within the site and on the building. Requests for major exterior renovation (50 percent or more based on front facade in linear feet) or major rehabilitation (50 percent or more increase in GFA) shall also submit a common sign plan. The location, height and area of building-mounted signs shall be equal to or less than that allowed by Part 12, Section 27-613, of the Zoning Ordinance.

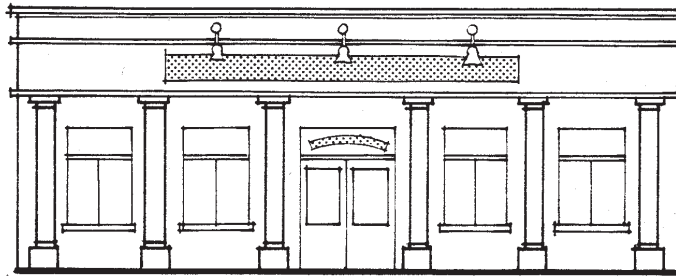
APPROPRIATE BUILDING SIGN LOCATIONS



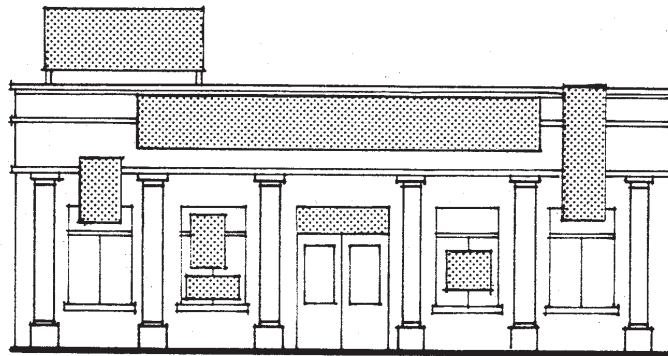
BUILDING DESIGN

B5. SIGNS (ATTACHED TO A BUILDING OR CANOPY)

SINGLE TENANT BUILDING



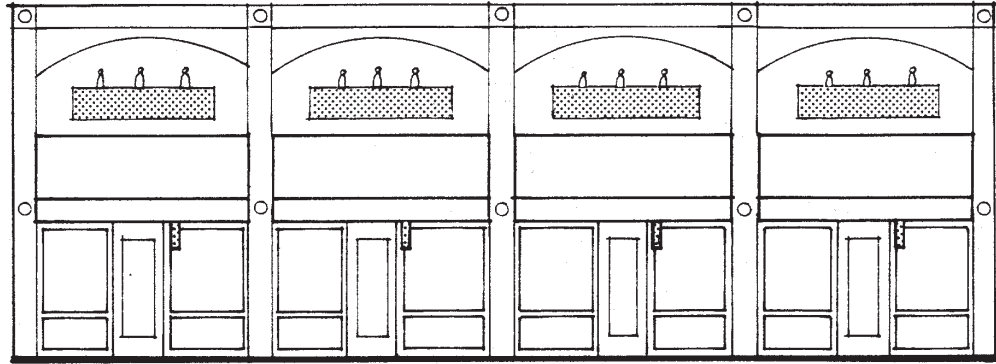
- DO incorporate sign location(s) into the overall architectural design of a building.
- DO construct a simply designed sign with permanent, quality materials.
- DO coordinate the placement, materials, color, type, style and size of signs with other architectural features of a building.



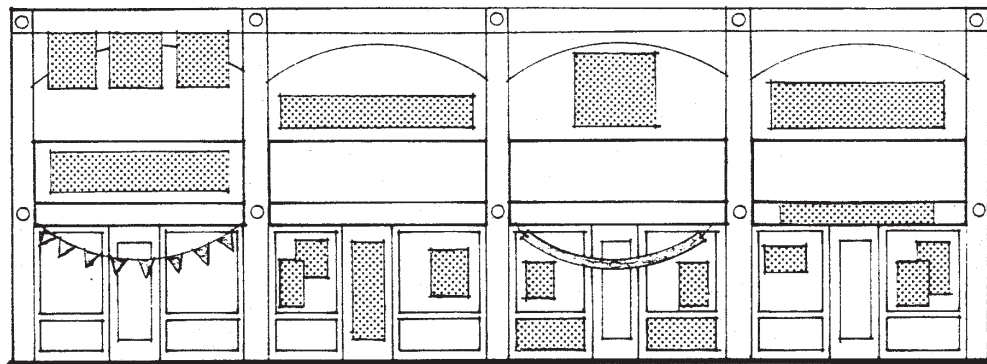
- DON'T create visual clutter or obscure architectural features/details on the building facade.
- DON'T cover windows with signs which obscure the interior view into a building.
- DON'T locate signs above or projecting from the roof line or parapet wall of a building.

BUILDING DESIGN
B5. SIGNS (ATTACHED TO A BUILDING OR CANOPY)

MULTITENANT BUILDING



DO coordinate sign locations for multitenant buildings. signs should be consistent in their placement, design, size, materials and color.



DON'T create visual clutter with uncoordinated signs placed in various locations on the building facade.

BUILDING DESIGN

B6. BUILDING SERVICES

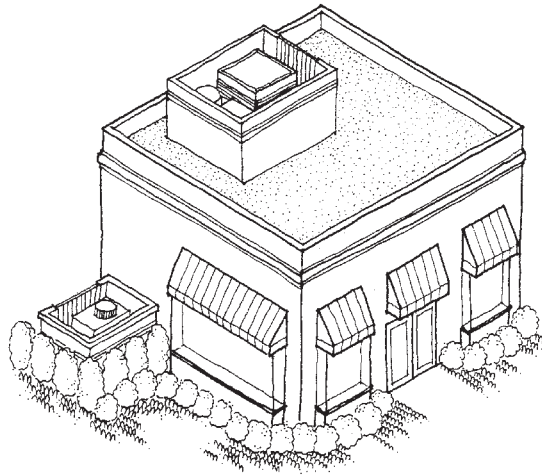
OBJECTIVE

To locate and screen all mechanical equipment and storage areas from public view.

DESIGN STANDARDS

- A. The screening requirements within the development district shall comply with the *Land-scape Manual*.
- B. Rooftop mechanical equipment shall be located below sight lines from adjacent streets and shall be architecturally integrated or screened with compatible building materials by the use of parapet walls or other comparable roof forms.
- C. Ground level mechanical equipment including storage, service and delivery areas shall be located in a visually inconspicuous area, such as in the rear of a building or site and out of public view.
- D. Exterior window air-conditioning units shall not be permitted on new building construction within the development district.
- E. Access to a building for services such as deliveries or trash removal should be provided from the rear of a site, whenever possible.
- F. Dumpsters shall be enclosed with a continuous solid, opaque masonry wall or other opaque screening treatment. Buildings should consolidate their garbage storage needs in a single, central location away from public view.

screen rooftop mechanical equipment from public view with the use of compatible building materials.



screen ground level mechanical equipment in a manner that is compatible with the architectural character of the building and the overall site design.

BUILDING DESIGN

B7. FORMER RESIDENTIAL BUILDINGS IN COMMERCIAL USE

OBJECTIVE

To improve the appearance and function of residential dwellings which have been converted to a commercial use.

DESIGN STANDARDS

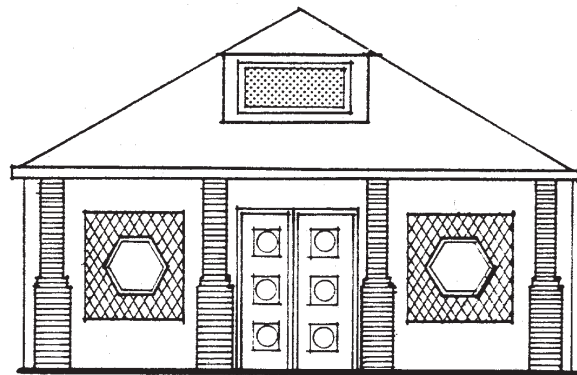
- A. Residential dwellings converted to a commercial use shall preserve the residential appearance of the building.
- B. All door and window openings should be preserved and maintained.
- C. The integrity of original building materials should be preserved and maintained.
- D. Parking shall be located to the side or rear of the lot and is not permitted in the front yard.
- E. Front yards shall relate to the adjacent residential buildings and surrounding neighborhood context.
- F. Front yards shall remain as open space and be planted and well-maintained.
- G. All mechanical equipment, storage and service areas shall be screened from public view, adjacent properties and rights-of-way with an appropriate buffer.

BUILDING DESIGN

B7. FORMER RESIDENTIAL BUILDINGS IN COMMERCIAL USE

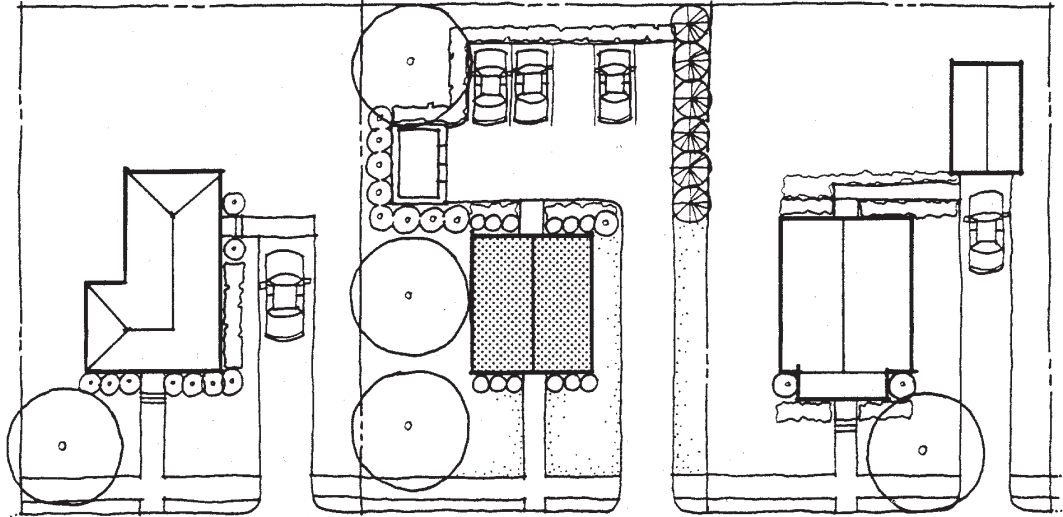


DO preserve the integrity and character of residential buildings which have been converted to a commercial use.

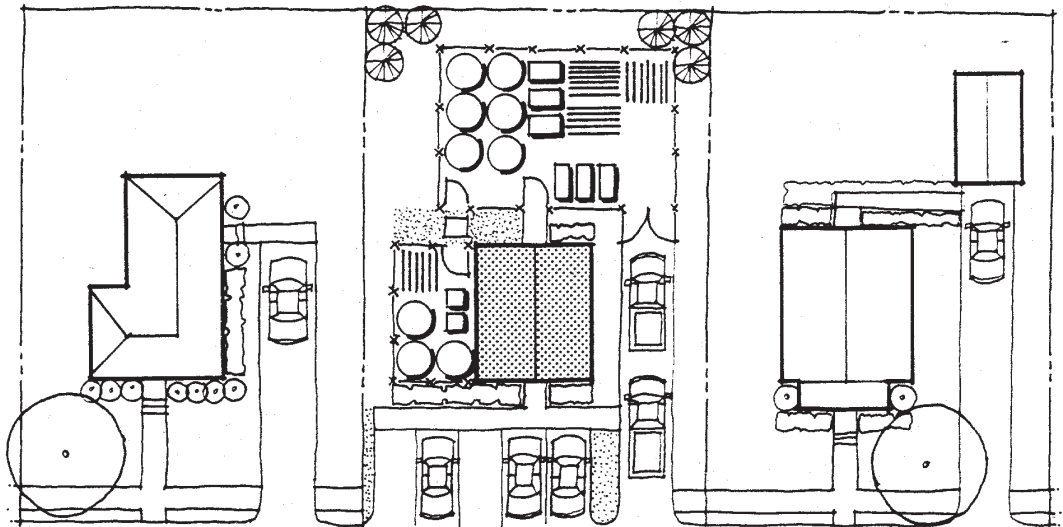


DON'T alter the exterior appearance with treatment(s) that would compromise the integrity and character of the building's architectural features, such as door/window openings, building materials, roof form, columns and porches.

BUILDING DESIGN B7. FORMER RESIDENTIAL BUILDINGS IN COMMERCIAL USE



DO preserve the residential appearance of the front yard by locating screened parking and storage areas towards the rear of the property.



DON'T create an incompatible appearance within a residential setting by locating parking areas in the front yard and unscreened storage areas on the property.

DIVISION 3. USES PERMITTED.

27-461. Uses permitted.

- (a) No use shall be allowed in the Commercial Zones, except as provided for in the following Table of Uses. In the table, the following applies:
- (1) The letter "P" indicates that the use is permitted in the zone indicated.
 - (2) The letter "P*" indicates that the use is permitted, but subject to the general special exception standards in Section 27-317(a)(1), (4), (5) and (6) and conforms to the recommendations of the sector plan.
 - (3) The letters "PA" indicate that the use is permitted, subject to the following:
 - (A) There shall be no entrances to the use directly from outside of the building;
 - (B) No signs or other evidence indicating the existence of the use shall be visible from the outside building, other than a business identification sign lettered on a window. The sign shall not exceed six (6) square feet in area; and
 - (C) The use shall be secondary to the primary use of the building.
 - (4) The letters "PB" indicate that the use is permitted, subject to the following:
 - (A) The use shall be related to, dependent on, and secondary to a principal use on the premises;
 - (B) The use shall be located on the same record lot as the principal use;
 - (C) The use shall not be located within a building not occupied by the principal use; and
 - (D) The floor area of any building (and the land area occupied by any structure other than a building) devoted to the use shall not exceed an area equal to 45 percent of the gross floor area of the building within which the principal use is located.
 - (5) The letter "X" or a blank (unless otherwise clear from the context) indicates that the use is prohibited.
 - (6) All uses not listed are prohibited.
 - (7) Whenever the tables refer to an allowed use, that use is either permitted (P), permitted as a (P*) but subject to certain general special exception provisions in accordance with Part 4 of this Subtitle, or permitted as a (PA) or (PB) use, as listed in the tables.
 - (8) Footnotes 37-42 stipulate that certain commercial uses are permitted in specific geographic subareas of the College Park US 1 Corridor Sector Plan.

Table 17

(b) TABLE OF USES I—Commercial Zones

USE	ZONE		
	C-O in DDOZ	C-S-C in DDOZ	M-U-I in DDOZ
(1) COMMERCIAL:			
(A) Eating or Drinking Establishments:			
Drive-in restaurant	X	P* ³⁷	P* ³⁷
Fast-food restaurant:			
(i) Within a wholly enclosed shopping mall, or department, variety, or drug store	X	P	P
(ii) Within an office building	PA	P	P
(iii) Within a hotel	X	P	P
(iv) Accessory to, and as an integral part of, an allowed recreational facility	P	P	P
(v) Within, and sharing the same points of vehicular access as, an integrated shopping center having six individual businesses (including the fast-food restaurant) and a minimum 50,000 square foot gross floor area ¹⁰ (CB-29-1986)	X	P	P
(vi) Provided that the fast-food restaurant is part of an integrated shopping center, is not attached to a gas station, is limited to a maximum 2,000 square foot gross floor area, no more than 20 seats, and no drive-in or delivery service ¹⁶ (CB-126-1989)	X	P	P
(vii) All others	X	P* ³⁷	P*
Other than a drive-in or fast-food restaurant (which may include incidental carry-out service, except where specifically prohibited):			
(i) Permitting no entertainment (of any sort) other than music, and no patron dancing	P	P	P
(ii) Within an office building	P	P	P
(iii) Accessory to an allowed use	X	P	P
(iv) Freestanding, within an office building complex of at least 20 acres	P ⁹	P	P
(v) Permitting live entertainment or patron dancing, with hours of operation that extend beyond 11:00 p.m., and with parking abutting land used for residential purposes, except hotel lounges	P* ³⁸	P* ³⁸	P* ³⁸
(vi) All others (CB-33-1985; CB-104-1985; CB-65-1989; CB-89-1993; CB-90-1993)	P*	P	P
(B) Vehicle, Mobile Home, Camping Trailer, and Boat Sales and Service:			
Bus maintenance accessory to:			
(i) A private school or educational institution	X	P* ³⁹	P* ³⁹
(ii) A church or other place of worship	X	P* ³⁹	P* ³⁹
Boat fuel sales at the waterfront	X	X	X
Boat sales, service, and repair, including outdoor storage of boats and boat trailers:			
(i) Accessory to a marina	X	X	X
(ii) All others	X	P* ³⁹	P* ³⁹

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
Boat storage yard	X	X	X
Car wash:			
(i) On a parcel of at least 10 acres with any structures located at least 200 feet from any land in any Residential Zone or land proposed to be used for residential purposes on an approved Basic Plan for a Comprehensive Design Zone, approved Official Plan for an R-P-C Zone, or any approved Conceptual or Detailed Site Plan	X	P	P
(ii) Self-service, coin operated, automatic car wash as an accessory use to the permitted use of a commercial parking lot, with shuttle service to Metro and located within two (2) miles of a Metro station	X	P* ³⁹	P* ³⁹
(iii) All others (CB-76-1998)	X	P* ³⁹	P* ³⁹
Gas station (in the C-M Zone, subject to Detailed Site Plan review in accordance with Section 27-358(a)(1),(2),(4),(5),(6), (7),(8),(9), and (10)) (CB-1-1989; CB-72-1999)	X	P* ³⁷	P* ³⁷
Incidental automobile service in a parking garage ³	X	P*	P*
Vehicle lubrication or tune-up facility, provided all sales and installation operations are conducted in a wholly enclosed building with no outdoor storage (CB-43-1987)	X	P* ³⁷	P* ³⁷
Vehicle, mobile home, or camping trailer repair and service station (CB-50-1993)	X	P* ³⁹	P* ³⁹
Vehicle, mobile home, or camping trailer sales lot, which may include dealer servicing and outdoor storage of vehicles awaiting sale; but shall exclude the storage or sale of wrecked or inoperable vehicles, except as accessory to the dealership for vehicles which the dealership will repair (CB-95-1987)	X	P* ³⁹	P* ³⁹
Vehicle or camping trailer rental (in the C-M Zone, subject to Section 27-417(a),(b)(2), and (c))	X	P* ³⁹	P* ³⁹
Vehicle or camping trailer storage yard (CB-80-1996)	X	X	X
Vehicle parts or tire store including installation facilities, provided all sales and installation operations are conducted in a wholly enclosed building with no outdoor storage:			
(i) On a parcel of at least 10 acres, with any structures located at least 200 feet from any land in any Residential Zone (or land proposed to be used for residential purposes on an approved Basic Plan for a Comprehensive Design Zone, approved Official Plan for an R-P-C Zone, or any approved Conceptual or Detailed Site Plan)	X	P* ³⁹	P* ³⁹
(ii) Accessory to a department store (CB-58-1990)	X	X	X
(iii) All others (CB-21-1992)	X	P* ³⁹	P* ³⁹
Vehicle parts or tire store without installation facilities	X	P	P

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
Vehicle towing station, provided it is enclosed by a sight-tight wall or fence at least 6 feet high, or an evergreen screen (CB-30-1992)	X	X	X
(C) Offices:			
Bank, savings and loan association, or other savings or lending institution:			
(i) Automatic teller machine, only	P	P	P
(ii) Check cashing establishment	P* ⁴³	P* ⁴³	P* ⁴³
(iii) All others	P	P	P
Contractor's office (see paragraph (3), Miscellaneous)			
Office accessory to an allowed use	P	P	P
Office (except as otherwise provided):			
(i) Within an integrated shopping center, and not exceeding 10% of the gross floor area of the center	X	X	X
(ii) All others	P	P	P
Office of a certified massage therapist (CB-44-2000)	P	P	P
Office of a medical practitioner or medical clinic (which may include an accessory private spa)	P	P	P
Real estate subdivision sales office as a temporary use, in accordance with Sections 27-260 and 27-261	P	P	P
Where not otherwise specifically permitted, any use allowed in the C-R-C Zone (excluding those permitted by special exception) may be located within an office building, provided that the uses shall not be located above the ground floor; not more than 15% of the gross floor area of the building shall be devoted to the use; and not more than 3,000 square feet of gross floor area shall be allotted to any one shop (CB-58-1990)	PA	X	X
Where not otherwise specifically permitted, any use allowed in the C-S-C Zone (excluding those permitted by special exception), may be located within an office building, provided that the uses shall not be located above the ground floor; not more than 15% of the gross floor area of the building shall be devoted to the uses; and not more than 3,000 square feet of gross floor area shall be allotted to any one shop	PA	X	X
Where not otherwise specifically permitted, any use allowed in the C-S-C Zone (excluding those permitted by special exception) may be located within an existing building no more than three (3) stories in height, including a maximum of 65,000 square feet of gross leasable area, provided such building and its associated parking are located on one or more contiguous parcels of property abutting two (2) streets shown on the Master Plan as arterial or higher classification, and located at an intersection where the three (3) other corners of said intersection are zoned C-S-C, and where the parcel or parcels of property upon which the building and its associated parking are located abut land zoned C-S-C at a minimum of two (2) locations (CB-69-1999)	P	X	X

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
Where not otherwise specifically permitted, any use allowed by special exception in the C-S-C Zone may be located within an existing building no more than three (3) stories in height, including a maximum of 65,000 square feet of gross leasable area, provided such building and its associated parking are located on one or more contiguous parcels of property abutting two (2) streets shown on the master plan as arterial or higher classification, and located at an intersection where the three (3) other corners of said intersection are zoned C-S-C, and where the parcel or parcels of property upon which the building and its associated parking are located abut land zoned C-S-C at a minimum of two (2) locations (CB-69-1999)	X	X	X
(D) Services:			
Ambulance service, private	X	X	X
Animal hospital, animal training, kennel	X	P*	P*
Artist's studio	P	P	P
Barber or beauty shop (CB-148-1987)	P	P	P
Bicycle repair shop:			
(i) Nonmotorized only	X	P	P
(ii) All others	X	P* ³⁷	P* ³⁷
Blacksmith shop	X	X	X
Blueprinting, photostating, or other photocopying establishment	X	P	P
Carpet or rug shampooing establishment	X	X	X
Catering establishment:			
(i) Accessory to an allowed use	P	P	P
(ii) As a primary use but including banquet facilities and a restaurant	P	P	P
(iii) For food preparation and administrative office only (no banquet facilities)	X	P ³³	P ³³
(iv) All others (CB-94-1996, CB-34-1999)	X	P*	P*
Data processing	P	P	P
Dry cleaning or laundry pickup station (CB-127-1986)	X	P	P
Dry cleaning store or plant:			
(i) Retail	X	P	P
(ii) Wholesale (may include retail service)	X	X	X
Electric or gas appliance, radio, or television repair shop	X	P	P
Employment agency	P	P	P
Farm implement repair	X	X	X
Fortune telling	P	P ³⁷	P
Funeral parlor, undertaking establishment (CB-2-1989)	P*	P*	P*
Household appliance or furniture repair shop	X	P	P

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
Key or locksmith shop (CB-128-1986)	X	P	P
Laboratory:			
(i) Accessory to an allowed use	PA	P	P
(ii) Dental laboratory	P	P	P
(iii) All other laboratories (CB-4-1986)	P*	P	P
Laundromat:			
(i) Accessory to an allowed use	X	X	X
(ii) All others	X	P	P
Laundry store or plant:			
(i) Retail	X	P	P
(ii) Wholesale (may include retail service)	X	X	X
Lawn mower repair shop:			
(i) Nonmotorized, only	X	P	P
(ii) All others, provided all repairs are performed within a wholly enclosed building	X	P* ³⁷	P* ³⁷
Limousine service:			
(i) Storage of up to 10 limousines (not to include buses and vans), may include routine vehicle repair or servicing within a wholly enclosed building, with no outdoor storage	X	P* ³⁹	P* ³⁹
(ii) All others (CB-120-1994)	X	X	X
Machine shop accessory to an allowed use	X	X	X
Massage establishment	X	P*	P*
Methadone Treatment Center (CB-103-1993)	P*	P*	P*
Model studio	X	X	X
Newspaper publishing establishment	X	P*	P*
Pet grooming shop, provided all animals are confined to the interior of the building and adequate measures are taken to control noise and odor	X	P	P
Photographic processing plant	X	X	X
Photography studio or darkroom	P	P	P
Pizza delivery service, limited to off-premises delivery with no eat-in, drive-in, or carry-out service:			
(i) With less than 2,200 sq. ft. of gross floor area	X	P	P
(ii) Unrestricted in size (CB-83-1986)	X	X	X
Printing shop:			
(i) Not exceeding 2,000 square feet of gross floor area	X	P	P
(ii) All others	X	P*	P*
Sauna or steam bath	X	P	P
Septic tank service	X	X	X

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
Sewage dump station for camping trailers or boats	X	X	X
Shoe repair shop	X	P	P
Tailor or dressmaking shop (may include incidental dyeing and pressing allowed as a "PB" use)	X	P	P
Taxidermy (CB-30-1986)	X	P	P
Travel bureau	P	P	P
Upholstery shop (CB-65-1989)	X	P	P
Veterinarian's office:			
(i) Outpatient	X	P	P
(ii) Inpatient (CB-96-1988)	X	PB	PB
Watch or jewelry repair shop	X	P	P
Welding shop:			
(i) Accessory to an allowed use	X	X	X
(ii) All others	X	X	X
(E) Trade (Generally Retail):			
Adult book store (CB-65-1989; CB-53-1996)	X	X	X
Arts, crafts, and hobby supply store	X	P	P
Bait shop	X	P	P
Bakery products, wholesale (may include retail sales)	X	X	X
Bicycle (sales) shop:			
(i) Nonmotorized, only	X	P	P
(ii) All others	X	P* ³⁷	P* ³⁷
Book (except adult bookstore) or camera store (CB-71-1993)	X	P	P
Bottled gas sales:			
(i) Accessory to an allowed use	X	P	P
(ii) All others	X	P	P
Building supply store:			
(i) Wholly enclosed, except for nursery stock	X	P	P
(ii) With outdoor storage on not more than 50% of the lot, provided it is enclosed by a slightly opaque wall or fence at least 8 feet high (CB-76-1992)	X	X	X
Bulk retailing:			
(i) Products allowed to be sold in a C-S-C Zone (CB-65-1989; CB-25-1999)	X	p ³²	p ³²
(ii) Products allowed to be sold in a C-M Zone	X	X	X
Buying of items within guest rooms and vehicles, pursuant to Section 27-115(a)(2)	X	X	X
Carpet or floor covering store	X	P	P

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
Clothing, dry goods, millinery, or shoe store (CB-58-1985; CB-71-1993)	X	P	P
Confectioner (not exceeding 40,000 square feet of gross floor area):			
(i) Retail (CB-65-1989)	X	P	P
(ii) Wholesale (may include accessory retail sales)	X	X	X
Department or variety store, excluding pawnshops (CB-71-1993; CB-28-1997, CB-4-1999)	X	P	P
Drug paraphernalia display or sales, pursuant to Section 27-115(a)(1)	X	X	X
Drug store:			
(i) Not exceeding 3,000 square feet of gross floor area	X	P	P
(ii) Within an office building or complex, and not exceeding 25% of the gross floor area, or 2,000 square feet, whichever is less (CB-65-1989)	P	P	P
(iii) All others	X	P	P
Farm implement sales	X	X	X
Feed sales	X	X	X
Firewood sales as a temporary use in accordance with Sections 27-260 and 27-261	P	P	P
Farmers' market or flea market as a temporary use, in accordance with Sections 27-260 and 27-261 (CB-63-1998)	X	P	P
Florist shop	X	P	P
Food or beverage goods preparation on the premises of a food or beverage store, provided the goods are only sold on the premises and at retail	X	PB	PB
Food or beverage goods preparation for wholesale sales:			
(i) Not exceeding 1,500 square feet of gross floor area	X	P	P
(ii) Containing 1,501 to 3,000 square feet of gross floor area	X	P*	P*
(iii) All others (CB-37-1992)	X	X	X
Food or beverage store:			
(i) Not exceeding 3,000 square feet of gross floor area	X	P	P
(ii) All others (CB-112-1986; CB-65-1989)	X	P	P
Garden supplies store, floricultural or horticultural nursery, which may include the outdoor display of nursery stock, such as plants, shrubbery, and trees (CB-65-1989)	X	P	P
Gift, jewelry, music, souvenir, or other specialty store not specifically listed (CB-71-1993)	X	P	P
Hardware store (CB-65-1989)	X	P	P
Household appliance or furniture store:			

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
(i) Not exceeding 50,000 square feet of gross floor area	X	P	P
(ii) Exceeding 50,000 square feet of gross floor area (CB-32-1986; CB-77-1998)	X	X	X
Ice vending machine (not exceeding 8 ton capacity)	X	X	X
Lawn mower (sales) store	X	P	P
Newspaper, magazine, or tobacco shop	X	P	P
Nursery and garden center, which may include the outdoor display of nursery stock, such as plants, shrubbery, and trees	X	P	P
Outdoor display of merchandise for sale (except as otherwise specified) and excluding merchandise displayed on gasoline pump islands associated with gas stations which is allowed):			
(i) Not more than 6 feet from main building (subject to Section 27-388)	X	P	P
(ii) More than 6 feet from main buildings(subject to Section 27-388)	X	X	X
Paint or wall covering store	X	P	P
Pawnshop:			
(i) In accordance with Section 27-250.01	X	X	X
(ii) In accordance with Section 27-394.01 (CB-28-1997)	X	X	X
Pet (sales) shop, provided all animals are confined to the interior of the building and adequate measures are taken to control noise and odor; may include the sale of pet feed and supplies (CB-2-1991)	X	P	P
Retail shop or store (not listed) similar to one permitted (P) in the:			
(i) C-S-C Zone	X	P	P
(ii) C-M Zone	X	X	X
(iii) C-R-C Zone (CB-65-1989; CB-58-1990)	X	X	X
Sales from guest rooms and vehicles, in accordance with Section 27-115(a)(2)	X	X	X
Seafood market:			
(i) Containing less than 3,000 square feet of gross retail space	X	P	P
(ii) Containing less than 7,000 square feet of gross retail space	X	P	P
(iii) Unrestricted in size (CB-49-1987)	X	P	P
Seasonal decorations display and sales as a temporary use, in accordance with Sections 27-260 and 27-261	P	P	P
Septic tank sales (CB-65-1989)	X	X	X
Sporting goods shop, which may include marine equipment and supplies	X	P	P
Stationery or office supply store which may include the sale of furniture or business machines	X	P	P
Swimming pool or spa sales and service:			
(i) Excluding outdoor display	X	P	P
(ii) Including outdoor display, provided it is enclosed by a 6-foot high fence (subject to Section 27-388)	X	X	X

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
Toy store (CB-71-1993)	X	P	P
Video game or tape store	X	P	P
Wayside stand:			
(i) As a temporary use, subject to Sections 27-260 and 27-261	P	P	P
(ii) All others (CB-122-1986)	P	P	P
(2) INSTITUTIONAL/EDUCATIONAL:			
Adult day care center	P*	P*	P*
Assisted living facility, subject to the requirements of Section 27-464.04 (CB-72-1996)	X	X	X
Church or similar place of worship, convent, or monastery (CB-23-1988)	P	P	P
Day care center for children:			
(A) In accordance with Section 27-464.02 ¹²	P	P	P
(B) All others (CB-23-1988)	P*	P*	P*
Eleemosynary or philanthropic institution:			
(A) A building containing no more than 7,000 square feet of gross floor area on a lot or parcel with not more than 1.5 acres for use by an organization providing benevolent services; any change in occupant or use shall require Detailed Site Plan approval by the District Council	X	P	P
(B) All others (CB-8-1998)	X	X	X
Hospital (may include a private spa)	P*	P*	P*
Modular classroom as a temporary use, in accordance with Sections 27-260 and 27-261 (CB-106-1989)	P	X	X
Nursing or care home (may include a private spa)	P*	P*	P*
School, Private:			
(A) Driving school, automobile only	P	P	P
(B) For artistic instruction (including a studio)	P	P	P
(C) Of business or trade, where the business or trade is permitted (P) in the respective zone	P	P	P
(D) Of business or trade, where the business or trade is permitted by special exception (SE) in the respective zone	P*	P*	P*
(E) Tutoring establishment	P	P	P
(F) Post-secondary educational institution	P	P	P
(G) Private schools, subject to Section 27-463	P	P	P
(H) All others (CB-40-1988; CB-50-1988; CB-113-1994; CB-93-1996)	P*	P*	P*
(3) MISCELLANEOUS:			
Accessory structures and uses, except as otherwise provided	P	P	P
Adaptive reuse of a surplus public school, when not otherwise allowed	P*	P*	P*

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
Adaptive use of a Historic Site, when not otherwise allowed (CB-58-1987)	P*	P*	P*
Auction house	X	P*	P*
Buildings and uses, serving public health purposes, on land owned by Prince George's County, Maryland, upon which hospitals or health centers are located, except if otherwise allowed as a Permitted (P) use ¹³ (CB-55-1988)	P	P	P
Carpentry, cabinet making, or other woodworking shop:			
(A) Accessory to an allowed use	X	X	X
(B) All others	X	X	X
Cemetery or crematory:			
(A) Cemetery, accessory to a church, convent, or monastery ¹⁸	P	P	P
(B) All others	X	X	X
(CB-11-1991)			
Collection of recyclable materials as a temporary use, in accordance with Sections 27-260 and 27-261	P	P	P
Commercial recreational development (CB-35-2000)	P	X	X
Consolidated Storage (CB-147-1986; CB-65-1989; CB-45-1999; CB-29-2000)	X	X	X
Contractor's office (general) as a permanent use, including the businesses of siding, flooring, roofing, plumbing, air conditioning, heating, painting, carpentry, electrical work, landscaping and the like, with buildings, and uses accessory to the business (as well as the office) use:			
(A) With no outdoor storage of materials or equipment	P	P	P
(B) With outdoor storage of materials, located only in a side or rear yard; enclosed by a slightly, opaque wall or fence at least 6 feet high; with no storing of material higher than the fence; but excluding the use or outdoor storage of earthmoving or other heavy equipment, or outdoor storage of machinery	X	X	X
(C) Including the fabrication (only within a wholly enclosed building) of plumbing, air conditioning, heating, carpentry and lighting (and the like) parts for installation off the site (CB-110-1994; CB-46-1995)	X	X	X
Contractor's office (must include sanitary facilities), construction yard or shed, or storage building (in connection with a construction project) as a temporary use:			
(A) In accordance with Sections 27-260 and 27-261	P	P	P
(B) All others	P*	P*	P*
Hardware fabrication and manufacturing of products from material produced elsewhere ²⁶ (CB-39-1996)	X	X	X
Mobile home, with use for which amusement taxes collected ²	P	P	P
Recycling collection center as a temporary use, in accordance with Sections 27-260 and 27-261	P	P	P
Recycling collection center, paper only (limited to collection, storage, and shipping):			

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
(A) On a lot contiguous to a railroad siding and not abutting land in any Residential Zone (or land proposed to be used for residential purposes on an approved Basic Plan for a Comprehensive Design Zone, approved Official Plan for an R-P-C Zone, or any approved Conceptual or Detailed Site Plan), subject to Section 27-391(a)(2) and (3)	X	X	X
(B) All others	X	X	X
Rental business:			
(A) Rental of motor vehicles or camping trailers(in the C-M Zone subject to the requirements of Section 27-417)	X	P* ³⁹	P* ³⁹
(B) Rental of boats	X	P* ³⁹	P* ³⁹
(C) Rental of any other merchandise allowed to be sold in the respective zone	X	P	P
Sanitary landfill, rubblefill	X	X	X
Sign, in accordance with Part 12:			
(A) Outdoor advertising (billboard)	X	X	X
(B) All others (CB-65-1989; CB-24-1991)	P	P ⁴⁰	P ⁴⁰
Sign shop	X	X	X
Stationery or office supply corporate headquarters including office, showroom, and distribution (no retail sales) also including office furniture as an accessory use, within an office building complex of at least twenty acres (CB-116-1986)	P*	X	X
Storage, wholly enclosed, accessory to an allowed use	P	P	P
Temporary shelter for commercial display, sale, or service use permitted (P) in the respective zones, as a temporary use, in accordance with Sections 27-260 and 27-261	X	P	P
Wholesaling, distribution, and related storage:			
(A) Incidental to any use allowed and in an office building, but limited to a floor area ratio of 0.1	X	X	X
(B) Of materials (products) not used or produced on the premises (CB-61-1995)	X	X	X
Wholesaling of products incidental to the retail sales of the products on the premises	X	PA	PA
(4) PUBLIC/QUASI PUBLIC:			
Ambulance service, private	X	X	X
Community building, except as otherwise provided	P*	P	P
Library, private	P	P	P
Post Office	P	P	P
Public building and use, except as otherwise prohibited	P	P	P
Sanitary landfill or rubblefill ¹⁷ (CB-15-1990)	X	X	X
Voluntary fire, ambulance, or rescue ¹ station	P	P	P
(5) RECREATIONAL/ENTERTAINMENT/SOCIAL/CULTURAL:			
Amusement arcade:			

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
(A) Not exceeding 2,500 square feet of gross floor area, with adult supervision on the premises during all hours of operation; provided the use is located either within a wholly enclosed shopping mall, or within the main group of stores of an integrated shopping center having a minimum gross floor area of 150,000 square feet	X	P	P
(B) All others	X	P*	P*
Amusement Center (CB-35-1994)	X	P	P
Amusement park:			
(A) Within a wholly enclosed shopping mall	X	P*	P*
(B) All others	X	X	X
Archery or baseball batting range	X	P*	P*
Arena or stadium (which may include a private spa)	X	X	X
Athletic field:			
(A) With no seating or nonpermanent bleacher-type seating for not more than 100 spectators	P	P	P
(B) With permanent bleacher-type seating for more than 100 spectators	P*	P*	P*
Auditorium	X	P	P
Beach	X	P	P
Billiard or pool parlor	X	P*	P*
Boat ramp	X	P	P
Bowling alley:			
(A) On a parcel of at least 10 acres, provided all structures are located at least 200 feet from any Residential Zone (or land proposed to be used for residential purposes on an approved Basic Plan for a Comprehensive Design Zone, approved Official Plan for an R-P-C Zone, or any approved Conceptual or Detailed Site Plan)	X	P	P
(B) All others	X	P*	P*
Carnival, circus, fair or similar use, not exceeding seventeen (17) days duration and located at least 250 feet from any dwelling, as a temporary use in accordance with Sections 27-260 and 27-261	P	P	P
Club or lodge (private) except as otherwise provided	P*	P	P
Employees' recreational facilities (private, nonprofit) accessory to an allowed use	P	P	P
Fishing pier	X	P	P
Go-cart track	X	X	X
Golf course or country club:			
(A) Accessory to a commercial use	P	P	P
(B) All others	P*	P*	P*
Golf driving range	P*	P*	P*
Marina (CB-72-1987)	X	X	X
Miniature golf course	P*	P	P
Museum, aquarium, art gallery, cultural center, or similar facility	P	P	P
Park or playground	P	P	P
Race track	X	X	X

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
Recreational campground (in the C-M Zone subject to paragraphs (1) thru (7) of Section 27-400(a))	X	X	X
Recreational or entertainment establishment of a commercial nature, if not otherwise specified:			
(A) Abutting residential property or land residentially zoned	X	P*	P*
(B) All others (CB-72-1998)	X	P*	P*
Reducing/exercise salon or health club	X	P	P
Riding stable	X	X	X
Rifle, pistol, or skeet shooting range:			
(A) Indoor	X	P*	P*
(B) Outdoor	X	X	X
Skating rink	X	P*	P*
Spa (community)	P	P	P
Spa (private), accessory to an allowed dwelling unit	P	P	P
Spa (public):			
(A) Accessory to a hotel or motel	X	P	P
(B) Accessory to a reducing/exercise salon or health club	X	P	P
(C) Accessory to a commercial swimming pool	X	P	P
(D) Accessory to a recreational campground	X	X	X
(E) Accessory to a summer camp	X	X	X
(F) Unrestricted	X	P*	P*
Summer camp	X	X	X
Swimming pool:			
(A) Accessory to a hotel or motel	X	P	P
(B) Accessory to a recreational campground	X	X	X
(C) Community	P	P	P
(D) Indoor	X	P	P
(E) Private, accessory to an allowed one-family detached dwelling	P	P	P
(F) All others	X	X	X
Tennis, basketball, handball, or similar court:			
(A) Indoor (within a permanent wholly enclosed building)	P	P	P
(B) Outdoor	P	P	P
(C) With a temporary removable cover (bubble)	P*	P	P
Theatre:			
(A) Indoor	P*	P	P
(B) Outdoor (including drive-in)	X	X	X
Zoo, not publicly owned	X	X	X
(6) RESIDENTIAL/LODGING:			
Apartment housing for the elderly or physically handicapped	X	P**41	P**41
Country Inn	X	P	P

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
Dwelling, provided that it was legally erected prior to the date upon which the property was classified in a Commercial Zone, or was legally erected in a Commercial Zone under prior regulations	P	P	P
Dwelling unit within a building containing commercial uses:			
(A) Not exceeding 3 units per building, to be located above the ground floor, except where otherwise allowed	P	P ⁴¹	P ⁴¹
(B) Not exceeding 3 units per building, with 1 unit at ground level for a resident manager, caretaker, or night watchman (and family)	X	X	X
(C) In a building containing 4 or more stories, provided the units are located above the third story	P*	P* ⁴¹	P* ⁴¹
Hotel or motel:			
(A) Hotel or motel in general	P	P	P
(B) Including any use allowed in the C-S-C Zone (but not generally allowed in the C-M Zone, excluding those permitted by special exception), when located within a hotel, provided the uses shall not be located above the ground floor; not more than fifteen (15) percent of the gross floor area of the building shall be devoted to the uses; and not more than 3,000 square feet shall be allotted to any one use (CB-105-1985; CB-58-1990)	X	X	X
Tourist cabin camp	X	X	X
Tourist home	X	P	P
Transitional shelter for the homeless:			
(A) Operated in conjunction with an adjacent eleemosynary institution; and containing 8 or fewer residential units	P	X	X
(B) All others (CB-62-1991)	X	X	X
(7) RESOURCE PRODUCTION/RECOVERY:			
Agricultural use	P	P	P
Sand and gravel wet-processing	X	X	X
Surface mining	X	X	X
(8) TRANSPORTATION/PARKING/COMMUNICATIONS/UTILITIES:			
Airport, airpark, airfield, airstrip, heliport, helistop	P*	P*	P*
Broadcasting studio (without tower)	P	P	P
Bus station or terminal	X	P*	P*
Parking garage, commercial	P	P	P
Parking garage or lot or loading area, used in accordance with Part 11	P	P	P
Parking lot, commercial:			
(A) With shuttle service to Metro and within two (2) miles of a Metro station	P*	P	P
(B) All others	P*	P*	P*
Parking of mobile home, except as otherwise specified	X	X	X
Parking of a mobile home in a public right-of-way ⁴	X	X	X
Parking of vehicles accessory to an allowed use	P	P	P

USE	C-O	C-S-C	M-U-I
	in DDOZ	in DDOZ	in DDOZ
Public utility use or structure:			
(A) Underground pipelines and electric power and telephone lines; structures commonly known as "telephone poles"; and railroad tracks (but not yards) and passenger stations	P	P	P
(B) Unmanned, self-contained telecommunications electronic equipment enclosures not more than 350 square feet in area, not more than 12 feet in height, having parking and access easements for one (1) vehicle where necessary, with minimum on-site visits of personnel, provided the enclosure is entirely surrounded with screening material. These structures must be used exclusively for the purpose of providing advanced communications services ¹¹	P	P	P
(C) Antennas, monopoles, and related equipment buildings for mobile telephone or other telecommunications transmission (excluding supporting towers and satellite dishes) in accordance with Section 27-464.03	P	P	P
(D) Other public utility uses or structures (including major transmission and distribution lines and structures, but excluding towers and poles not otherwise permitted, railroad yards, roundhouses, car barns, and freight stations) (CB-25-1987; CB-61-1988; CB-8-1990; CB-123-1994; CB-102-1997)	P*	P	P
Satellite dish antenna, in accordance with Section 27-451.01:			
(A) Up to 10 feet in diameter, to serve only 1 dwelling unit	P	P	P
(B) More than 10 feet in diameter to serve only 1 dwelling	X	X	X
(C) All others (CB-19-1985)	P	P	P
Storage of any motor vehicle which is wrecked, dismantled or not currently licensed, except where specifically allowed ⁶ (CB-4-1987)	X	X	X
Taxicab dispatching station:			
(A) Without cab storage, repair, or servicing	P	P	P
(B) With cab storage	X	P* ³⁹	P* ³⁹
(C) With cab repair or servicing within a wholly enclosed building (CB-50-1987)	X	X	X
Taxicab stand	P	P	P
Telegraph or messenger service	P	P	P
Tower, pole, or antenna (electronic, public utility when not otherwise permitted, radio, or television, transmitting or receiving), except a satellite dish antenna:			
(A) Nonprofit, noncommercial purposes, with no height restrictions	P	P	P
(B) Freestanding for commercial purposes, not exceeding 100 feet above ground level	P	P	P
(C) Attached to a roof for commercial purposes, not exceeding 40 feet above the height of the building	P ²³	P ²³	P ²³
(D) All others (CB-8-1990; CB-41-1994; CB-123-1994)	P*	P*	P*

- 1** Provided the site is either:
 - (A) In the proximity of an area designated as a fire or rescue station on an approved Functional Master Plan of Fire and Rescue Stations;
 - (B) In a location that the Fire Chief has indicated (in writing) is appropriate; or
 - (C) Occupied by a station that was in use immediately prior to July 1, 1982.
- 2** Provided:
 - (A) The mobile home is located on a lot having a net area of at least five (5) acres;
 - (B) The use of the mobile home is in connection with another use on the property for which the county levies or collects an amusement tax;
 - (C) The occupants of the mobile home are employed by or reasonably connected with the other use; and
 - (D) The mobile home shall not be located on the property for more than 120 cumulative days per calendar year, except mobile homes used in connection with pari-mutuel racetracks when the use shall not exceed 218 cumulative days per calendar year.
- 3** Provided:
 - (A) The service shall be limited to supplying gasoline, oil, water, tire pressure, and washing;
 - (B) Only automobiles parking in the parking garage may be served;
 - (C) No signs visible from outside the structure shall indicate the presence of the service facilities; and
 - (D) The garage shall be wholly enclosed.
- 4** Except in an emergency. In this case, the parking shall be subject to the traffic and parking regulations applicable to the right-of-way.
- 5** Reserved.
- 6** This shall not apply to:
 - (A) Storage accessory (and related) to an allowed use; or
 - (B) One (1) such vehicle stored in a wholly enclosed garage.
- 7** Approval as an accessory use with approval of the special exception for the hotel or motel.
(CB-28-1985)
- 8** Approval as an accessory use with approval of the special exception for the recreational campground.
(CB-28-1985)
- 9** Provided:
 - (A) The minimum seating capacity is 150;
 - (B) More than 50 percent of its revenue is derived from the sale of food;
 - (C) The operation is limited to the sale of food and beverages for consumption on the premises;
 - (D) Customer service is at table side. No counter service and no cafeteria-style service is provided; and
 - (E) The restaurant is not open to the public before 11:00 a.m. (CB-104-1985)
- 10** The requirement for at least 6 businesses and a 50,000 square feet minimum gross floor area does not apply to a fast-food restaurant which is legally existing or which was subsequently constructed pursuant to a building permit filed prior to May 6, 1986.
(CB-29-1986)
- 11** This does not provide for accessory antennas or overhead distribution lines.
(CB-25-1987)
- 12** In a publicly owned recreational facility, a school, a church, or a public building, a day care center shall only be permitted as an accessory use. A church must provide its tax-exempt identification number when applying for a Detailed Site Plan or a building or use and occupancy permit for an accessory day care center for children.
(CB-23-1988; CB-98-1988; CB-44-1989)
- 13** Provided the health center is located on a minimum of 25 acres.
(CB-55-1988)

- 14 Provided it is an adaptive reuse of existing space, such space having been previously utilized for bulk retailing, and only where the property on which the use is located abuts land in the I-3 Zone.
(CB-61-1988; CB-81-1993; CB-123-1994; CB-61-1995)
- 15 May include an accessory crematory.
(CB-2-1989)
- 16 Delivery service is permitted provided an additional parking space, over and above the required number of parking spaces, is provided for each vehicle used for delivery. No more than six (6) vehicles shall be permitted for the delivery service.
(CB-126-1989)
- 17 A sanitary landfill or rubblefill may include a rock crusher only if it is approved as part of the special exception.
(CB-15-1990)
- 18 Provided both uses were existing as of January 1, 1991.
(CB-11-1991)
- 19 For:
 - (A) The relocation of such uses, provided the last site on which the use was located was in the I-1 Zone, not more than three miles from the subject property, is currently used by a public entity for a mass transit facility, and was acquired prior to June 1, 1993; or
 - (B) A property of 15,000 to 20,000 square feet, formerly the site of a full-service gas station, abutting on at least one side property in the C-S-C Zone with frontage on a public street of lower than collector classification on the applicable master plan, limited to repair of vehicles with a maximum gross vehicle weight of 17,000 pounds.
(CB-50-1993; CB-68-1999)
- 20 Provided the use is on a parcel or contiguous parcels of land in the C-M Zone, the gross tract area of which is a minimum of fifty (50) acres, which is contiguous to an existing street right-of-way at least 120 feet wide, and of which no more than 35 percent is occupied by the uses subject to this requirement. The entire tract of land in the C-M Zone shall require Detailed Site Plan approval in accordance with Part 3, Division 9 of this Subtitle. Each use subject to these requirements shall consist of at least 25,000 square feet of gross floor area, and uses consisting of less than 50,000 square feet of gross floor area are permitted only if there is one existing retail use consisting of more than 100,000 square feet of gross floor area for every 2 retail uses consisting of less than 50,000 square feet of gross floor area. Clothing, dry goods, millinery, and shoe stores shall be permitted by special exception in all other cases. For the purposes of this footnote, the word "contiguous" shall include parcels that are separated only by a right-of-way. However, a department or variety store consisting of no more than 15,000 square feet of gross floor area shall be permitted upon a parcel or contiguous parcels of land in the C-M Zone, the gross tract area of which is no more than 3 acres, and which is contiguous to an existing street right-of-way at least 120 feet wide, subject to Detailed Site Plan approval in accordance with Part 3, Division 9, of this Subtitle.
(CB-71-1993; CB-70-1998)
- 21 If the use has a valid use and occupancy permit as of September 30, 1993, and a sight-tight fence or wall at least six feet in height is erected along the perimeter of all abutting residential property as of December 31, 1993, the use shall be permitted by right. Change in ownership of the use shall not affect the conforming use status.
(CB-89-1993)
- 22 Subject to Detailed Site Plan approval, in accordance with Part 3, Division 9, of this Subtitle, if the use is abutting land in a residential zone, or land proposed to be used for residential purposes on an approved Basic Plan, approved Official Plan, or any approved Conceptual or Detailed Site Plan.
(CB-90-1993)
- 23 Provided the building to which it is attached is at least 50 feet in height. Otherwise, a special exception is required.
(CB-41-1994)
- 24 Subject to Detailed Site Plan approval in accordance with Part 3, Division 9, of this Subtitle.
(CB-120-1994)

- 25 Provided the property abuts property in a commercial zone, a residential zone in common ownership with the subject property, or a transportation facility right-of-way.
(CB-46-1995)
- 26 Provided the property on which the use is located is under single ownership which includes the I-1 and C-M zones, where the uses on the C-M zoned portion are an expansion of the currently existing uses on the I-1 zoned portion of the property.
(CB-39-1996)
- 27 Provided the property on which the use is located is abutting an existing vehicle storage yard with a valid use and occupancy permit.
(CB-80-1996)
- 28 If not conducted in an existing office building, a Detailed Site Plan shall be approved in accordance with Part 3, Division 9, of this Subtitle.
(CB-93-1996)
- 29 Provided that a certified nonconforming pawnshop that does not meet the location requirements of Section 27-394.01 may relocate without the necessity of obtaining a special exception if locational requirements of the proposed location are satisfied and the nonconforming use at the former location is discontinued.
(CB-28-1997)
- 30 Except when located on a tract of land which is less than 1.5 acres in size and surrounded on all sides by land in any residential zone in order to ensure safety on contiguous parcels and to minimize negative aesthetic impact on neighboring areas. A maximum of one (1) monopole and two (2) antennas are permitted. For any use for which the original permit was applied for prior to November 25, 1997, and legally issued, telecommunications-related equipment may be moved inside an existing structure without obtaining a special exception for the alteration of a nonconforming use.
(CB-102-1997; CB-38-1998)
- 31 Provided:
- (A) The store shall be no less than 10,000 square feet nor more than 15,000 square feet;
 - (B) The use involves the demolition and redevelopment of a commercial use that has been vacant for a minimum of five years; and
 - (C) The site contains a minimum of three acres and is adjacent to, or across from, an enclosed mall of at least 500,000 square feet.
- (CB-4-1999)
- 32 If located outside a Revitalization Tax Credit Area in a commercial center with less than 30 acres, a bulk retailing store may not have gross floor area greater than 50,000 square feet. But if the store was in use and had necessary permits issued on or before September 1, 1998, then the restriction in this note does not apply and the store is not subject to nonconforming use requirements in Part 3, Division 6, unless the store discontinues bulk retailing operations for 180 or more consecutive calendar days. In this note, a commercial center is one or more contiguous, commercially zoned lots separated from other commercially zoned lots by public streets or rights-of-way.
(CB-25-1999)
- 33 Provided the use does not exceed 5,000 square feet.
(CB-34-1999)
- 34 Limited to the adaptive reuse of vacant or partially vacant property in former or existing shopping centers which are limited in their ability to modify or expand. The C-S-C parcels in the shopping center shall:
- (A) Lie adjacent to federal government property;
 - (B) Include not less than 10 or more than 15 acres;
 - (C) Be confined by road networks which limit access changes to the parcels; and
 - (D) Lie contiguous to and below the grade of a multilane limited-access highway.
- All such consolidated storage units shall meet the requirements of Sections 27-375(a)(5), (6) and (7), and 27-281 through 27-290.
(CB-45-1999)

- 35** The use is permitted on C-O zoned property abutting R-R zoned property leased from a public agency before January 1, 1974, where both properties are developed in an integrated fashion pursuant to the same agreement. Parking and loading facilities shall be provided in accordance with Part 11 (parking and loading requirements). Landscaping, buffering, and screening shall be provided in accordance with the *Landscape Manual*. Development regulations for building setbacks shall be provided in accordance with Part 6 (Commercial Zone regulations). There shall be no building setbacks required between the internal R-R and C-O zoned portions of the site.
The following uses are not permitted: car wash, animal hospital, training, kennel, grooming, blacksmith, carpet or rug shampooing, electric or gas appliance repair, farm implement sales and repair, upholstery or furniture repair, locksmith, laboratories, lawn mower repair, machine shop, massage establishment, methadone treatment center, model studio, photo processing plant, studio or darkroom, pizza delivery, print shop, newspaper publishing, sauna or steam bath, septic tank sales, service, sewage dump (pump out) services, shoe repair, taxidermy, welding shop, bait shop, bottled gas, feed sales, wayside stand, and any use prohibited in the lease with the public agency, as modified or amended.
(CB-35-2000)
- 36** Special exception applications filed prior to January 1, 2000, may continue through the review and hearing procedures in Part 4. Uses which are approved may continue in effect, may be revised or amended under procedures in Part 4, and shall not be considered nonconforming. The maximum height of structures not approved by January 1, 2000, shall be 36 feet.
(CB-29-2000)
- 37** Future new uses are not permitted in the town center and main street subareas (Subareas 1 and 3) of the College Park US 1 Corridor Sector Plan.
- 38** Future new uses are only permitted in the town center and main street subareas (Subareas 1 and 3) of the College Park US 1 Corridor Sector Plan and are subject to pertinent Development District Standards.
- 39** Future new uses are only permitted in the mixed-commercial subareas (Subareas 6b, 4f, portions of 4e north of Fox Street, and portions of 4d north of Erie Street) of the College Park US 1 Corridor Sector Plan and are subject to pertinent Development District Standards.
- 40** Permitted, but must be developed in accordance with development standards governing freestanding signs and building signage in the College Park US 1 Corridor Sector Plan.
- 41** Permitted when developed in accordance with the intent of the College Park US 1 Corridor Sector Plan and associated development standards which provide that residential development may be located above the ground floor of buildings that contain ground floor or street level commercial uses and may include more than three units per building with no provision for a ground floor resident manager unit.
- 42** Bedroom unit percentage limitations as required by Section 27-419 do not apply.
- 43** A check cashing establishment is a private business, whose check cashing practices and procedures are not federally regulated or audited. Institutions that perform financial transactions such as receiving, investing, and lending money, that are federally regulated and audited (banks), shall not be considered check cashing establishments.

USES PERMITTED.

27-441. Uses permitted.

- (a) No use shall be allowed in the Residential Zones, except as provided for in the following Table of Uses. In the table, the following applies:
- (1) The letter "P" indicates that the use is permitted in the zone indicated.
 - (2) The letter "P*" indicates that the use is permitted, but subject to the general special exception standards in Section 27-317(a)(1), (4), (5) and (6) and conforms to the recommendations of the sector plan.
 - (3) The letters "PA" indicate that the use is permitted, subject to the following:
 - (A) There shall be no entrances to the use directly from outside of the building;
 - (B) No signs or other evidence indicating the existence of the use shall be visible from the outside building, other than a business identification sign lettered on a window. The sign shall not exceed six (6) square feet in area; and
 - (C) The use shall be secondary to the primary use of the building.
 - (4) The letters "PB" indicate that the use is permitted, subject to the following:
 - (A) The use shall be related to, dependent on, and secondary to a principal use on the premises;
 - (B) The use shall be located on the same record lot as the principal use;
 - (C) The use shall not be located within a building not occupied by the principal use; and
 - (D) The floor area of any building (and the land area occupied by any structure other than a building) devoted to the use shall not exceed an area equal to 45 percent of the gross floor area of the building within which the principal use is located.
 - (5) The letter "X" or a blank (unless otherwise clear from the context) indicates that the use is prohibited.
 - (6) All uses not listed are prohibited.
 - (7) Whenever the tables refer to an allowed use, that use is either permitted (P), permitted as a (P*) but subject to certain general special exception (SE) provisions in accordance with Part 4 of this Subtitle, or permitted as a (PA) or (PB) use, as listed in the tables.
 - (8) Footnote 67 stipulates that development should be in accordance with the development standards of the College Park/US 1 Corridor Sector Plan and the residential uses are permitted above ground floor commercial uses when developed in accordance with the development standards provided in the sector plan. Footnote 68 refers to bedroom percentages permitted for The University of Maryland student housing.

Table 18

(b) TABLE OF USES—Residential Zones

USE	ZONE				
	R-O-S (in DDOZ)	O-S (in DDOZ)	R-R (in DDOZ)	R-55 (in DDOZ)	M-U-I (in DDOZ)
(1) COMMERCIAL:					
Animal Hospital, veterinary office	P*	P*	P*	X	
Antique shop	X	P*	P*	X	
Bus maintenance accessory to a private school, church, or other place of worship (CB-23-1988)	X	P*	P*	P*	
Buying of items within guest rooms and pursuant to Section 27-115(a)(2)	X	X	X	X	
Collection of recyclable materials as a temporary use, in accordance with Sections 27-260 and 27-261	P	P	P	P	
Commercial recreational development (CB-35-2000)	X	X	P ⁶⁶	X	
Contractor's office (must include sanitary facilities), construction yard or shed, or storage building (in connection with a construction project) as a temporary use:					
(A) Subject to Sections 27-260 and 27-261	X	P	P	P	
(B) All others	X	P*	P*	P*	
Distillery for the production of fuel alcohol	X	X	X	X	
Drug paraphernalia display or sales, pursuant to Section 27-115(a)(1)	X	X	X	X	
Farm implement sales or repair; farm supplies sales	X	X	X	X	
Farmers' market or flea market as a temporary use, in accordance with Sections 27-260 and 27-261 (CB-63-1998)	P	P	P	P	
Firewood sales as a temporary use, in accordance with Sections 27-260 and 27-261	P	P	P	P	
Funeral parlor, undertaking establishment	X	P*	P*	P*	
Kennel:					
(A) On a lot having a net area of 20,000 sq. ft. or less	X	P*	P*	X	
(B) On a lot having a net area between 20,000 sq. ft. and 80,000 sq. ft.	X	P	P	X	
(C) On a lot having a net area exceeding 80,000 sq. ft. (CB-37-1991; CB-16-1993)	P	P	P	X	
Landscaping contractor's business (CB-10-1996)	P*	P*	P*	X	
Limited professional uses in multifamily projects	X	X	X	X	
Monument and headstone sales establishment (CB-60-1998)	X	X	P ⁶¹	X	
Offices:					
(A) Accountants, architects, clergymen, engineers, lawyers, medical practitioners, and similar recognized and learned professions, as an accessory use in a dwelling	P ⁷	P ⁷	P ¹⁹	P ¹⁹	

USE	R-O-S (in DDOZ)	O-S (in DDOZ)	R-R (in DDOZ)	R-55 (in DDOZ)	M-U-I (in DDOZ)
(B) Business office and model apartments in a multifamily dwelling or multifamily project and used only in connection with the sale, rental, operation, service, and maintenance of the dwelling or project (CB-36-1987)	X	X	X	X	
(C) General business and professional offices	X	X	X	X	
(D) Insurance sales office as an accessory use in a dwelling	X	X	P*	P*	
(E) Medical practitioner's office in a one-family dwelling (except as provided in (A) above)	X	X	P*	P*	
(F) Real estate sales office as an accessory use in a dwelling	X	X	P*	P*	
(G) Real estate subdivision sales office as a temporary use:					
(i) Subject to Sections 27-260 and 27-261	X	P	P	P	
(ii) All others	X	P*	P*	P*	
(H) Multifamily dwelling management company (must manage the project within which it is located)	X	X	X	X	
(I) Temporary trailer for office space accessory to an existing group residential facility, which services more than eight (8) persons, in accordance with Sections 27-260 and 27-261 (CB-35-1996)	X	P	X	X	
Parking lot, required, serving adjacent Commercial or Industrial Zone	X	P*	P*	P*	
Photography studio and darkroom, as an accessory use solely by the resident of a one-family detached dwelling and located within such dwelling (CB-140-1986)	X	X	X	P*	
Retail sales and consumer service establishment (CB-140-1986)	X	X	X	X	
Riding stable:					
(A) On a tract consisting of less than 20,000 sq. ft.	X	P*	X	X	
(B) On a tract consisting of between 20,000 sq. ft. and 9 contiguous acres	P	P*	P*	X	
(C) All others (CB-117-1991; CB-5-1992)	P*	P	P*	X	
Seasonal decorations display and sales as a temporary use, in accordance with Sections 27-260 and 27-261 ⁴³ (CB-23-1989)	P	P	P	P	
Waterfront Entertainment/Retail Complex, in accordance with Section 27-445.08 (CB-44-1997)	P	P	P	X	
Wayside stand as a temporary use:					
(A) Subject to Sections 27-260 and 27-261	P	P	P	P	
(B) All others	X	X	X	X	
(2) INSTITUTIONAL/EDUCATIONAL:					
Adult day care center	X	P*	P*	P*	
Chancery, on a lot having a net area of at least 15 acres	X	X	X	P	
Church or similar place of worship:					

USE	R-O-S (in DDOZ)	O-S (in DDOZ)	R-R (in DDOZ)	R-55 (in DDOZ)	M-U-I (in DDOZ)
(A) Located on a lot less than 1 acre in size	X	X	P*	P*	
(B) Located in a building that was originally constructed as a dwelling, on a lot less than 1 acre in size	X	X	X	X	
(C) Located on a lot between 1 and 2 acres in size ⁵²	X	X	P	P	
(D) Located in a building that was originally constructed as a dwelling, on a lot between 1 and 2 acres in size ⁵²	X	X	X	X	
(E) All others (CB-23-1988; CB-23-1993; CB-76-1993)	P*	P ⁵³	P	P	
Day care center for children:					
(A) Accessory to a publicly owned recreational facility, a school, a surplus school building, improved property (other than a school) that is under the control of the Board of Education, a church, a public building, or a community building, in accordance with Section 27-445.03 ³⁴	P	P	P	P	
(B) Accessory to a multifamily dwelling or project when located within a community room for the sole use of the residents or employees, in accordance with Section 27-445.03	X	X	X	X	
(C) Accessory to a multifamily development when located within an existing building in accordance with Section 27-445.03	X	P	P	P	
(D) All others (CB-23-1988; CB-44-1989; CB-24-1999)	P*	P*	P*	P*	
Eleemosynary or philanthropic institution:					
(A) An adaptive reuse of a structure last occupied by a Federal postal facility on a lot or parcel not more than 25,000 square feet in area for use by an organization serving the homebound	P*	P*	P	P*	
(B) A building containing no more than 7,000 square feet of gross floor area on a lot or parcel with not more than 1.5 acres for use by an organization providing benevolent services; for a permitted use, any change in occupant or use shall require Detailed Site Plan approval by the District Council	P*	P*	P*	P	
(C) All others (CB-78-1997; CB-8-1998)	P*	P*	P*	P*	
Family day care	P	P	P	P	
Health campus	X	X	P*	P*	
Hospital	X	P*	P*	P*	
Medical/residential campus	X	P*	P*	P*	
Modular classroom as a temporary use, in accordance with Sections 27-260 and 27-261 (CB-106-1989)	P	P	P	P	
Nursing or care home (may include a private spa)	X	P*	P*	P*	
School, private:					
(A) In accordance with Section 27-443	X	P	P	P	
(B) All others	P*	P*	P*	P*	

USE	R-O-S (in DDOZ)	O-S (in DDOZ)	R-R (in DDOZ)	R-55 (in DDOZ)	M-U-I (in DDOZ)
Small group child care center (CB-131-1993)	P	P	P	P	
(3) MISCELLANEOUS:					
Accessory structures and uses (when not otherwise provided for)	P	P	P	P	
Adaptive reuse of a surplus public school, when not otherwise allowed	P*	P*	P*	P*	
Adaptive use of a Historic Site, when not otherwise allowed (CB-58-1987)	P*	P*	P*	P*	
Animals, not customarily household pets (CB-117-1986; CB-55-1988)	X	X	X	P*	
Buildings and uses, serving public health purposes, on land owned by Prince George's County, Maryland, upon which hospitals or health centers are located, except if otherwise allowed as a Permitted (P) use ⁴¹ (CB-55-1988)	P	P	P	P	
Cemetery, crematory:					
(A) Cemetery, in accordance with Section 27-445.06	P*	P*	X	X	
(B) Cemetery, accessory to a church, convent, or monastery ⁴⁹	P*	P	P	P	
(C) All others (CB-86-1989; CB-11-1991)	P*	P*	P*	P*	
Home occupations for residents, when not otherwise allowed ²⁰ (CB-86-1989)	P	P	P	P	
Increase in height of accessory building, used for:					
(A) Servant, household help living quarters ³⁰	P*	P*	P*	P*	
(B) Agricultural purposes on a lot having a net area of less than 5 acres	P*	P*	P*	X	
(C) Agricultural purposes on a lot having a net area of at least 5 acres	P	P	P	X	
(D) Office	X	X	X	X	
Signs, in accordance with Part 12, associated with uses allowed in the applicable Residential Zone (CB-85-1988)	P ⁶⁹	P ⁶⁹	P ⁶⁹	P ⁶⁹	
Signs, outdoor advertising (Billboards) (CB-85-1988)	X	X	X	X	
Temporary structures and uses not otherwise allowed	P*	P*	P*	P*	
(4) PUBLIC/QUASI PUBLIC:					
Library	P	P	P	P	
Public buildings and uses, except as otherwise provided	P	P	P	P	
Sanitary landfill or rubblefill ⁴⁷ (CB-15-1990)	X	X	X	X	
Voluntary fire, ambulance, or rescue squad services ²⁶	P	P	P	P	
(5) RECREATIONAL/ENTERTAINMENT/SOCIAL/CULTURAL:					
Archery range, privately owned and commercially operated on land leased from, and owned by, a public agency	P	P	P	X	

USE	R-O-S (in DDOZ)	O-S (in DDOZ)	R-R (in DDOZ)	R-55 (in DDOZ)	M-U-I (in DDOZ)
Athletic field, outdoor, private nonprofit (CB-43-1994)	P*	P ⁵⁵	P ²⁷	P*	
Boathouse (private) as an accessory use	P	P	X	X	
Carnival, circus, fair, or similar use, not exceeding 17 days duration and only on a parking lot as a temporary use in accordance with Sections 27-260 and 27-261	P	P	P	P	
Club, private	P*	P*	P*	P*	
Commercial recreational attraction	X	P*	P*	X	
Commercial recreational facilities (privately owned) on land leased from a public agency, except as otherwise allowed:					
(A) Leased on or after January 1, 1974	P*	P*	P*	X	
(B) Leased before January 1, 1974	P*	P*	P	X	
Community building or similar nonprofit social use, not publicly owned or operated:					
(A) Only for residents and guests	P*	P*	P*	P*	
(B) All others	P*	P*	P*	P*	
(CB-85-1988; CB-33-1989)					
Conference center and uses accessory thereto (such as restaurants, tennis courts, auditoriums, swimming pools, racquetball courts, riding stables, golf courses, or other recreational, physical fitness, or educational activities) privately owned and commercially operated, on a tract having a gross area of at least 500 acres, owned by a public agency, on which a public golf course is operated on a regular basis	P*	P	X	X	
Courts (indoor or outdoor) (tennis, handball, racquetball, or volleyball), not including courts accessory to a dwelling:					
(A) Privately owned and commercially operated on land leased from, and owned by, a public agency ⁵⁶	P	P	P	X	
(B) All others	X	X	X	X	
(CB-47-1995)					
Golf course:					
(A) At least 18 holes on a tract having a gross area of at least 200 acres; provided that any accessory recreational facilities shall be located at least 100 feet from the nearest property line and effectively screened from view of any adjoining land in a Residential Zone, or land proposed to be used for residential purposes on an approved Basic Plan for a Comprehensive Design Zone, approved Official Plan for an R-P-C Zone, or any approved Conceptual or Detailed Site Plan, not on publicly owned land	P*	P*	P*	P*	
(B) Privately owned and commercially operated on land leased from, and owned by, a public agency ⁵⁶	P	P	P	X	
(C) All others	P*	P*	P*	P*	
(CB-47-1995)					
Golf course, miniature (indoor or outdoor):					
(A) Privately owned and commercially operated on land leased from, and owned by, a public agency ⁵⁶	P	P	P	X	

USE	R-O-S (in DDOZ)	O-S (in DDOZ)	R-R (in DDOZ)	R-55 (in DDOZ)	M-U-I (in DDOZ)
(B) All others (CB-47-1995)	X	P*	P*	X	
Golf driving range:					
(A) Privately owned and commercially operated on land leased from, and owned by, a public agency ⁵⁶	P	P	P	X	
(B) All others (CB-47-1995)	P*	P*	P*	X	
Homes Association Recreational Use, in accordance with Section 27-445	P*	P	P	P	
Museum, art gallery, aquarium, cultural center, or similar facility (noncommercial)	P*	P*	P	P	
Racetrack, including pari-mutuel	X	X	X	X	
Racetrack, pari-mutuel only	X	P*	P*	X	
Recreational campground	P*	P*	P*	X	
Recreational program, before- and after-school	P	P	P	P	
Recreational use (nonprofit) not publicly owned or operated, when not otherwise allowed:					
(A) Only for residents and guests	P*	P*	P*	P*	
(B) All others (CB-33-1989)	P*	P*	P*	P*	
Saunas, solariums, and health clubs, noncommercial, for the sole use of residents and their guests	X	X	X	X	
Shooting range (rifle, pistol, or skeet):					
(A) On a lot having a net area of at least 20 acres, and subject to annual renewal	X	X	X	X	
(B) All others	X	X	P*	X	
Skating facility:					
(A) Privately owned and commercially operated on land leased from, and owned by, a public agency ⁵⁶	P	P	P	X	
(B) All others (CB-89-1994; CB-47-1995)	P*	P*	P*	X	
Spa, private	P*	P	P	P	
Spa, community	P*	P*	P*	P*	
Stable, private (CB-29-1985)	P ³⁵	P ³⁵	P ³⁵	P ^{35,37}	
Swimming pool (community) for sole use of residents and their guests, in accordance with Section 27-411	X	X	X	X	
Swimming pool (community), in accordance with Section 27-411	P*	P*	P*	P*	
Swimming pool (private):					
(A) Accessory to a one-family detached dwelling	P	P	P	P	
(B) Accessory to other dwellings	X	X	X	X	
Swimming pool, privately owned and commercially operated on land leased from, and owned by, a public agency ⁵⁶ (CB-47-1995)	P	P	P	X	
(6) RESIDENTIAL/LODGING:					

USE	R-O-S (in DDOZ)	O-S (in DDOZ)	R-R (in DDOZ)	R-55 (in DDOZ)	M-U-I (in DDOZ)
Agricultural Preservation Development, Section 27-445.01	X	X	P	X	
Apartment hotel	X	X	X	X	
Apartment housing for elderly or handicapped families in a building other than a surplus public school building (with provisions for increased density and reduced lot size in Multifamily Zones) (CB-85-1988; CB-91-1991; CB-44-1992)	X	X	P*	P*	
Apartment housing for elderly or handicapped families in a surplus public school building	P*	P*	P*	P*	
Boardinghouse	P*	P	P	X	
Congregate living facility for more than eight elderly or physically handicapped residents (CB-90-1985)	P*	P*	P*	P*	
Congregate living facility for not more than eight elderly or physically handicapped residents (CB-90-1985)	P	P	P	P	
Convent or monastery (CB-23-1993)	P	P	P	P	
Conversion of one-family detached dwelling to a building containing up to three dwelling units (not considered as a two-family, three-family, or multifamily dwelling): ⁵⁷					
(A) Prior to November 29, 1949, if the owner of the building resides in the building, and a valid Use and Occupancy permit was in effect on July 1, 1986	X	X	P	P	
(B) Prior to November 29, 1949, if the owner of the building does not reside in the building, or a valid Use and Occupancy permit was not in effect on July 1, 1986	X	X	P*	P*	
(C) Prior to November 18, 1980, but on or after November 29, 1949	X	X	P* ³⁷	P* ³⁷	
(D) On or after November 18, 1980 (CB-58-1986; CB-73-1996)	X	X	X	X	
Country Inn	P*	P*	P*	P*	
Dwelling, farm tenant	P	P	P ³⁶	X	
Dwelling, multifamily:					
(A) In general	X	X	X	X	
(B) Subject to applicable bedroom percentages	X	X	X	X	
(C) In excess of applicable bedroom percentages	X	X	X	X	
(D) Restricted to one-bedroom and efficiency apartments	X	X	X	X	
(E) Higher than 110 feet (CB-85-1988)	X	X	X	X	
Dwelling, one-family attached, for the elderly ⁵⁸ (CB-71-1996)	X	X	P*	X	
Dwelling, one-family detached, cluster development	X	X	P	P	
Dwelling, one-family detached (in general)	P	P	P	P	

USE	R-O-S (in DDOZ)	O-S (in DDOZ)	R-R (in DDOZ)	R-55 (in DDOZ)	M-U-I (in DDOZ)
Dwelling, one-family semidetached ¹ (CB-85-1988)	X	X	P ¹³	P ^{13,32}	
Dwelling, quadruple-attached (CB-83-1997)	X	X	X	X	
Dwelling, three-family	X	X	X	X	
Dwelling, two-family detached (CB-85-1988)	X	X	X	X	
Dwelling, two-family (in general)	X	X	X	X	
Dwellings, one-family attached, cluster development, shown on a preliminary plat of subdivision approved prior to September 1, 1986	X	X	X	P ³³	
Dwellings, one-family triple-attached, cluster development, shown on a preliminary plat of subdivision approved prior to September 1, 1986	X	X	X	P ³²	
Dwellings, one-family triple-attached (in general)	X	X	X	X	
Flag lot development:					
(A) In accordance with preliminary plats approved prior to February 1, 1990, pursuant to Subtitle 24 and recorded within the prescribed time period	X	X	P	P	
(B) In accordance with Section 24-138.01 of Subtitle 24 (CB-72-1989)	X	X	P	X	
Fraternity or sorority house:					
(A) If legally existing prior to May 20, 1983, and not extended beyond the boundary lines of the lot as it legally existed (prior to May 20, 1983)	X	X	X	X	
(B) All others	X	X	X	X	
Group residential facility for more than eight mentally handicapped dependent persons, or for five or more other dependent persons	P*	P*	P*	P*	
Group residential facility for not more than eight mentally handicapped dependent persons	P	P	P	P	
Guest house, as an accessory use	P	P	X	X	
Mobile home used as a dwelling for emergency purposes as a temporary use, in accordance with Sections 27-260 and 27-261	P	P	P	X	
Mobile home used as a one-family detached dwelling (CB-79-1999)	P*	P*	X	X	
Mobile home, with use for which amusement taxes collected ²⁸	X	X	X	X	
Motel	X	X	X	X	
Opportunity housing dwelling units (CB-66-1991)	X	X	P	P	
Planned retirement community	X	X	P*	P*	
Recreational community development, in accordance with Section 27-444 (CB-16-1989)	P*	P	P ⁴²	X	
Rental of guest rooms (by the residents):					
(A) To 1 or 2 persons (unrelated to all principal residents)	P	P	P	X	
(B) To 3 persons (unrelated to all principal residents)	P	P	P	X	

USE	R-O-S (in DDOZ)	O-S (in DDOZ)	R-R (in DDOZ)	R-55 (in DDOZ)	M-U-I (in DDOZ)
(C) To not more than 3 persons (unrelated to all principal residents) by a family of related individuals, 1 individual, or 2 unrelated individuals (CB-122-1986)	P	P	P	P	
Rooming houses	P*	P	P	X	
Tourist cabin camp	X	X	P*	X	
Tourist homes	P*	X	P*	X	
Townhouse, cluster development, shown on a preliminary plat of subdivision approved prior to September 1, 1986 (CB-54-1986)	X	X	X	P* ³²	
Townhouse, in general (CB-84-1990; CB-47-1996)	X	X	X	X ⁴⁸	
Townhouse, shown on a preliminary plat of subdivision approved pursuant to part 4A. (CB-47-1996)	X	X	X	P	
(7) RESOURCE PRODUCTION/RECOVERY:					
Agricultural Preservation Development, in accordance with Section 27-445.01	X	X	P	X	
Agricultural uses:					
(A) All general agriculture ²²	P	P	P ²³	P*	
(B) Keeping of horses or ponies	P	P	P ²³	P ³⁷	
(C) Limited to floriculture, horticulture, gardening, and private, noncommercial greenhouses	X	X	X	P	
(D) Keeping of homing or racing pigeons, provided the use was in existence:					
(i) Prior to June 30, 1987	P	P	P ⁴	P ⁵⁰	
(ii) On or after June 30, 1987 (CB-45-1987; CB-36-1991)	P	P	P ⁴	X	
Nursery and garden center:					
(A) In accordance with Section 27-445.05	X	P	P	X	
(B) All others (CB-35-1989; CB-143-1989; CB-135-1993)	P*	P*	P*	X	
Sand or gravel wet-processing, in accordance with Section 27-445.02	X	X	X	X	
Sawmill:					
(A) Only for timber grown on the premises	X	X	X	X	
(B) In connection with an agricultural operation	X	X	X	X	
Surface mining, in accordance with Section 27-445.02	X	X	X	X	
(8) TRANSPORTATION/PARKING/COMMUNICATIONS/UTILITIES:					
Airport, airpark, airfield, heliport, or helistop; private (CB-14-1992)	X	X	X	X	
Airstrip, private:					
(A) In accordance with Section 27-445.07	X	X	X	X	
(B) All others (CB-14-1992)	X	X	X	X	

USE	R-O-S (in DDOZ)	O-S (in DDOZ)	R-R (in DDOZ)	R-55 (in DDOZ)	M-U-I (in DDOZ)
Farm vehicles and farm machinery used on farm premises ⁵¹ (CB-105-1993)	P	P	P	X	
Parking lot or garage, or loading area, used in accordance with Part 11 to serve:					
(A) A permitted, PA, or PB use	P	P	P	P	
(B) A special exception use (CB-85-1988)	X	X	X	X	
Parking lot used in accordance with Part 11 to serve a use in an adjacent Commercial, Industrial, or M-X-T Zone (CB-85-1988; CB-88-1999)	X	P*	P*	P*	
Parking of mobile home except as otherwise specified	X	X	X	X	
Parking of mobile home in a public right-of-way ³¹	X	X	X	X	
Parking of vehicles owned or used by the occupants of the premises or their bona fide guests:					
(A) Boats and boat trailers ⁴⁴	P	P	P ¹⁷	P ¹⁷	
(B) Buses, 18 on the same lot with, and accessory to, the principal use, such as a school or church	P*	P	P	P	
(C) Camping trailer (unoccupied): ⁴⁴					
(i) Not more than one	P	X	P	P	
(ii) Unlimited number (CB-43-1989)	X	P	X	X	
(D) Not more than 1 commercial vehicle:					
(i) Having a maximum manufacturer's gross vehicle weight specification of up to 17,000 pounds, and which may include unlimited advertising on the side of the vehicle:					
(aa) If parked within a wholly enclosed private parking garage	P	P	P	P	
(bb) If parked in a side or rear yard ¹¹	P ⁴	P ⁴	X	X	
(ii) If parked on the premises, having a maximum manufacturer's gross vehicle weight specification of up to 8,500 pounds, no advertising (other than a firm name or similar designation not exceeding 4 inches high), and excluding vehicles exceeding 300 cubic feet of load space, stake platform trucks, dump trucks, crane or tow trucks, and vehicles with dual rear axles	P	P	P	P	
(iii) Owned and registered by an occupant of the premises, having a manufacturer's gross vehicle weight specification of greater than 17,000 pounds, parked only in the side or rear yard for not more than 72 continuous hours on a lot at least 5 acres in size, and set back 300 feet from all lot lines ¹¹	P	P	X	X	
(iv) Owned and registered by an occupant of the premises, having a manufacturer's gross vehicle weight specification of greater than 17,000 pounds, parked only in the side or rear yard for not more than 72 continuous hours, on a lot at least 2 acres in size ¹¹ (CB-53-1987; CB-35-1993)	P*	P*	X	X	

USE	R-O-S (in DDOZ)	O-S (in DDOZ)	R-R (in DDOZ)	R-55 (in DDOZ)	M-U-I (in DDOZ)
(E) Commercial vehicles not exceeding a manufacturer's gross vehicle weight specification of 8,500 pounds; containing no advertising other than a firm name or similar designation not more than 4 inches high; and excluding vehicles exceeding 300 cubic feet of load space, stake platform trucks, dump trucks, crane or tow trucks, or vehicles with dual rear wheels	X	X	X	X	
(F) Private passenger vehicles	P	P	P	P	
Public utility use or structure:					
(A) Underground pipelines and electric power and telephone lines; structures commonly known as "telephone poles"; and railroad tracks (but not a yard) and passenger stations	P	P	P	P	
(B) Antennas, monopoles, and related equipment buildings for mobile telephone transmission or other telecommunications, excluding satellite dish antenna:					
(i) In accordance with Section 27-445.04	P	P	P	P	
(ii) All others (CB-1-1986; CB-14-1987; CB-61-1988; CB-123-1994)	P*	P*	P*	P*	
(C) Unmanned, self-contained telecommunications electronic equipment enclosures not more than 350 square feet in area, not more than 12 feet in height, having parking and access easements for one (1) vehicle where necessary, with minimum on-site visits of personnel, provided the enclosure is entirely surrounded with screening material. These structures must be used exclusively for the purpose of providing advanced communications services ⁴⁰ (CB-25-1987)	P	P	P	P	
(D) Other public utility uses or structures (including major transmission and distribution lines and structures, but excluding railroad yards, round houses, car barns, and freight stations) (CB-25-1987)	P*	P*	P*	P*	
Satellite dish antenna, in accordance with Section 27-424.02:					
(A) Up to 10 feet in diameter, to serve only 1 dwelling unit	P	P	P	P	
(B) More than 10 feet in diameter, to serve only 1 dwelling unit	X	X	X	X	
(C) All others (CB-19-1985)	P	P	P	P	
Storage of any motor vehicle which is wrecked, dismantled, or not currently licensed, except where specifically allowed ¹² (CB-4-1987)	X	X	X	X	
Tower, pole, or antenna (electronic, radio, or television, transmitting or receiving), except a public utility structure or satellite dish antenna:					
(A) Commercial purposes	P*	P*	X	X	
(B) Nonprofit, noncommercial purposes (CB-18-1984; CB-39-1984; CB-94-1984; CB-133-1984; CB-33-1985; CB-123-1994)	P	P	P	P	

USE	ZONE					
	R-T (in DDOZ)	R-18 (in DDOZ)	R-18C (in DDOZ)	R-10A (in DDOZ)	R-10 (in DDOZ)	M-U-I (in DDOZ)
(1) COMMERCIAL:						
Animal Hospital, veterinary office	X	X	X	X	X	
Antique shop	X	X	X	X	X	
Bus maintenance accessory to a private school, church, or other place of worship (CB-23-1988)	P*	P*	P*	P*	X	
Buying of items within guest rooms and pursuant to Section 27-115(a)(2)	X	X	X	X	X	
Collection of recyclable materials as a temporary use, in accordance with Sections 27-260 and 27-261	P	P	P	P	P	
Commercial recreational development (CB-35-2000)	X	X	X	X	X	
Contractor's office (must include sanitary facilities), construction yard or shed, or storage building (in connection with a construction project) as a temporary use:						
(A) Subject to Sections 27-260 and 27-261	P	P	P	P	P	
(B) All others	P*	P*	P*	P*	X	
Distillery for the production of fuel alcohol	X	X	X	X	X	
Drug paraphernalia display or sales, pursuant to Section 27-115(a)(1)	X	X	X	X	X	
Farm implement sales or repair; farm supplies sales	X	X	X	X	X	
Farmers' market or flea market as a temporary use, in accordance with Sections 27-260 and 27-261 (CB-63-1998)	P	P	P	P	P	
Firewood sales as a temporary use, in accordance with Sections 27-260 and 27-261	P	P	P	P	P	
Funeral parlor, undertaking establishment	P*	P*	P*	P*	P*	
Kennel:						
(A) On a lot having a net area of 20,000 sq. ft. or less	X	X	X	X	X	
(B) On a lot having a net area between 20,000 sq. ft. and 80,000 sq. ft.	X	X	X	X	X	
(C) On a lot having a net area exceeding 80,000 sq. ft. (CB-37-1991; CB-16-1993)	X	X	X	X	X	
Landscaping contractor's business (CB-10-1996)	X	X	X	X	X	
Limited professional uses in multifamily projects	X	P*	P*	X	X	
Monument and headstone sales establishment (CB-60-1998)	X	X	X	X	X	
Offices:						

USE	R-T (in DDOZ)	R-18 (in DDOZ)	R-18C (in DDOZ)	R-10A (in DDOZ)	R-10 (in DDOZ)	M-U-I (in DDOZ)
(A) Accountants, architects, clergymen, engineers, lawyers, medical practitioners, and similar recognized and learned professions, as an accessory use in a dwelling	P ^{16,19}	P	P	P	P	
(B) Business office and model apartments in a multifamily dwelling or multifamily project and used only in connection with the sale, rental, operation, service, and maintenance of the dwelling or project (CB-36-1987)	X	P ^{16,19}	PB ¹⁰	PB ¹⁰	PB ¹⁰	
(C) General business and professional offices	X	X	X	X	P*	
(D) Insurance sales office as an accessory use in a dwelling	X	X	X	X	X	
(E) Medical practitioner's office in a one-family dwelling (except as provided in (A) above)	X	X	X	X	X	
(F) Real estate sales office as an accessory use in a dwelling	X	X	X	X	X	
(G) Real estate subdivision sales office as a temporary use:						
(i) Subject to Sections 27-260 and 27-261	P	P	P	P	P	
(ii) All others	P*	P*	P*	X	P*	
(H) Multifamily dwelling management company (must manage the project within which it is located)	X	X	X	X	X	
(I) Temporary trailer for office space accessory to an existing group residential facility, which services more than eight (8) persons, in accordance with Sections 27-260 and 27-261 (CB-35-1996)	X	X	X	X	X	
Parking lot, required, serving adjacent Commercial or Industrial Zone	P*	P*	P*	P*	X	
Photography studio and darkroom, as an accessory use solely by the resident of a one-family detached dwelling and located within such dwelling (CB-140-1986)	X	X	X	X	X	
Retail sales and consumer service establishment (CB-140-1986)	X	X	X	P	P*	
Riding stable:						
(A) On a tract consisting of less than 20,000 sq. ft.	X	X	X	X	X	
(B) On a tract consisting of between 20,000 sq. ft. and 9 contiguous acres	X	X	X	X	X	
(C) All others (CB-117-1991; CB-5-1992)	X	X	X	X	X	
Seasonal decorations display and sales as a temporary use, in accordance with Sections 27-260 and 27-261 ⁴³ (CB-23-1989)	P	P	P	P	P	
Waterfront Entertainment/Retail Complex, in accordance with Section 27-445.08 (CB-44-1997)	X	X	X	X	X	
Wayside stand as a temporary use:						
(A) Subject to Sections 27-260 and 27-261	P	P	P	P	P	
(B) All others	X	X	X	X	X	

USE	R-T (in DDOZ)	R-18 (in DDOZ)	R-18C (in DDOZ)	R-10A (in DDOZ)	R-10 (in DDOZ)	M-U-I (in DDOZ)
(2) INSTITUTIONAL/EDUCATIONAL:						
Adult day care center	X	P*	X	X	X	
Chancery, on a lot having a net area of at least 15 acres	X	X	X	X	X	
Church or similar place of worship:						
(A) Located on a lot less than 1 acre in size	X	X	X	X	X	
(B) Located in a building that was originally constructed as a dwelling, on a lot less than 1 acre in size	X	X	X	X	X	
(C) Located on a lot between 1 and 2 acres in size ⁵²	P	P	P	P	P	
(D) Located in a building that was originally constructed as a dwelling, on a lot between 1 and 2 acres in size ⁵²	X	X	X	X	X	
(E) All others (CB-23-1988; CB-23-1993; CB-76-1993)	P	P	P	PA	P	
Day care center for children:						
(A) Accessory to a publicly owned recreational facility, a school, a surplus school building, improved property (other than a school) that is under the control of the Board of Education, a church, a public building, or a community building, in accordance with Section 27-445.03 ³⁴	P	P	P	P	P	
(B) Accessory to a multifamily dwelling or project when located within a community room for the sole use of the residents or employees, in accordance with Section 27-445.03	X	P	P	P	P	
(C) Accessory to a multifamily development when located within an existing building in accordance with Section 27-445.03	X	P	P	P	P	
(D) All others (CB-23-1988; CB-44-1989; CB-24-1999)	P*	P*	P*	P*	P*	
Eleemosynary or philanthropic institution:						
(A) An adaptive reuse of a structure last occupied by a Federal postal facility on a lot or parcel not more than 25,000 square feet in area for use by an organization serving the homebound	P*	P*	P*	P*	P*	
(B) A building containing no more than 7,000 square feet of gross floor area on a lot or parcel with not more than 1.5 acres for use by an organization providing benevolent services; for a permitted use, any change in occupant or use shall require Detailed Site Plan approval by the District Council	P*	P*	P*	P*	P*	
(C) All others (CB-78-1997; CB-8-1998)	P*	P*	P*	P*	P*	
Family day care	P	P	P	P	P	
Health campus	P*	P*	P*	X	P*	
Hospital	P*	P*	P*	X	P*	
Medical/residential campus	P*	P*	P*	X	P*	

USE	R-T (in DDOZ)	R-18 (in DDOZ)	R-18C (in DDOZ)	R-10A (in DDOZ)	R-10 (in DDOZ)	M-U-I (in DDOZ)
Modular classroom as a temporary use, in accordance with Sections 27-260 and 27-261 (CB-106-1989)	P	P	P	P	P	
Nursing or care home (may include a private spa)	P*	P*	P*	X	P*	
School, private:						
(A) In accordance with Section 27-443	P	P	P	P	P	
(B) All others	P*	P*	P*	X	P*	
Small group child care center (CB-131-1993)	P	P	P	P	P	
(3) MISCELLANEOUS:						
Accessory structures and uses (when not otherwise provided for)	P	P	P	P	P	P
Adaptive reuse of a surplus public school, when not otherwise allowed	P*	P*	P*	X	P*	P*
Adaptive use of a Historic Site, when not otherwise allowed (CB-58-1987)	P*	P*	P*	P*	P*	P*
Animals, not customarily household pets (CB-117-1986; CB-55-1988)	X	X	X	X	X	X
Buildings and uses, serving public health purposes, on land owned by Prince George's County, Maryland, upon which hospitals or health centers are located, except if otherwise allowed as a Permitted (P) use ⁴¹ (CB-55-1988)	P	P	P	P	P	P
Cemetery, crematory:						
(A) Cemetery, in accordance with Section 27-445.06	X	X	X	X	X	X
(B) Cemetery, accessory to a church, convent, or monastery ⁴⁹	X	X	X	X	X	X
(C) All others (CB-86-1989; CB-11-1991)	X	X	X	X	X	X
Home occupations for residents, when not otherwise allowed ²⁰ (CB-86-1989)	P ¹⁶	X	X	X	X	X
Increase in height of accessory building, used for:						
(A) Servant, household help living quarters ³⁰	X	X	X	X	X	X
(B) Agricultural purposes on a lot having a net area of less than 5 acres	X	X	X	X	X	X
(C) Agricultural purposes on a lot having a net area of at least 5 acres	X	X	X	X	X	X
(D) Office	X	P* ⁶⁷	X	X	X	P* ⁶⁷
Signs, in accordance with Part 12, associated with uses allowed in the applicable Residential Zone (CB-85-1988)	P ⁶⁷	P ⁶⁷	P ⁶⁷	P ⁶⁷	P ⁶⁷	P ⁶⁷
Signs, outdoor advertising (Billboards) (CB-85-1988)	X	X	X	X	X	X
Temporary structures and uses not otherwise allowed	P*	P*	P*	P*	P*	P*
(4) PUBLIC/QUASI PUBLIC:						
Library	P	P	P	PA	P	
Public buildings and uses, except as otherwise provided	P	P	P	P	P	

USE	R-T (in DDOZ)	R-18 (in DDOZ)	R-18C (in DDOZ)	R-10A (in DDOZ)	R-10 (in DDOZ)	M-U-I (in DDOZ)
Sanitary landfill or rubblefill ⁴⁷ (CB-15-1990)	X	X	X	X	X	
Voluntary fire, ambulance, or rescue squad services ²⁶	P	P	P	P	P	
(5) RECREATIONAL/ENTERTAINMENT/SOCIAL/CULTURAL:						
Archery range, privately owned and commercially operated on land leased from, and owned by, a public agency	X	X	X	X	X	
Athletic field, outdoor, private nonprofit (CB-43-1994)	P*	P*	P*	P*	P*	
Boathouse (private) as an accessory use	X	X	X	X	X	
Carnival, circus, fair, or similar use, not exceeding 17 days duration and only on a parking lot as a temporary use in accordance with Sections 27-260 and 27-261	P	P	P	P	P	
Club, private	P*	P*	P*	X	P*	
Commercial recreational attraction	X	X	X	X	X	
Commercial recreational facilities (privately owned) on land leased from a public agency, except as otherwise allowed:						
(A) Leased on or after January 1, 1974	X	X	X	X	X	
(B) Leased before January 1, 1974	X	X	X	X	X	
Community building or similar nonprofit social use, not publicly owned or operated:						
(A) Only for residents and guests	P*	P	P	P	P	
(B) All others (CB-85-1988; CB-33-1989)	P*	P*	P*	X	P*	
Conference center and uses accessory thereto (such as restaurants, tennis courts, auditoriums, swimming pools, racquetball courts, riding stables, golf courses, or other recreational, physical fitness, or educational activities) privately owned and commercially operated, on a tract having a gross area of at least 500 acres, owned by a public agency, on which a public golf course is operated on a regular basis	X	X	X	X	X	
Courts (indoor or outdoor) (tennis, handball, racquetball, or volleyball), not including courts accessory to a dwelling:						
(A) Privately owned and commercially operated on land leased from, and owned by, a public agency ⁵⁶	X	X	X	X	X	
(B) All others (CB-47-1995)	X	X	X	X	X	
Golf course:						

USE	R-T (in DDOZ)	R-18 (in DDOZ)	R-18C (in DDOZ)	R-10A (in DDOZ)	R-10 (in DDOZ)	M-U-I (in DDOZ)
(A) At least 18 holes on a tract having a gross area of at least 200 acres; provided that any accessory recreational facilities shall be located at least 100 feet from the nearest property line and effectively screened from view of any adjoining land in a Residential Zone, or land proposed to be used for residential purposes on an approved Basic Plan for a Comprehensive Design Zone, approved Official Plan for an R-P-C Zone, or any approved Conceptual or Detailed Site Plan, not on publicly owned land	P*	P*	P*	X	P*	
(B) Privately owned and commercially operated on land leased from, and owned by, a public agency ⁵⁶	X	X	X	X	X	
(C) All others (CB-47-1995)	P*	P*	P*	X	P*	
Golf course, miniature (indoor or outdoor):						
(A) Privately owned and commercially operated on land leased from, and owned by, a public agency ⁵⁶	X	X	X	X	X	
(B) All others (CB-47-1995)	X	X	X	X	X	
Golf driving range:						
(A) Privately owned and commercially operated on land leased from, and owned by, a public agency ⁵⁶	X	X	X	X	X	
(B) All others (CB-47-1995)	X	X	X	X	X	
Homes Association Recreational Use, in accordance with Section 27-445	P	X	X	X	X	
Museum, art gallery, aquarium, cultural center, or similar facility (noncommercial)	P	P	P	PA	P	
Racetrack, including pari-mutuel	X	X	X	X	X	
Racetrack, pari-mutuel only	X	X	X	X	X	
Recreational campground	X	X	X	X	X	
Recreational program, before- and after-school	P	P	P	X	P	
Recreational use (nonprofit) not publicly owned or operated, when not otherwise allowed:						
(A) Only for residents and guests	P*	P	P	P	P	
(B) All others (CB-33-1989)	P*	P*	P*	X	P*	
Saunas, solariums, and health clubs, noncommercial, for the sole use of residents and their guests	X	X	X	P	P*	
Shooting range (rifle, pistol, or skeet):						
(A) On a lot having a net area of at least 20 acres, and subject to annual renewal	X	X	X	X	X	
(B) All others	X	X	X	X	X	
Skating facility:						

USE	R-T (in DDOZ)	R-18 (in DDOZ)	R-18C (in DDOZ)	R-10A (in DDOZ)	R-10 (in DDOZ)	M-U-I (in DDOZ)
(A) Privately owned and commercially operated on land leased from, and owned by, a public agency ⁵⁶	X	X	X	X	X	
(B) All others (CB-89-1994; CB-47-1995)	X	X	X	X	X	
Spa, private	P	P	P	P	P	
Spa, community	P	P	P	P	P	
Stable, private (CB-29-1985)	X	X	X	X	X	
Swimming pool (community) for sole use of residents and their guests, in accordance with Section 27-411	X	P	P	P	P	
Swimming pool (community), in accordance with Section 27-411	P	X	X	X	X	
Swimming pool (private):						
(A) Accessory to a one-family detached dwelling	P	P	P	X	P	
(B) Accessory to other dwellings	P*	P*	P*	X	X	
Swimming pool, privately owned and commercially operated on land leased from, and owned by, a public agency ⁵⁶ (CB-47-1995)	X	X	X	X	X	
(6) RESIDENTIAL/LODGING:						
Agricultural preservation development, Section 27-445.01	X	X	X	X	X	X
Apartment hotel	X	X	X	X	P*	X
Apartment housing for elderly or handicapped families in a building other than a surplus public school building (with provisions for increased density and reduced lot size in Multifamily Zones) (CB-85-1988; CB-91-1991; CB-44-1992, CB-46-1999)	P*	P*	P*	X	P*	P*
Apartment housing for elderly or handicapped families in a surplus public school building	P*	P*	P*	X	P*	P*
Boardinghouse	X	P	P	X	P	P
Congregate living facility for more than 8 elderly or physically handicapped residents (CB-90-1985)	X	P	X	X	X	P
Congregate living facility for NOT more than 8 elderly or physically handicapped residents (CB-90-1985)	X	P*	X	X	X	P*
Convent or monastery (CB-23-1993)	P	P	P	PA	P	P
Conversion of one-family detached dwelling to a building containing up to 3 dwelling units (not considered as a two-family, three-family, or multifamily dwelling): ⁵⁷						
(A) Prior to November 29, 1949, if the owner of the building resides in the building, and a valid Use and Occupancy permit was in effect on July 1, 1986	X	X	X	X	X	X

USE	R-T (in DDOZ)	R-18 (in DDOZ)	R-18C (in DDOZ)	R-10A (in DDOZ)	R-10 (in DDOZ)	M-U-I (in DDOZ)
(B) Prior to November 29, 1949, if the owner of the building does not reside in the building, or a valid Use and Occupancy permit was NOT in effect on July 1, 1986	X	X	X	X	X	X
(C) Prior to November 18, 1980, but on or after November 29, 1949	X	X	X	X	X	X
(D) On or after November 18, 1980 (CB-58-1986; CB-73-1996)	X	X	X	X	X	X
Country inn	X	X	X	X	X	X
Dwelling, farm tenant	X	X	X	X	X	X
Dwelling, multifamily:						
(A) In general	X	X	P ⁶	X	X	X
(B) Subject to applicable bedroom percentages	X	P	X	X	P	P
(C) In excess of applicable bedroom percentages	X	P* ⁶⁸	X	X	P* ⁶⁸	P ⁶⁸
(D) Restricted to one-bedroom and efficiency apartments	X	X	X	P	X	X
(E) Higher than 110 feet (CB-85-1988)	X	X	X	P*	P*	P*
Dwelling, one-family attached, for the elderly ⁵⁸ (CB-71-1996)	P	P ²	P ²	X	X	P ²
Dwelling, one-family detached, cluster development	X	X	X	X	X	X
Dwelling, one-family detached (in general)	P	P	P	X	P	P
Dwelling, one-family semidetached ¹ (CB-85-1988)	P	P ²	P ²	X	X	P ²
Dwelling, quadruple-attached (CB-83-1997)	P ²	P ^{2.5}	P ²	X	X	P ^{2.5}
Dwelling, three-family	P	P ²	P ²	X	X	P ²
Dwelling, two-family detached (CB-85-1988)	P ³	P ²	P ²	X	X	P ²
Dwelling, two-family (in general)	P ³	P ²	P ²	X	X	P ²
Dwellings, one-family attached, cluster development, shown on a preliminary plat of subdivision approved prior to September 1, 1986	X	X	X	X	X	X
Dwellings, one-family triple-attached, cluster development, shown on a preliminary plat of subdivision approved prior to September 1, 1986	X	X	X	X	X	X
Dwellings, one-family triple-attached (in general)	X	X	X	X	X	X
Flag lot development:						
(A) In accordance with preliminary plats approved prior to February 1, 1990, pursuant to Subtitle 24 and recorded within the prescribed time period	X	X	X	X	X	X
(B) In accordance with Section 24-138.01 of Subtitle 24 (CB-72-1989)	X	X	X	X	X	X
Fraternity or sorority house:						
(A) If legally existing prior to May 20, 1983, and not extended beyond the boundary lines of the lot as it legally existed (prior to May 20, 1983)	X	P	P	X	P	P
(B) All others	X	P*	P*	X	P	P*

USE	R-T (in DDOZ)	R-18 (in DDOZ)	R-18C (in DDOZ)	R-10A (in DDOZ)	R-10 (in DDOZ)	M-U-I (in DDOZ)
Group residential facility for more than 8 mentally handicapped dependent persons, or for 5 or more other dependent persons	X	P*	X	X	X	P*
Group residential facility for not more than 8 mentally handicapped dependent persons	P	P	P	X	P	P
Guest house, as an accessory use	X	X	X	X	X	X
Mobile home used as a dwelling for emergency purposes as a temporary use, in accordance with Sections 27-260 and 27-261	X	X	X	X	X	X
Mobile home used as a one-family detached dwelling	X	X	X	X	X	X
Mobile home, with use for which amusement taxes collected ²⁸	X	X	X	X	X	X
Motel	X	X	X	X	X	P
Opportunity housing dwelling units ⁵⁹ (CB-66-1991; CB-55-1996)	P	P	P	P	P	P
Planned retirement community ⁵⁹ (CB-55-1996, CB-21-1999)	P*	P*	P*	X	P*	P*
Recreational community development, in accordance with Section 27-444 ⁵⁹ (CB-16-1989; CB-55-1996)	X	X	X	X	X	X
Rental of guest rooms (by the residents):						
(A) To 1 or 2 persons (unrelated to all principal residents)	X	X	X	X	X	X
(B) To 3 persons (unrelated to all principal residents)	X	X	X	X	X	X
(C) To not more than 3 persons (unrelated to all principal residents) by a family of related individuals, 1 individual, or 2 unrelated individuals (CB-122-1986)	X	X	X	X	X	X
Rooming houses	X	P	P	X	P	P
Tourist cabin camp	X	X	X	X	X	X
Tourist homes	X	P*	P*	X	P	P*
Townhouse, cluster development, shown on a preliminary plat of subdivision approved prior to September 1, 1986 (CB-54-1986)	X	X	X	X	X	X
Townhouse, shown on a Detailed Site Plan approved prior to December 30, 1996, and in compliance with Section 3 of CB-55-1996 (CB-84-1990; CB-55-1996)	P	P ^{2.5}	P ²	X	X	P ^{2.5}
Townhouse, shown on a preliminary plat of subdivision approved pursuant to part 4A. (CB-47-1996)	P	P ^{2.5}	P ²	X	X	P ^{2.5}
Townhouse, all others (CB-55-1996)	P	P*	P*	X	X	P*

USE	R-T (in DDOZ)	R-18 (in DDOZ)	R-18C (in DDOZ)	R-10A (in DDOZ)	R-10 (in DDOZ)	M-U-I (in DDOZ)
(7) RESOURCE PRODUCTION/RECOVERY:						
Agricultural Preservation Development, in accordance with Section 27-445.01	X	X	X	X	X	
Agricultural uses:						
(A) All general agriculture ²²	X	X	X	X	X	
(B) Keeping of horses or ponies	X	X	X	X	X	
(C) Limited to floriculture, horticulture, gardening, and private, noncommercial greenhouses	P	P	P	P	P	
(D) Keeping of homing or racing pigeons, provided the use was in existence:						
(i) Prior to June 30, 1987	X	X	X	X	X	
(ii) On or after June 30, 1987	X	X	X	X	X	
(CB-45-1987; CB-36-1991)						
Nursery and garden center:						
(A) In accordance with Section 27-445.05	X	X	X	X	X	
(B) All others	X	X	X	X	X	
(CB-35-1989; CB-143-1989; CB-135-1993)						
Sand or gravel wet-processing, in accordance with Section 27-445.02	X	X	X	X	X	
Sawmill:						
(A) Only for timber grown on the premises	X	X	X	X	X	
(B) In connection with an agricultural operation	X	X	X	X	X	
Surface mining, in accordance with Section 27-445.02	X	X	X	X	X	
(8) TRANSPORTATION/PARKING/COMMUNICATIONS/UTILITIES:						
Airport, airpark, airfield, heliport, or helistop; private (CB-14-1992)	X	X	X	X	X	
Airstrip, private:						
(A) In accordance with Section 27-445.07	X	X	X	X	X	
(B) All others	X	X	X	X	X	
(CB-14-1992)						
Farm vehicles and farm machinery used on farm premises ⁵¹ (CB-105-1993)	X	X	X	X	X	
Parking lot or garage, or loading area, used in accordance with Part 11 to serve:						
(A) A permitted, PA, or PB use	P	P	P	P	P	
(B) A special exception use	X	X	X	X	X	
(CB-85-1988)						
Parking lot used in accordance with Part 11 to serve a use in an adjacent Commercial, Industrial, or M-X-T Zone (CB-85-1988)	P*	P*	P*	X	P*	
Parking of mobile home except as otherwise specified	X	X	X	X	X	
Parking of mobile home in a public right-of-way ³¹	X	X	X	X	X	

USE	R-T (in DDOZ)	R-18 (in DDOZ)	R-18C (in DDOZ)	R-10A (in DDOZ)	R-10 (in DDOZ)	M-U-I (in DDOZ)
Parking of vehicles owned or used by the occupants of the premises or their bona fide guests:						
(A) Boats and boat trailers ⁴⁴	P ¹⁷	P	P	P	P	
(B) Buses, 18 on the same lot with, and accessory to, the principal use, such as a school or church	P	P	P	P	P	
(C) Camping trailer (unoccupied): ⁴⁴						
(i) Not more than one	P	X	X	X	X	
(ii) Unlimited number (CB-43-1989)	X	P	P	P	P	
(D) Not more than 1 commercial vehicle:						
(i) Having a maximum manufacturer's gross vehicle weight specification of up to 17,000 pounds, and which may include unlimited advertising on the side of the vehicle:						
(aa) If parked within a wholly enclosed private parking garage	P	X	X	X	X	
(bb) If parked in a side or rear yard ¹¹	X	X	X	X	X	
(ii) If parked on the premises, having a maximum manufacturer's gross vehicle weight specification of up to 8,500 pounds, no advertising (other than a firm name or similar designation not exceeding 4 inches high), and excluding vehicles exceeding 300 cubic feet of load space, stake platform trucks, dump trucks, crane or tow trucks, and vehicles with dual rear axles	P	X	X	X	X	
(iii) Owned and registered by an occupant of the premises, having a manufacturer's gross vehicle weight specification of greater than 17,000 pounds, parked only in the side or rear yard for not more than 72 continuous hours on a lot at least 5 acres in size, and set back 300 feet from all lot lines ¹¹	X	X	X	X	X	
(iv) Owned and registered by an occupant of the premises, having a manufacturer's gross vehicle weight specification of greater than 17,000 pounds, parked only in the side or rear yard for not more than 72 continuous hours, on a lot at least 2 acres in size ¹¹ (CB-53-1987; CB-35-1993)	X	X	X	X	X	
(E) Commercial vehicles not exceeding a manufacturer's gross vehicle weight specification of 8,500 pounds; containing no advertising other than a firm name or similar designation not more than 4 inches high; and excluding vehicles exceeding 300 cubic feet of load space, stake platform trucks, dump trucks, crane or tow trucks, or vehicles with dual rear wheels	X	P	P	P	P	
(F) Private passenger vehicles	P	P	P	P	P	
Public utility use or structure:						
(A) Underground pipelines and electric power and telephone lines; structures commonly known as telephone poles"; and railroad tracks (but not a yard) and passenger stations	P	P	P	P	P	

USE	R-T (in DDOZ)	R-18 (in DDOZ)	R-18C (in DDOZ)	R-10A (in DDOZ)	R-10 (in DDOZ)	M-U-I (in DDOZ)
(B) Antennas, monopoles, and related equipment buildings for mobile telephone transmission or other telecommunications, excluding satellite dish antenna:						
(i) In accordance with Section 27-445.04	P	P	P	P	P	
(ii) All others (CB-1-1986; CB-14-1987; CB-61-1988; CB-123-1994)	P*	P*	P*	P*	P*	
(C) Unmanned, self-contained telecommunications electronic equipment enclosures not more than 350 square feet in area, not more than 12 feet in height, having parking and access easements for one (1) vehicle where necessary, with minimum on-site visits of personnel, provided the enclosure is entirely surrounded with screening material. These structures must be used exclusively for the purpose of providing advanced communications services ⁴⁰ (CB-25-1987)	P	P	P	P	P	
(D) Other public utility uses or structures (including major transmission and distribution lines and structures, but excluding railroad yards, round houses, car barns, and freight stations) (CB-25-1987)	P*	P*	P*	X	P*	
Satellite dish antenna, in accordance with Section 27-424.02:						
(A) Up to 10 feet in diameter, to serve only 1 dwelling unit	P	P	P	P	P	
(B) More than 10 feet in diameter, to serve only 1 dwelling unit	X	X	X	X	X	
(C) All others (CB-19-1985)	P	P	P	P	P	
Storage of any motor vehicle which is wrecked, dismantled, or not currently licensed, except where specifically allowed ¹² (CB-4-1987)	X	X	X	X	X	
Tower, pole, or antenna (electronic, radio, or television, transmitting or receiving), except a public utility structure or satellite dish antenna:						
(A) Commercial purposes	X	X	X	X	X	
(B) Nonprofit, noncommercial purposes (CB-18-1984; CB-39-1984; CB-94-1984; CB-133-1984; CB-33-1985; CB-123-1994)	P	P	P	P	P	

- 1 Provided both of an adjoining pair are erected at the same time.
- 2 Subject to all requirements applicable to the R-T Zone (except as specifically modified for the R-20 Zone).
- 3 Limited to dwelling units arranged one above the other.
- 4 On lots having a net area exceeding 20,000 square feet.
(CB-45-1987)
- 5 The townhouses may be developed without conforming to the regulations applicable to townhouses governing roadways and drives, tract widths and sizes, density, and net lot area, provided:
 - (A) A special exception for multifamily dwelling bedroom percentages increase (Section 27-382) has been granted for the subject property with a condition that the property be developed with townhouses;
 - (B) A preliminary plat of subdivision has been approved for the property as of June 1, 1975, in accordance with the net lot area and lot frontage requirements applicable to multifamily dwellings in the R-18 Zone, with a maximum density of 22 dwelling units per acre; and
 - (C) A final plat was recorded prior to June 1, 1976.
- 6 Provided a condominium plat is recorded, in accordance with the provisions of the Maryland Condominium Act, setting out each dwelling unit as a separate unit.
- 7 Provided the use is limited to a person residing in the dwelling.
- 8 Except as allowed without a special exception.
- 9 Provided the use is located in a community building (constructed as part of a multifamily project), owned by a homes association, that does not contain any dwelling units. Not more than one-third (1/3) of the gross floor area of the community building may be used for professional office space.
- 10 Provided the multifamily dwelling or project contains at least 24 dwelling units.
(CB-36-1987)
- 11 For lots having frontage on more than one (1) street (i.e., a corner lot), a commercial vehicle may only be parked in a yard that does not have street frontage.
(CB-53-1987)
- 12 This shall not apply to:
 - (A) Such storage accessory to an allowed use; or
 - (B) One (1) such vehicle which is stored in a wholly enclosed garage.
- 13 For zero lot line development, in accordance with Optional Residential Design Approach provisions of Subtitle 24.
- 14 Only for the expansion of the existing business on abutting land in the C-M, I-1, I-2, or I-4 Zones.
- 15 Restricted to one-family detached and semidetached dwellings.
- 16 Restricted to one-family detached dwellings.
- 17 Only one (1) of each.
- 18 Provided:
 - (A) The parking area shall be in addition to any required parking lot on the premises. The parking area shall be connected to a public street by means of a driveway (constructed in compliance with the minimum standards of the Department of Public Works and Transportation) with a minimum width of eleven (11) feet for each lane;
 - (B) The parking area shall be screened from any adjoining land in any Residential Zone (on land proposed to be used for residential purposes on an approved Basic Plan for a Comprehensive Design Zone, approved Official Plan for an R-P-C Zone, or any approved Conceptual or Detailed Site Plan; and
 - (C) No repairs, service, maintenance, or gasoline dispensing or storage facility shall be permitted without a special exception.
- 19 Provided:
 - (A) The use is limited to one (1) bona fide resident of the dwelling;
 - (B) Not more than two (2) nonresident, nonprofessional assistants may be employed;
 - (C) Professional consultation at a professional's dwelling with a visiting consultant, or the employment of an alternate professional in the event of the death, disability, illness, temporary absence, or vacation of the resident professional, is also allowed;
 - (D) The use shall not alter the residential character or appearance of the premises; and
 - (E) The use shall not occupy more than 50 percent of the gross floor area of the dwelling.
- 20 Home occupations consisting of general clerical work or professional offices require a use and occupancy permit.
(CB-31-1985)

- 21 Not applicable to multifamily dwellings.
- 22 Slaughterhouses, fertilizer works, bone yards, plants for the reduction of animal matter, and any uses which are noxious or offensive because of odor, dust, smoke, gas, or noise, are prohibited; may include a stable in conjunction with the agricultural use.
- 23 On lots having a net area of 20,000 square feet or less, keeping cattle, horses, poultry, or other animals or birds (other than customary household pets) shall only be permitted upon approval of a special exception.
- 24 As a temporary use subject to annual renewal and located at least 500 feet from the boundary line of any other land in a Residential Zone, or land proposed to be used for residential purposes in a Comprehensive Design, Mixed Use, or Planned Community Zone.
- 25 Limited to 400 square feet.
- 26 Provided the site is either:
- (A) In the proximity of an area designated as a fire or rescue station on an approved Functional Master Plan of Fire and Rescue Stations;
 - (B) In a location that the Fire Chief has indicated (in writing) is appropriate; or
 - (C) Occupied by a station that was in use as a station on June 30, 1982.
- 27 The field shall be located on a lot having a net area of at least 10 acres, which is owned and operated by an eleemosynary or philanthropic institution. Any accessory building shall not exceed 1,000 square feet of gross floor area, and shall only be used for maintenance and storage. Otherwise, a special exception is required.
- 28 Provided:
- (A) The mobile home is located on a lot having a net area of at least five acres;
 - (B) The use of the mobile home is in connection with another use on the property for which the County levies an amusement tax;
 - (C) The occupants of the mobile home are employed by, or reasonably connected with, the other use; and
 - (D) The mobile home shall not be located on the property for more than 120 cumulative days per calendar year, except mobile homes used in connection with pari-mutuel racetracks where the use shall not exceed 218 cumulative days per calendar year.
- 29 Limited to two (2) vehicles (total, all types) for a lot used for one-family semidetached dwelling, and four (4) vehicles (total, all types) for a two-family detached dwelling.
- 30 Only in connection with one-family detached dwellings.
- 31 Except in an emergency. In this case, the parking shall be subject to the traffic and parking regulations applicable to the right-of-way.
- 32 In a cluster development for which the preliminary plat of subdivision was approved prior to September 1, 1986, showing such one-family attached dwellings. Up to 20 percent in the R-80 Zone, and 25 percent in the R-55 Zone, of the total number of dwelling units in the cluster development may be one-family attached dwellings. The remainder shall be one-family detached dwellings.
(CB-54-1986)
- 33 Only for expansion of an existing sanitary landfill or rubble fill on abutting land for which an approved special exception has not expired.
- 34 Minimum lot size of two (2) acres required. A church must provide its tax-exempt identification number when applying for a Detailed Site Plan or a building or use and occupancy permit for an accessory day care center for children.
(CB-23-1988; CB-44-1989)
- 35 In conjunction with an agricultural use.
- 36 Not allowed in an Agricultural Preservation Development, unless it existed prior to the approval of the site plan.
- 37 Permitted only on lots having a gross lot area of one acre or more, otherwise a special exception is required.
(CB-29-1985)
- 38 Provided the use either:
- (A) Is located at or below the ground floor level of a multifamily dwelling and does not exceed 2,000 square feet; or
 - (B) Is located in a community building (constructed as part of a multifamily project) owned by a homeowners' association and not containing dwelling units, and does not occupy more than one-half of the gross floor area of the community building.
(CB-81-1985)
- 39 The use shall be related to, dependent on, secondary to, and located on the same record lot as, the multifamily dwelling or project.
(CB-36-1987)
- 40 This does not provide for accessory antennas or overhead distribution lines.
(CB-25-1987)

- 41 Provided the health center is located on a minimum of 25 acres.
(CB-55-1988)
- 42 Either:
(A) In conjunction with an existing golf course or equestrian center; or
(B) The golf course or equestrian center shall be constructed within five years of approval of the Detailed Site Plan.
(CB-16-1989)
- 43 Minimum lot size of 30,000 square feet required, except for bona fide nonprofit groups or organizations.
(CB-23-1989)
- 44 Parking shall be provided as follows:
(A) The vehicle shall be located at least eight feet from a street line; and
(B) If parked in a yard abutting a street, it shall be parked on a dust-free surfaced area.
(CB-43-1989)
- 45 The sale of gazebos and sheds is permitted for a special exception approved in 1984 as incidental to its operation if such sale and display is in accordance with Section 27-385 and provided no more than two gazebos and two sheds are visible from any public street.
(CB-143-1989)
- 46 If the property is located within the Chesapeake Bay Critical Area, was zoned R-80 prior to December 18, 1989, and is not the subject of a record plat.
(CB-72-1989)
- 47 A sanitary landfill or rubblefill may include a rock crusher only if it is approved as part of the special exception.
(CB-15-1990)
- 48 Townhouses which were permitted when developed pursuant to former Part 4A of this Subtitle prior to January 21, 1997, are permitted. No more than 20 percent of the total number of dwelling units in the development may be townhouses.
(CB-84-1990; CB-47-1996)
- 49 Provided both uses were existing as of January 1, 1991.
(CB-11-1991)
- 50 On lots having a total area exceeding 12,000 square feet.
(CB-36-1991)
- 51 Includes semitrailers for an agricultural use located on a minimum of ten (10) acres.
(CB-105-1993)
- 52 A church or similar place of worship that is located on a lot between one (1) and two (2) acres in size shall require a Detailed Site Plan in accordance with Part 3, Division 9, of this Subtitle. In addition to the requirements of Section 27-285(b), the following requirements shall be met:
(A) The minimum setback for all buildings shall be 25 feet from each lot line;
(B) When possible, there should be no parking or loading spaces located in the front yard; and
(C) The maximum allowable lot coverage for the zone in which the use is proposed shall not be increased.
(CB-76-1993)
- 53 Provided the net lot area is at least five (5) acres.
(CB-76-1993)
- 54 Any property rezoned to the R-E Zone by a Sectional Map Amendment prior to January 1, 1994, on which a previous special exception was approved for a nursery and garden center may continue to operate as a permitted special exception use, notwithstanding the provisions of Section 27-320 of this Subtitle.
(CB-135-1993)
- 55 Provided the field is located on a lot having a net area of at least 40 acres, and any field constructed after August 1, 1996, is set back 100 feet from all property lines. Otherwise, a special exception is required.
(CB-43-1994; CB-33-1996)
- 56 Subject to Detailed Site Plan approval in accordance with Part 3, Division 9 of this Subtitle, unless the use is located in a Regional Park owned by the M-NCPPC.
(CB-47-1995)

- 57 Conversion shall not occur until:
 (A) The building is structurally modified to include the additional dwelling units; and
 (B) The additional dwelling units are occupied.
 (CB-73-1996)
- 58 For the purposes of this section, a dwelling for the elderly shall be housing which is operated in accordance with State and Federal Fair Housing laws.
 (CB-71-1996)
- 59 Townhouses shall comply with the design guidelines set forth in Section 27-274(a)(11) and the regulations for development set forth in Section 27-433(d).
 (CB-55-1996)
- 60 Section 3 of CB-55-1996 reads as follows: "BE IT FURTHER ENACTED that the provisions of this Ordinance shall not apply to projects for which a Detailed Site Plan has been filed and accepted prior to November 1, 1996, provided the design guidelines and regulations not resulting in a requirement of resubdivision are applicable, and provided building permits for ten percent of the dwelling units included in the Detailed Site Plan are issued within one year of the effective date of this legislation (December 30, 1996), and extensions of time for the permits do not exceed six months, and that the dwelling units are constructed pursuant to the permits.
- 61 Provided the use is located on a lot or parcel with not more than one-half acre which is adjoining and contiguous to an existing cemetery.
 (CB-60-1998)
- 62 Permitted use without requirement for special exception provided the use is on a parcel of land in the R-H Zone, the gross tract area of which is a maximum of 20 acres, which is adjoining R-R zoned land developed with an existing Medical Residential Campus. The entire tract of land in the R-H Zone shall require Detailed Site Plan approval in accordance with Part 3, Division 9, of this Subtitle. Regulations restricting the height of structures, lot size and coverage, frontage, setbacks, density, and other requirements of the zone shall be consistent with existing development in the adjacent Medical Residential Campus. The dimensions and percentages shown on the approved site plan shall constitute the regulations for development.
 (CB-21-1999)
- 63 Provided:
 (A) The use is located on a lot or parcel not less than 15 or more than 20 acres in size and has frontage on a public street having a proposed right-of-way width of at least 120 feet;
 (B) The lot or parcel abuts property in the C-O Zone; and
 (C) The property is located in a Revitalization Tax Credit Area.
 (CB-46-1999)
- 64 Use of permitted mobile homes is restricted to employees at a riding stable on the special exception property. No more than two mobile homes may be located on such a property, and each must be on its own R-E lot as required by Section 27-118.01(c). A building permit shall be issued by the Department of Environmental Resources for each mobile home. Any mobile home unoccupied for more than 60 days must be removed from the property.
 (CB-79-1999)
- 65 Permitted use without requirement for special exception provided the land on which the lot exists is in the R-55 Zone, immediately adjoins land in the C-S-C Zone, is a part of the same parcel as the land in the C-S-C Zone, and is located within the municipal limits of the City of New Carrollton.
 (CB-88-1999)
- 66 The use is permitted on R-R zoned property leased from a public agency before January 1, 1974. Parking and loading facilities shall be provided in accordance with Part 11 (parking and loading requirements). Landscaping, buffering, and screening shall be provided in accordance with the *Landscape Manual*. Development regulations for building setbacks shall be provided in accordance with Part 6 (Commercial Zone regulations).
 The following uses are not permitted: car wash, animal hospital, training, kennel, grooming, blacksmith, carpet or rug shampooing, electric or gas appliance repair, farm implement sales and repair, upholstery or furniture repair, locksmith, laboratories, lawn mower repair, machine shop, massage establishment, methadone treatment center, model studio, photo processing plant, studio or darkroom, pizza delivery, print shop, newspaper publishing, sauna or steam bath, septic tank sales, service, sewage dump (pump out) services, shoe repair, taxidermy, welding shop, bait shop, bottled gas, feed sales, wayside stand, and any use prohibited in the lease with the public agency, as modified or amended.
 (CB-35-2000)
- 67 Permitted when developed in accordance with the intent of the College Park US 1 Corridor Sector Plan and associated development standards.
- 68 Bedroom percentages for student housing may exceed applicable bedroom percentages according to the Zoning Ordinance.



Appendices



Appendix A

RESIDENTIAL ZONES¹

R-O-S: Reserved Open Space - Provides for permanent maintenance of certain areas of land in an undeveloped state, with the consent of the property owners; encourages preservation of large areas of trees and open space; designed to protect scenic and environmentally sensitive areas and ensure retention of land for nonintensive active or passive recreational uses; provides for very low density residential development and a limited range of public, recreational, and agricultural uses.

Minimum lot size - 20 acres*

Maximum dwelling units per net acre - 0.05

* Except for public recreational uses, for which no minimum area is required.

O-S: Open Space - Provides for areas of low-intensity residential (5 acre) development; promotes the economic use and conservation of land for agriculture, natural resource use, large-lot residential estates, nonintensive recreational use.

Standard lot size - 5 acres

Maximum dwelling units per net acre - 0.20

R-A: Residential-Agricultural - Provides for large-lot (2 acre) residential uses while encouraging the retention of agriculture as a primary land use.

Standard lot size - 2 acres

Maximum dwelling units per net acre - 0.50

¹ Definitions:

Minimum or Standard lot size: The current minimum net contiguous land area required for a lot.

Average dwelling units per acre: The number of dwelling units which may be built on a tract--including the typical mix of streets, public facility sites and areas within the 100-year floodplain--expressed as a per-acre average.

Maximum dwelling units per net acre: The number of dwelling units which may be built on the total tract--excluding streets and public facility sites, and generally excluding land within the 100-year floodplain--expressed as a per-acre average.

R-E: Residential-Estate - Permits large-lot estate subdivisions containing lots approximately one acre or larger.

Standard lot size - 40,000 sq. ft.

Maximum dwelling units per net acre - 1.08

Estimated average dwelling units
per acre - 0.85

R-R: Rural Residential - Permits approximately one-half-acre residential lots; subdivision lot sizes depend on date of recordation; allows a number of nonresidential special exception uses.

Standard lot size - 20,000 sq. ft.
- 15,000 sq. ft. if recorded prior to February 1, 1970
10,000 sq. ft. if recorded prior to July 1, 1967

Maximum dwelling units per net acre - 2.17

Estimated average dwelling units
per acre - 1.85

R-80: One-Family Detached Residential - Provides for variation in the size, shape, and width of subdivision lots to better utilize the natural terrain and to facilitate planning of single-family developments with lots and dwellings of various sizes and styles.

Standard lot size - 9,500 sq. ft.

Maximum dwelling units per net acre - 4.5

Estimated average dwelling units
per acre - 3.4

R-55: One-Family Detached Residential - Permits small-lot residential subdivisions; promotes high density, single-family detached dwellings.

Standard lot sizes - 6,500 sq. ft.

Maximum dwelling units per net acre - 6.70

Estimated average dwelling units
per acre - 4.2

R-35: One-Family Semidetached, and Two-Family Detached, Residential - Provides generally for single-family attached development; allows two-family detached; Detailed Site Plan approval required for lots served by private rights-of-way.

Standard lot sizes - 3,500 sq. ft. for one-family, semi-detached
- 7,000 sq. ft. for two-family, detached

Maximum dwelling units per net acre - 12.44

Estimated average dwelling units
per acre - 8.5

R-T: Townhouse - Permits one-family detached and attached, two-family, and three-family dwellings; promotes the maximum amount of freedom in the design of attached dwellings and their grouping and layout; Detailed Site Plan approval required for attached dwellings.

Standard lot size per attached dwelling - 1,800 sq. ft.

Maximum dwelling units per net acre - Three-family dwellings - 9
- Two-family dwellings - 8
- Other attached dwellings - 6

Minimum area for development - 2 acres

R-20: One-Family Triple-Attached Residential - Permits single-family detached, semidetached and triple-attached and townhouse development. Detailed Site Plan approval required for townhouses.

Standard lot sizes - 3,200 sq. ft. for end lots
- 2,000 sq. ft. for interior townhouse lots

Maximum triple-attached
dwellings per net acre - 16.33

Maximum townhouses per net acre - 6.0 (same as R-T)

Estimated average triple-attached
dwelling units per net acre - 11

R-30: Multifamily Low Density Residential - Provides for low density garden apartments; single-family detached; single-family attached, two-family and three-family dwellings in accordance with R-T Zone provisions; Detailed Site Plan approval required for multifamily and attached dwellings.

Standard lot size - Garden apartments - 14,000 sq. ft.
- Two-family dwellings - 1,500 sq. ft.
- Other attached dwellings - 1,800 sq. ft.

Maximum dwelling units per net acre - Garden apartments - 10
- Three-family dwellings - 9
- Two-family dwellings - 8
- Other attached dwellings - 6

R-30C: Multifamily Low Density Residential-Condominium - Same as R-30 above except ownership must be condominium, or development in accordance with the R-T Zone; Detailed Site Plan approval required for multifamily and attached dwellings.

Standard lot size - Garden apartments - 14,000 sq. ft.
- Two-family dwellings - 1,500 sq. ft.
- Other attached dwellings - 1,800 sq. ft.

Maximum dwelling units per net acre - Garden apartments - 12
- Three-family dwellings - 9
- Two-family dwellings - 8
- Other attached dwellings - 6

R-18: Multifamily Medium Density Residential - Provides for multiple family (apartment) development of moderate density; single-family detached; single-family attached, two-family and three-family dwellings in accordance with R-T Zone provisions; Detailed Site Plan approval required for multifamily and attached dwellings.

Standard lot size - Apartments - 16,000 sq. ft.
- Two-family dwellings - 1,500 sq. ft.
- Other attached dwellings - 1,800 sq. ft.

Maximum dwelling units per net acre - Garden apartments and three-family dwellings - 12
- Mid-rise apartments (4 or more stories with elevator) - 20
- Three-family dwellings - 9
- Two-family dwellings - 8
- Other attached dwellings - 6

R-18C: Multifamily Medium Density Residential-Condominium - Same as above except ownership must be condominium, or development in accordance with the R-T Zone; Detailed Site Plan approval required for multifamily and attached dwellings.

Standard lot size - Apartments - 1 acre
- Two-family dwellings - 1,500 sq. ft.
- Other attached dwellings - 1,800 sq. ft.

Maximum dwelling units per net acre - Garden apartments - 14
- Mid rise apartments (4 or more stories with elevator) - 20
- Three-family dwellings - 9
- Two-family dwellings - 8
- Other attached dwellings - 6

R-H: Multifamily High-Rise Residential - Provides for suitable sites for high density, vertical residential development; also permits single-family detached dwellings; Detailed Site Plan approval required for multifamily dwellings.

Minimum lot size - 5 acres

Maximum dwelling units per net acre - 48.4

R-10: Multifamily High Density Residential - Provides for suitable sites for high density residential in proximity to commercial and cultural centers; also permits single-family detached dwellings. Detailed Site Plan approval required for buildings 110 feet in height or less; special exception required for buildings over 110 feet in height.

Minimum lot size - 20,000 sq. ft.

Maximum dwelling units per net acre - 48

R-10A: Multifamily, High Density Residential-Efficiency - Provides for a multifamily zone designed for the elderly, singles, and small family groups. Detailed Site Plan approval required for buildings 110 feet in height or less; special exception required for buildings over 110 feet in height.

Minimum lot size - 2 acres

Maximum dwelling units per net acre - 48 plus one for each 1,000 sq. ft. of indoor common area for social, recreational, or educational purposes.

MIXED USE/PLANNED COMMUNITY ZONES

M-X-T: Mixed Use - Transportation Oriented - Provides for a variety of residential, commercial, and employment uses; mandates at least two out of the following three use categories: (1) Retail businesses; (2) Office/ Research/Industrial; (3) Dwellings, hotel/motel; encourages a 24-hour functional environment; must be located near a major intersection or a major transit stop or station and will provide adequate transportation facilities for the anticipated traffic or at a location for which the applicable Master Plan recommends mixed uses similar to those permitted in the M-X-T Zone.

Lot size and dwelling types - No Restrictions

Maximum floor area ratio - 0.4 without optional method;
- 8.0 with optional method (provision of amenities)

M-X-C: Mixed Use Community - Provides for a comprehensively planned community with a balanced mix of residential, commercial, light manufacturing, recreational and public uses; includes a multistep review process to assure compatibility of proposed land uses with existing and proposed surrounding land uses, public facilities and public services; mandates that each development include residential uses, community use areas, neighborhood centers and an integrated public street system with a variety of street standards.

Minimum tract size - 750 gross acres

Lot size and dwelling types - No Restrictions

Maximum dwelling units per gross acre - 2

Maximum floor area ratio for commercial uses - 0.4

M-U-TC: Mixed-Use Town Center - Provides for a mix of commercial and limited residential uses which establish a safe, vibrant, 24-hour environment; designed to promote appropriate redevelopment of, and the preservation and adaptive reuse of selected buildings in, older commercial areas; establishes a flexible regulatory framework, based on community input, to encourage compatible development and redevelopment; mandates approval of a Development Plan at the time of zoning approval, that includes minimum and maximum Development Standards and Guidelines, in both written and graphic form, to guide and promote local revitalization efforts; provides for legally existing buildings to be expanded or altered, and existing uses for which valid permits have been issued to be considered permitted uses, and eliminating nonconforming building and use regulations for same.

M-U-I: Mixed-Use Infill - Promotes Smart Growth principles by encouraging the efficient use of land, public facilities and services in areas that are substantially developed. These regulations are intended to create community environments enhanced by a mix of residential, commercial, recreational, open space, employment and institutional uses in accordance with approved plans. The infill zone may only be approved for property located in a Transit District Overlay Zone or a Development District Overlay Zone.

R-P-C: Planned Community - Provides for a combination of uses permitted in all zones, to promote a large-scale community development with a full range of dwellings providing living space for a minimum of 500 families; encourages recreational, commercial, institutional, and employment facilities within the planned community; requires conformance with an Official Plan identifying zoning subcategories, that has been adopted by the Planning Board following approval of a Final Plan by the District Council at the time of rezoning, and for certain R-P-C Zones, approval of a Detailed Site Plan prior to development.

Lot size and dwelling types - Varied

Maximum dwelling units
per gross acre - 8

R-M-H: Planned Mobile Home Community - Provides for suitable sites for planned mobile home communities, including residences and related recreational, commercial, and service facilities, subject to Detailed Site Plan approval.

Minimum lot size - 4,000 sq. ft.

Maximum mobile homes per acre - 7

COMPREHENSIVE DESIGN ZONES

(These zones require three-phase development plan review, the first of which is Basic Plan approval at the time of rezoning that establishes general land use types, land use relationships, and minimum land use quantities. In zones providing for density and intensity ranges, increases in base density and intensity within the limits prescribed are allowed in return for public benefit features provided by the developer.)

R-L: Residential Low Development - Provides for low-density residential development in areas recommended by a Master Plan for alternative low-density development techniques. The zone allows a mixture of residential types and lot sizes generally corresponding to single-family development; provides for limited commercial uses necessary to serve the dominant residential uses.

- | | |
|--------------------|---|
| Minimum tract size | - Generally 100 adjoining gross acres |
| Low .5 | - Base density (dwelling units per gross acre) - .5
- Maximum density - .9
- Maximum mixed retirement development density - 8 du/gross acre |
| Low 1.0 | - Base Density (dwelling units per gross acre) - 1.0
- Maximum density - 1.5
- Maximum mixed retirement development density - 8 du/gross acre |

R-S: Residential Suburban Development - A mixture of residential types within the suburban density range generally corresponding to low-density single-family development; provides for limited commercial uses necessary to serve the dominant residential uses.

- | | |
|--------------------|---|
| Minimum tract size | - Generally 25 adjoining gross acres |
| Suburban 1.6 | - Base density (dwelling units per gross acre) - 1.6
- Maximum density - 2.6
- Maximum mixed retirement development density - 8 du/gross acre |
| Suburban 2.7 | - Base density (dwelling units per gross acre) - 2.7
- Maximum density - 3.5
- Maximum mixed retirement development density - 8 du/gross acre |

R-M: Residential Medium Development - A mixture of residential types with a medium-density range; provides for limited commercial uses necessary to serve the dominant residential uses.

- Minimum tract size - Generally 10 adjoining gross acres
- Medium 3.6
 - Base density (dwelling units per gross acre) - 3.6
 - Maximum density - 5.7
 - Maximum mixed retirement development density - 8 du/gross acre
- Medium 5.8
 - Base density (dwelling units per gross acre) - 5.8
 - Maximum density - 7.9
 - Maximum mixed retirement development density - 8 du/gross acre

R-U: Residential Urban Development - A mixture of residential types generally associated with an urban environment; provides for limited commercial uses necessary to serve the dominant residential uses.

- Minimum tract size - Generally 5 adjoining gross acres
- Urban 8.0
 - Base density (dwelling units per gross acre) - 8.0
 - Maximum density - 11.9
 - Maximum mixed retirement development density - 8 du/gross acre
- Urban 12.0
 - Base density (dwelling units per gross acre) - 12.0
 - Maximum density - 16.9
 - Maximum mixed retirement development density - 8 du/gross acre

L-A-C: Local Activity Center - A mixture of commercial retail and service uses along with complementary residential densities within a hierarchy of centers servicing three distinct service areas: neighborhood, village, and community.

	<u>Neighborhood</u>	<u>Village</u>	<u>Community</u>
Minimum tract size	4 adjoining gross ac.	10 adjoining gross ac.	20 adjoining gross ac.
Base resid. density	8 du/gross resid. ac.	10 du/gross resid. ac.	10 du/gross resid. ac.
Max. resid. density	12.1 du/gross resid. ac.	15 du/gross resid. ac.	20 du/gross resid. ac.
Base comm. intensity	0.16 FAR	0.2 FAR	0.2 FAR
Max. comm. intensity	0.31 FAR	0.64 FAR	0.68 FAR
Max. mixed retirement development density	8 du/gross ac.	8 du/gross ac.	8 du/gross ac.

M-A-C: Major Activity Center - A mixture of uses which serve a regional residential market or provide concentrated employment, arranged to allow easy pedestrian access between uses; two types of functional centers are described: Major Metro and New Town or Corridor City.

Minimum tract size - Generally 40 adjoining gross acres

	<u>Metro Center</u>	<u>New Town or City Corridor Center</u>
Base residential density	48 du/gross resid. ac.	10 du/gross resid. ac.
Max. residential density	125 du/gross resid. ac.	47.9 du/gross resid. ac.
Base commercial intensity	1.0 FAR/gross commercial ac.	0.2 FAR/gross commercial ac.
Max. commercial intensity	2.7 FAR/gross commercial ac.	0.88 FAR/gross commercial ac.
Min. residential floor area	20% of total at time of full development	20% of total at time of full development
Max. mixed retirement development density	8 du/gross ac.	8 du/gross ac.

E-I-A: Employment and Institutional Area - A concentration of nonretail employment and institutional uses and services such as medical, manufacturing, office, religious, educational, recreational, and governmental.

Minimum tract size - Generally 5 adjoining gross acres

Minimum open space improved by landscaping - 20% of net lot area

V-L: Village-Low - Provides for a variety of residential, commercial, recreational, and employment uses within a traditional village setting surrounded by open space; mandates the following land use area categories: (1) Village Proper; (2) Village Fringe; (3) Residential Areas; (4) Village Buffer; and (5) Recreational Areas. Land use areas are arranged to allow a sense of community with linkage via a pedestrian network to a core which contains commercial, civic, community, and residential uses; also mandates a mixture of residential types and lot sizes, including affordable housing units; includes detailed design standards and building materials requirements. This Zone may be utilized in areas recommended for permanent low density by a Master Plan.

Minimum tract size - 150 contiguous gross acres

Maximum density - 1.3 dwelling units per gross acre

V-M: Village-Medium - Provides for a variety of residential, commercial, recreational, and employment uses within a traditional village setting surrounded by open space; mandates the following land use area categories: (1) Village Proper; (2) Village Fringe; (3) Residential Areas; (4) Village Buffer; and (5) Recreational Areas. Land use areas are arranged to allow a sense of community with linkage via a pedestrian network to a core which contains commercial, civic, community, and residential uses; also mandates a mixture of residential types and lot sizes, including affordable housing units; includes detailed design standards and building materials requirements. This Zone may be utilized in areas recommended for permanent low density by a Master Plan.

Minimum tract size - 300 contiguous gross acres

Maximum density - 2.0 dwelling units per gross acre

COMMERCIAL ZONES

C-O: Commercial Office - Uses of a predominantly nonretail commercial nature, such as business, professional and medical offices, or related administrative services.

C-A: Ancillary Commercial - Certain small retail commercial uses, physician and dental offices, and similar professional offices that are strictly related to and supply necessities in frequent demand and daily needs of an area with a minimum of consumer travel; maximum size of zone: 3 net acres.

C-1: Local Commercial, Existing - All of the uses permitted in the C-S-C Zone.

C-2: General Commercial, Existing - All of the uses permitted in the C-S-C Zone, with additions and modifications.

C-C: Community Commercial, Existing - All of the uses permitted in the C-S-C Zone.

C-G: General Commercial, Existing - All of the uses permitted in the C-S-C Zone.

C-S-C: Commercial Shopping Center - Retail and service commercial activities generally located within shopping center facilities; size will vary according to trade area.

C-H: Highway Commercial, Existing - All of the uses permitted in the C-M Zone.

C-M: Commercial Miscellaneous - Varied commercial uses, including office and highway-oriented uses, which may be disruptive to the compactness and homogeneity of retail shopping centers.

C-W: Commercial Waterfront - Marine activities related to tourism, vacationing, boating and sports, water-oriented recreation, together with limited employment areas which cater to marine activities along a waterfront.

C-R-C: Commercial Regional Center - Provides locations for major regional shopping malls and related uses that are consistent with the concept of an upscale mall. Minimum area for development - one hundred (100) gross continuous acres; maximum FAR - .75; maximum building height - 75 ft.; maximum building coverage, excluding parking - 50%; Detailed Site Plan approval required.

INDUSTRIAL ZONES

- I-1:** Light Industrial - Light intensity manufacturing, warehousing, and distribution uses; 10% green area required.
- I-2:** Heavy Industrial - Highly intensive industrial and manufacturing uses; 10% green area required.
- I-3:** Planned Industrial/Employment Park - Uses that will minimize detrimental effects on residential and other adjacent areas; a mixture of industrial, research, and office uses with compatible institutional, recreational, and service uses in a manner that will retain the dominant industrial/employment character of the zone; standard minimum tract size of 25 adjoining gross acres; standard minimum lot size of two acres; Conceptual and Detailed Site Plan approval required; 25% green area required; outdoor uses restricted; warehousing and wholesaling uses limited.
- I-4:** Limited Intensity Industrial - Limited intensity (0.3 FAR) commercial, manufacturing, warehousing, and distribution uses; development standards extended to assure limited intensity industrial and commercial development, and compatibility with surrounding zoning and uses; 25% green area required.
- U-L-I:** Urban Light Industrial - Designed to attract and retain a variety of small-scale light industrial uses in older, mostly developed industrial areas located close to established residential communities; establishes a flexible regulatory process with appropriate standards to promote reinvestment in, and redevelopment of, older urban industrial areas as employment centers, in a manner compatible with adjacent residential areas.

OVERLAY ZONES²

- T-D-O:** Transit District Overlay - Intended to ensure that development in a designated district meets the goals established in a Transit District Development Plan. Transit Districts may be designated in the vicinity of Metro stations to maximize transit ridership, serve the economic and social goals of the area, and take advantage of the unique development opportunities which mass transit provides.
- D-D-O:** Development District Overlay - Intended to ensure that development in a designated district meets the goals established in a Master Plan, Master Plan Amendment or Sector Plan. Development Districts may be designated for town centers, Metro areas, commercial corridors, employment centers, revitalization areas, historic areas and other special areas as identified in approved plans.

²These overlay zones are superimposed over other zones, and they may modify provisions of the underlying zones concerning uses allowed and standards for development. In addition, new development is generally subject to approval of a Detailed Site Plan by the Planning Board.

CHESAPEAKE BAY CRITICAL AREA OVERLAY ZONES³

- I-D-O:** Intense Development Overlay - To conserve and enhance fish, wildlife, and plant habitats and improve the quality of runoff that enters the Chesapeake Bay, while accommodating existing residential, commercial, or industrial land uses. To promote new residential, commercial and industrial land uses with development intensity limits. Maximum residential density is the same as the underlying zone.
- L-D-O:** Limited Development Overlay - To maintain and/or improve the quality of runoff entering the tributaries of the Chesapeake Bay and to maintain existing areas of natural habitat, while accommodating additional low-or moderate-intensity development. Maximum residential density is the same as the underlying zone, up to 4.0 du/net acre maximum.
- R-C-O:** Resource Conservation Overlay - To provide adequate breeding, feeding and wintering habitats for wildlife, to protect the land and water resources base necessary to support resource-oriented land uses, and to conserve existing woodland and forests for water quality benefits along the tributaries of the Chesapeake Bay. Maximum residential density - .05 du/gross acre.

REVITALIZATION OVERLAY DISTRICTS⁴

- R-O-D:** Revitalization Overlay District - Intended to ensure the orderly development or redevelopment of land within a designated district. Revitalization Districts provide a mechanism for the county to delegate full authority to local municipalities to approve departures from parking, landscaping and sign standards. In addition, limited authority is also delegated for the approval of variances from building setbacks, lot coverage, yards and other dimensional requirements of existing zoning.

ARCHITECTURAL OVERLAY DISTRICTS⁵

- A-C-O:** Architectural Conservation Overlay - Intended to ensure that development and redevelopment efforts preserve and protect the architectural or design character of neighborhoods in accordance with an approved Architectural Conservation Plan. Conservation Districts may be designated in areas where the majority of properties have been developed and they exhibit distinct, unifying elements, characteristics, design or other physical features.

³These overlay zones are superimposed over other zones, and they may modify provisions of the underlying zones concerning uses allowed and standards for development. In addition, new development is generally subject to approval of a Conservation Plan and Conservation Agreement by the Planning Board.

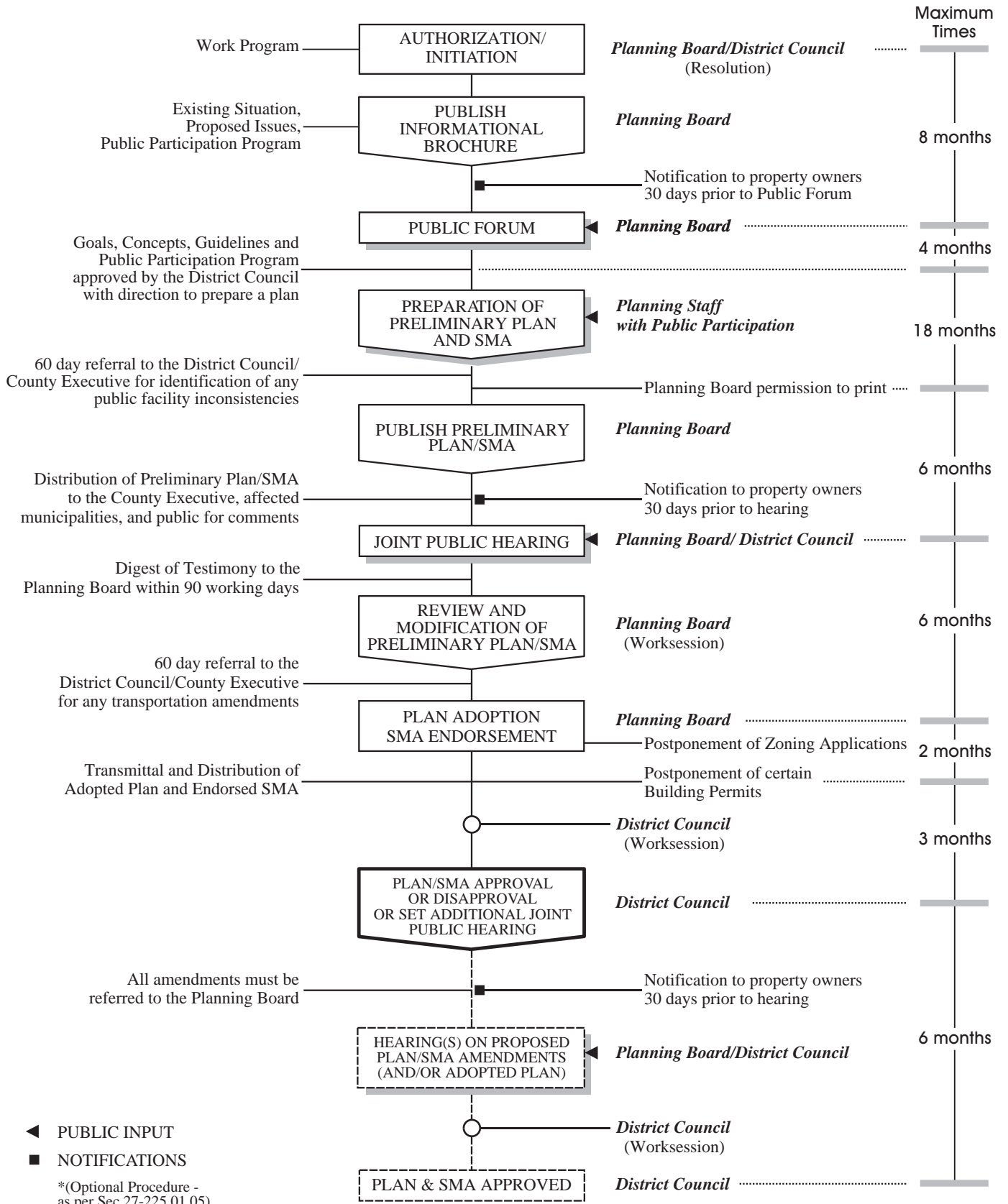
⁴These overlay districts are superimposed over other zones. However, they do not modify provisions of the underlying zones concerning uses allowed and standards for development.

⁵These overlay zones are superimposed over other zones, and they may modify provisions of the underlying zones concerning design regulations. However, they do not modify provisions of the underlying zones concerning allowed uses. In addition, a Detailed Site Plan for Architectural Conservation shall be approved by the Planning Board prior to the issuance of a building or grading permit.

Appendix B

PROCEDURAL SEQUENCE CHART

For the Concurrent Preparation of
Comprehensive Master Plans, Sector Plans and Sectional Map Amendments*



Appendix C

COUNTY COUNCIL OF PRINCE GEORGE'S COUNTY, MARYLAND
SITTING AS THE DISTRICT COUNCIL
2002 Legislative Session

Resolution No. CR-18-2002
Proposed by The Chairman (by request – Planning Board)
Introduced by Council Members Shapiro, Hendershot and Scott
Co-Sponsors _____
Date of Introduction April 30, 2002

RESOLUTION

1 A RESOLUTION concerning

2 The College Park US 1 Corridor

3 Sector Plan and Sectional Map Amendment

4 For the purpose of approving with amendments, the Sector Plan and Sectional Map Amendment
5 (SMA) for the College Park US 1 Corridor, thereby defining long-range land use and
6 development policies, detailed zoning changes, design standards, and a Development District
7 Overlay Zone for that portion of Prince George's County generally bounded by the Capital
8 Beltway on the north, commercial and some residential properties along the east and west sides
9 of Baltimore Avenue (US 1), the East Campus area of The University of Maryland along the
10 Paint Branch Parkway corridor to the Metrorail tracks on the east, established residential areas
11 along Guilford Drive and Knox Road on the south, and the Paint Branch stream on the west, as
12 shown on the attached map. The area includes portions of Planning Area 66.

13 WHEREAS, on February 2, 2000, in Council Resolution 4-2000, the County Council sitting
14 as the District Council directed the Maryland-National Capital Park and Planning Commission to
15 prepare a Sector Plan for the College Park US 1 Corridor, in accordance with the requirements of
16 Section 27-641 of the Zoning Ordinance; and

17 WHEREAS, the Prince George's County Planning Board published an informational
18 brochure for a Sector Plan and Sectional Map Amendment and held a public forum on February
19 7, 2000, to inform the public of the intent and procedures for the Sector Plan and a SMA;
20 established goals, concepts, guidelines, and a public participation program; convened a Sector
21 Advisory Planning Group to assist in formulating solutions to issues; and hosted charrettes and

1 advisory planning group meetings for the community; and

2 WHEREAS, as a result of testimony presented at the February 7, 2000, public forum to
3 include additional properties within the sector plan boundary, the County Council, sitting as the
4 District Council, on May 9, 2000, again directed the Maryland-National Capital Park and
5 Planning Commission to prepare the Sector Plan in accordance with the requirements of Section
6 27-641 of the Zoning Ordinance, for the purpose of including additional properties within the
7 Sector Plan boundary; and

8 WHEREAS, on June 1, 2000, the Prince George's County Planning Board held a second
9 public forum for the purpose of receiving public comment on the proposal to increase the area
10 within the Sector Plan boundary by including the additional properties; and

11 WHEREAS, on June 27, 2000, in Council Resolution 37-2000 the District Council directed
12 the Maryland-National Capital Park and Planning Commission to prepare a Sectional Map
13 Amendment for the Sector Plan area, including the additional properties, in accordance with the
14 requirements of Section 27-225.01.05 of the Zoning Ordinance; and

15 WHEREAS, the District Council and the Planning Board held a duly-advertised joint public
16 hearing on the Preliminary College Park US 1 Corridor Sector Plan and Proposed Sectional Map
17 Amendment on June 11, 2001, and the Planning Board adopted the preliminary Sector Plan and
18 endorsed the proposed SMA, with amendments, as described in Planning Board Resolution
19 PGCPB No. 01-200 on October 18, 2001; and

20 WHEREAS, the Adopted Sector Plan and Endorsed Sectional Map Amendment were
21 transmitted to the District Council on October 22, 2001, and the Council held a work session on
22 November 6, 2001, to review the joint public hearing testimony and the recommended
23 amendments of the Planning Board; and

24 WHEREAS, the District Council proposed four amendments to the Adopted Sector Plan
25 and Endorsed Sectional Map Amendment at the November 6, 2001, work session and approved a
26 resolution of amendments (CR-82-2001) at the Council meeting on November 19, 2001, and set
27 a second joint public hearing for February 5, 2002, to allow public comment on the proposed
28 amendments; and

29 WHEREAS, the District Council sought public comment on the amendments as described
30 in CR-82-2001, referred the amendments to the Planning Board, held a duly advertised joint
31 public hearing with the Planning Board on the proposed amendments on February 5, 2002,

1 received comments on the proposed amendments from the Planning Board on February 7, 2002,
2 and conducted a work session on April 9, 2002, to review public hearing testimony; and

3 WHEREAS, upon approval by the District Council, the Sector Plan will define land use
4 policies and development regulations while serving as the primary guide for future development
5 of this Sector Plan area within portions of Planning Area 66, will supercede portions of the 1989
6 *Approved Langley Park-College Park-Greenbelt Master Plan*, portions of the 1990 *Adopted*
7 *Sectional Map Amendment for Planning Area 66*, and will amend the 1982 *Master Plan of*
8 *Transportation and the Countywide Trails Plan*; and

9 WHEREAS, a principal objective of the Sector Plan and SMA is protection of the health,
10 safety, and general welfare of the citizens of Prince George's County; and

11 WHEREAS, the Sector Plan and SMA process provides for periodic comprehensive review
12 of long-range land use policies and zoning; and

13 WHEREAS, it is the intent of the SMA to ensure that future development will be in
14 accordance with the principles of orderly, comprehensive land use planning as expressed in the
15 Sector Plan, and the District Council has found it necessary to change the zoning of properties
16 which in its judgement, are in conflict with the Sector Plan's land use recommendations; and

17 WHEREAS, the District Council, having reviewed supporting materials submitted as part
18 of the comprehensive rezoning proposal and examined the testimony presented, finds that the
19 accumulated record along with County plans and policies justifies the zoning changes within this
20 SMA.

21 NOW, THEREFORE, BE IT RESOLVED by the County Council of Prince George's
22 County, Maryland, sitting as the District Council for that part of the Maryland-Washington
23 Regional District in Prince George's County, Maryland, that the Sector Plan and SMA for the
24 College Park US 1 Corridor are hereby approved with the amendments described below:

25 **AMENDMENT 1**

26 Location: The Autoville Drive North R-55 zoned residential area located west of the Sector Plan
27 area boundary, south of Cherry Hill Road, north of the undeveloped Mazza parcel,
28 including the rear portion of Lots 1, 4, 5, Parcel 25, and part of Parcel 33 that have frontage
29 on the east side of Autoville Drive, and excluding developed Parcels 19, 20, 22, 23 and Plat
30 4219 (Beckwith Property Subdivision). Also including two undeveloped R-55 zoned lots
31 (lots 8 and 9 of Block C in the Autoville Subdivision) south of the Mazza property and

1 along the west side of the undeveloped street known as Autoville Drive.

2 Previous Zoning: R-55 Zone.

3 Adopted Sector Plan Recommendation: Mixed-Use.

4 Endorsed SMA Recommendation: R-55 Zone.

5 SMA Amendment Action: M-U-I Zone, excluding Parcels 19, 20, 22, 23 and Plat 4219, which
6 remain in the R-55 Zone.

7 8 AMENDMENT 2

9 Location: Revise Sector Plan text references relating to potential land use impacts associated
10 with operation of airplane flights out of College Park Airport by deleting: subarea
11 references; Map 8 on page 39 (Proposed College Park Airport Aviation Policy Areas); and
12 removal of Table 3 on page 40 of the adopted Sector Plan.

13 Adopted Sector Plan Recommendation: Under Subareas 1b, 1c, 1d, 1e, 3a, and 3b, the plan
14 states "compliance with safety regulations relating to proximity to the College Park Airport.
15 Portions of these subareas lie within the aviation policy areas identified in a November 10,
16 2000 Airport Land Use Compatibility and Air Safety Study (see Map 8 and Table 3)". The
17 Development District text refers to these same subareas and the above quoted text. It also
18 states in reference to the airport issues that: "Compliance with applicable safety regulations
19 shall be demonstrated."

20 Approved Sector Plan Amendment Text: Revise the plan text by adding the following as Item 9
21 under the section entitled "Corridor-Wide Land Use and Zoning Recommendations" (page
22 34) to indicate: "**Proximity to College Park Airport** -- Because portions of the Sector
23 Plan area are proximate to the College Park Airport, strategies to develop these areas in a
24 manner compatible with airport operations should be part of the development planning
25 process."

AMENDMENT 3

Location: Shell gasoline station property located on the west side of US 1 at 9322 Baltimore Avenue.

Previous Zoning: C-S-C Zone.

Adopted Sector Plan Recommendation: Mixed-Use with no provision to allow a car wash as a permitted use.

Endorsed SMA Recommendation: M-U-I Zone.

SMA Amendment Action: M-U-I Zone with a revised footnote 39 in the Plan's Table of Permitted Uses (page 243) to allow a "Car Wash: (iii) All others" as a permitted use in portions of Subarea 4d north of Erie Street, by revising footnote 39 to read, substituting the following revised text: "Future new uses are only permitted in the mixed-commercial subareas (Subareas 6b, 4f, portions of 4e north of Fox Street, and portions of 4d north of Erie Street) of the College Park US 1 Corridor Sector Plan and are subject to pertinent Development District Standards".

AMENDMENT 4

Location: Uses with drive-in and drive-through windows within Subareas 1 and 3.

Adopted Sector Plan Recommendation: The Adopted Plan text (page 31, Item 7) prohibits uses with drive-through windows in Subareas 1 and 3, where compact, pedestrian-oriented development should be located.

Approved Sector Plan Amendment Text: Revise text (page 31, Item 7) to indicate: "Retail, office, hotel, and multifamily uses are recommended as the most intensive uses within the Sector Plan area. The intent of the plan is to prohibit industrial uses, including consolidated storage and warehouses, and to limit new auto-related sales and services to specific subareas. Also, uses with drive-in or drive-through windows are prohibited in Subareas 1 and 3, where compact, pedestrian-oriented development should be located. The exception is for existing uses where drive-in or drive-through windows must be removed because of government actions. In such cases, they shall be considered permitted uses in Subarea 3 only. In addition, the Table of Permitted Uses further restricts other uses deemed undesirable within the context of achieving the goals of this Sector Plan and the City's visions for the US 1 Corridor and Town Center (see the Permitted Uses chapter)". Also

1 revise the Adopted Plan's language (page 179, Item E) to state: "All new drive-in or drive-
2 through windows for any use are prohibited in Subareas 1 and 3. However, any existing
3 drive-in or drive-through windows in Subarea 3 that must be removed because of
4 governmental action shall be considered a permitted use and shall be exempt from the
5 Development District Standards and site plan review."

6 BE IT FURTHER RESOLVED that staff is authorized to make appropriate text and map
7 revisions to correct identified errors, reflect updated information, and incorporate the changes
8 resulting from Council actions described in this Resolution.

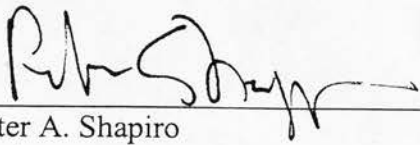
9 BE IT FURTHER RESOLVED that this Sectional Map Amendment is an amendment to
10 the Zoning Ordinance and to the official Zoning Map for the Maryland-Washington Regional
11 District in Prince George's County. The zoning changes approved by this Resolution shall be
12 depicted on the official Zoning Maps of the county.

13 BE IT FURTHER RESOLVED that the provisions of this Resolution are severable: if any
14 zone, provision, sentence, clause, section, or part thereof is held illegal, invalid, unconstitutional,
15 or inapplicable to any person or circumstances, such illegality, invalidity, unconstitutionality, or
16 inapplicability shall not affect or impair any of the remaining provisions, sentences, clauses,
17 sections, or parts of the Resolution or their application to other zones, persons, or circumstances,
18 since the same would have been adopted without the incorporation in this Resolution of any such
19 illegal, invalid, unconstitutional or inapplicable zone, provision, sentence, clause, section, or part
20 thereof.

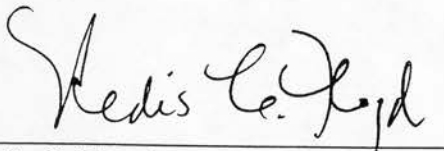
1 BE IT FURTHER RESOLVED that this Resolution shall take effect on the date of its
2 adoption.

Adopted this 30th day of April, 2002.

COUNTY COUNCIL OF PRINCE GEORGE'S
COUNTY, MARYLAND, SITTING AS THE
DISTRICT COUNCIL FOR THAT PART OF
THE MARYLAND-WASHINGTON REGIONAL
DISTRICT IN PRINCE GEORGE'S COUNTY,
MARYLAND

BY: 
Peter A. Shapiro
Chair

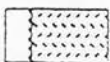
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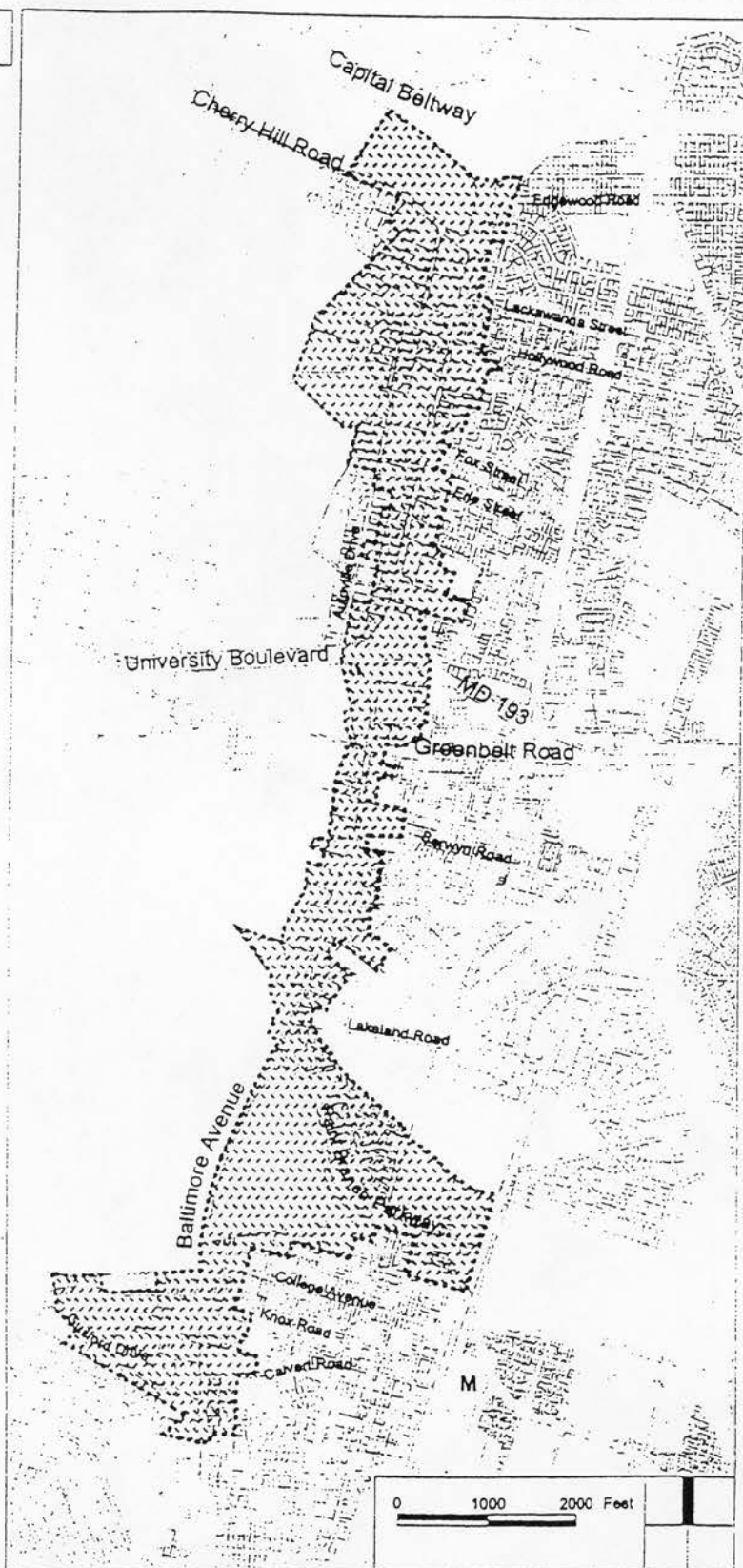
Redis C. Floyd
Acting Clerk of the Council

maps available in hard copy only

LEGEND

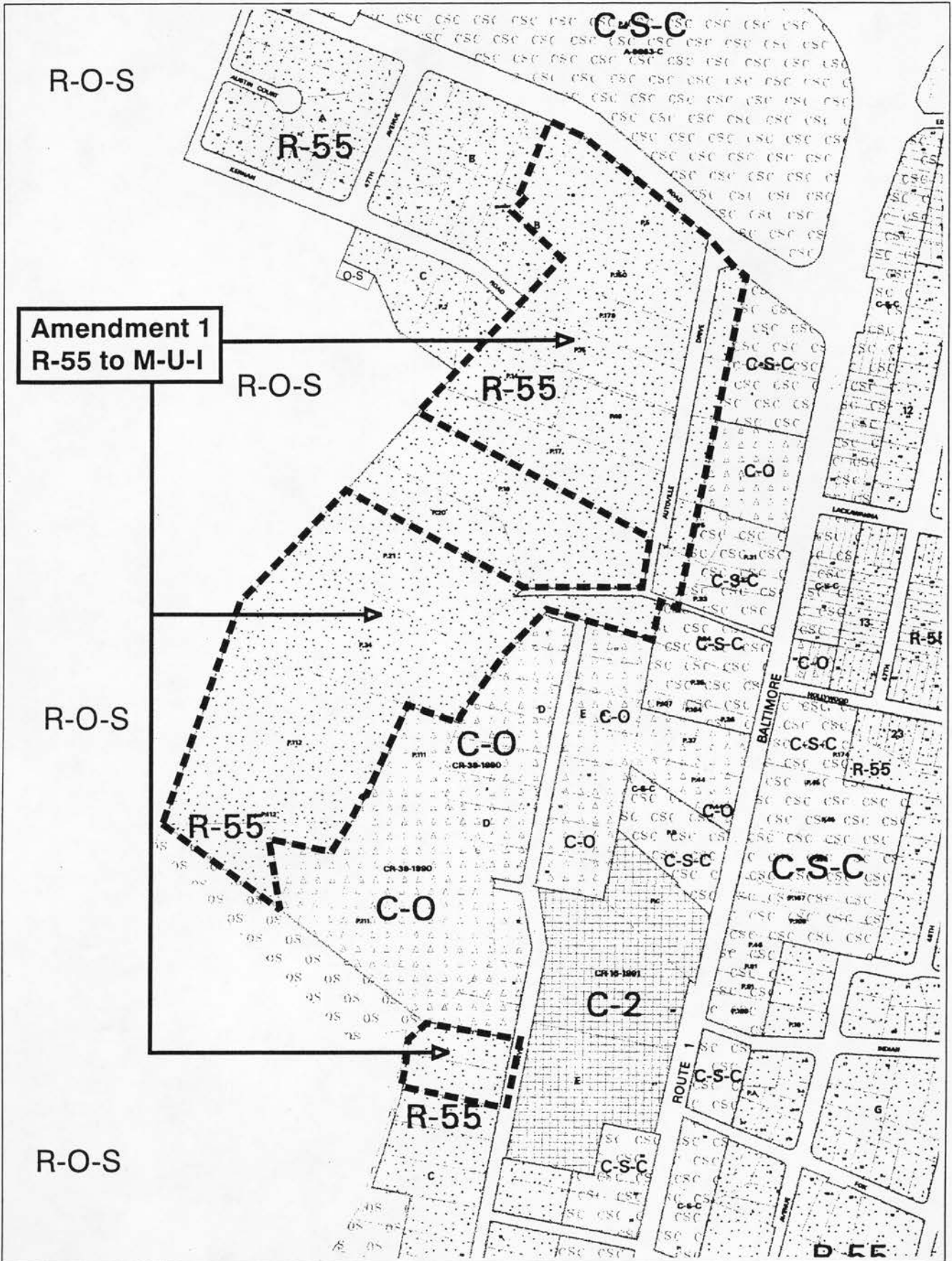


DDOZ Boundary
CPUS1-1
(superimpose DDO)



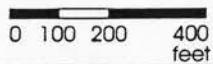
DDOZ Boundary

Map I



Subareas 4F, 5B and 6A

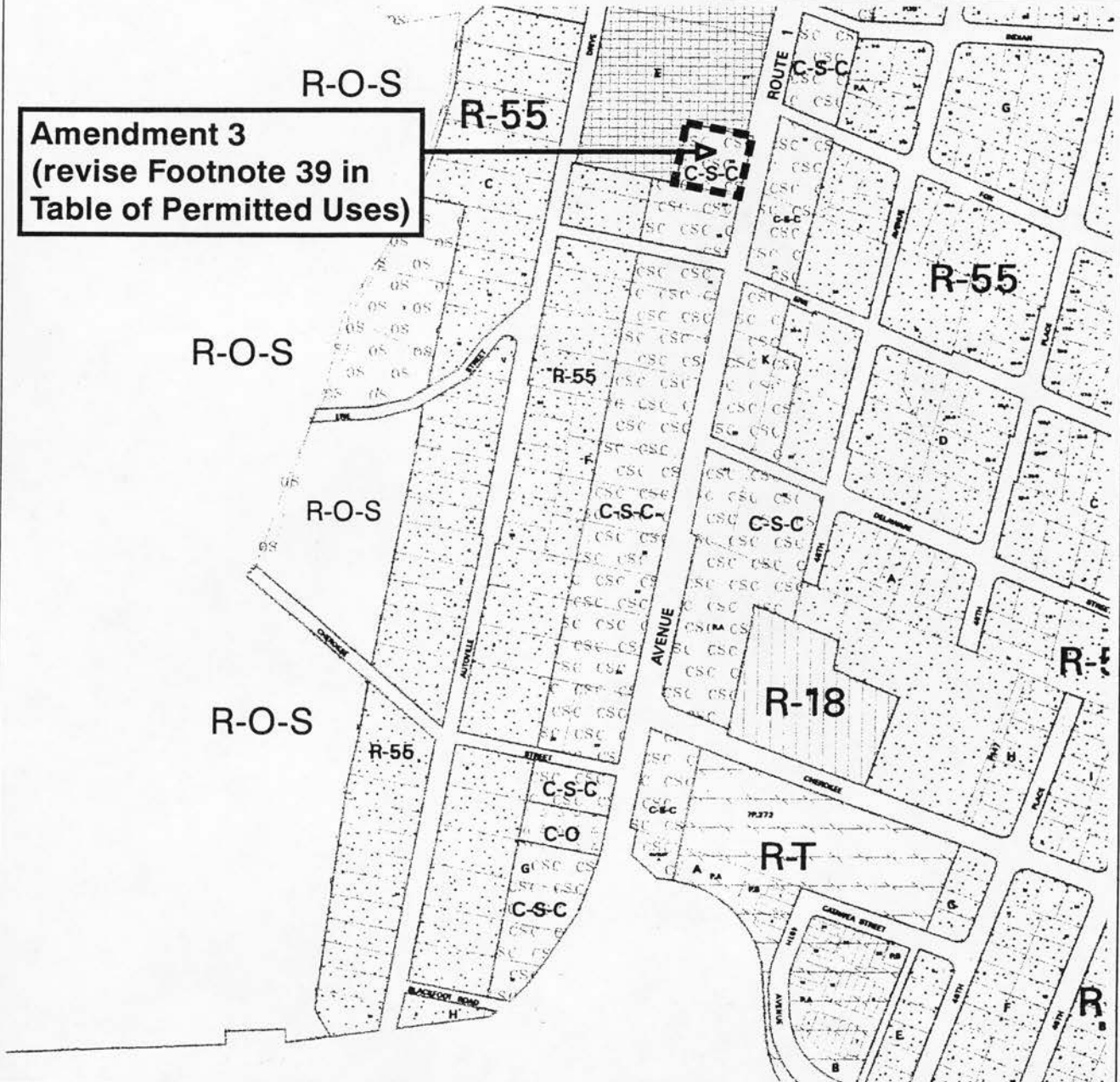
Amendment # 1 Properties





Subarea 4

Amendment 3
(revise Footnote 39 in
Table of Permitted Uses)



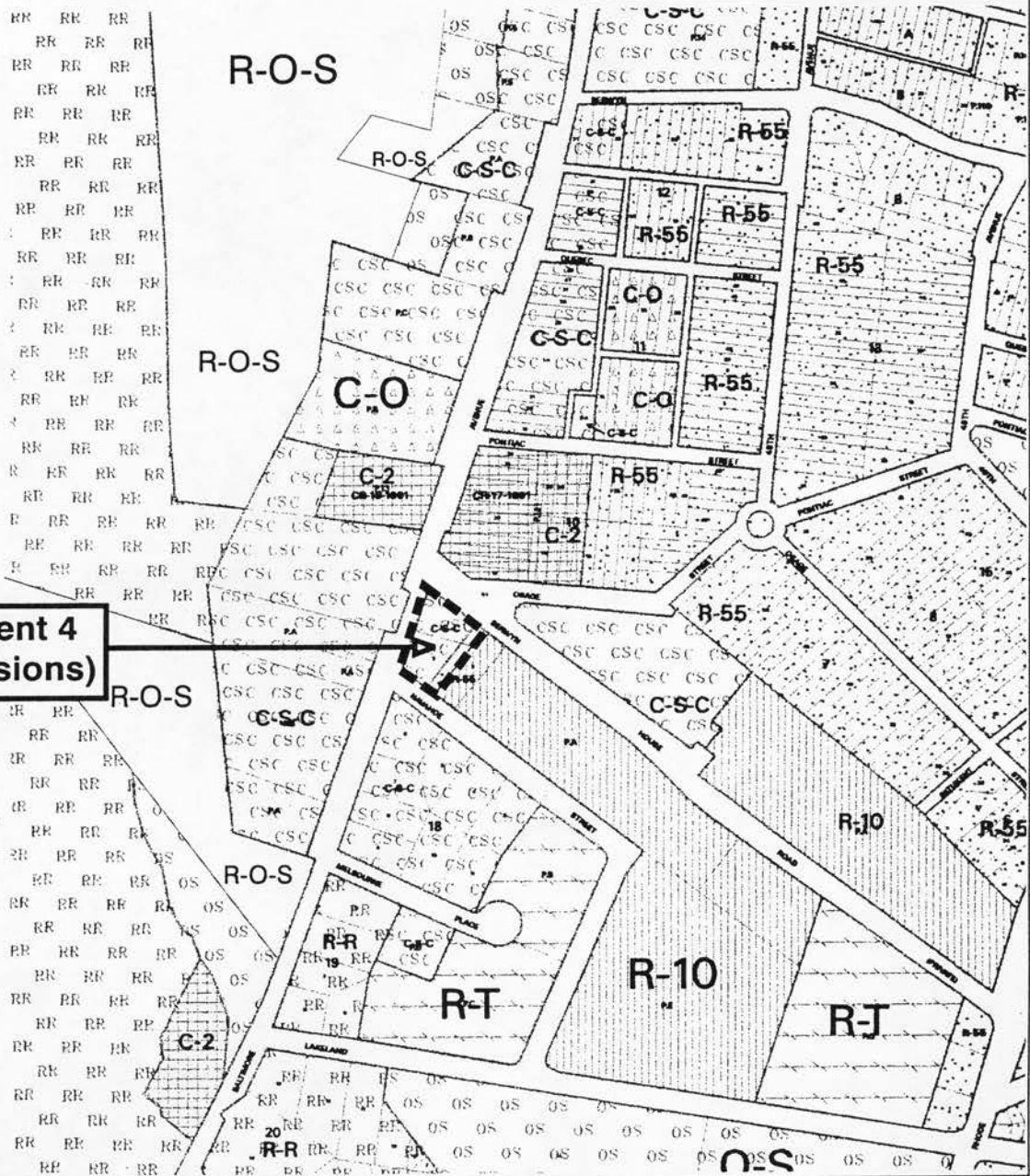
Subarea 4D

Amendment 3





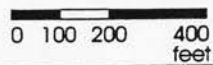
Subarea 3



**Amendment 4
(text revisions)**

Subarea 3B

Amendment 4



**PRINCE GEORGE'S COUNTY COUNCIL
AGENDA ITEM SUMMARY**

Meeting Date: 4/30/2002

Reference No.: CR-18-2002

Proposer: Planning Board

Draft No.: 1

Sponsors: Shapiro, Hendershot, Scott

Item Title: A Resolution approving with amendments, the Sector Plan and Sectional Map Amendment for the College Park US 1 Corridor

Drafter: Reggie Baxter
Community Planning, M-NCPPC

Resource Personnel: Steven M. Gilbert
Principal Counsel

LEGISLATIVE HISTORY:

Date Presented: ___/___/___

Executive Action: ___/___/___

Committee Referral: ___/___/___

Effective Date: 4/30/2002

Committee Action: ___/___/___

Date Introduced: 4/30/2002

Public Hearing: ___/___/___

Council Action: 4/30/2002 ADOPTED

Council Votes: PS:A, DB:A, TD:N, JE:A, TH:A, RVR:-, AS:A, MW:A

Pass/Fail: P

Remarks: _____

5/30/2001: Technical error corrected on page 3 line 31 "(lots 8 & 9 of Block C in the Autoville Subdivision)

BACKGROUND INFORMATION/FISCAL IMPACT

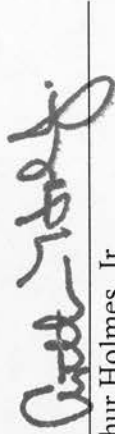
(Includes reason for proposal, as well as any unique statutory requirements)

This resolution proposes to approve with amendments, the Sector Plan and Sectional Map Amendment for the College Park US 1 Corridor, thereby defining long-range land use and development policies, detailed zoning changes, design standards, and a Development District Overlay Zone for that portion of Prince George's County generally bounded by the Capital Beltway on the north, commercial and some residential properties along the east and west sides of Baltimore Avenue (US 1), the East Campus area of the University of Maryland along the Paint Branch Parkway corridor to the Metro rail tracks on the east, established residential areas along Guilford Drive and Knox Road on the south, and the Paint Branch stream on the west as shown on the attached map. The area includes portions of Planning Area 66.

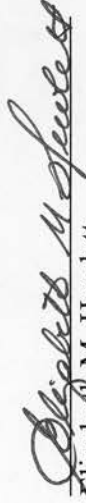
Appendix D

CERTIFICATE OF ADOPTION AND APPROVAL

This sector plan amends the 1989 Approved Master Plan and the 1990 Adopted Sectional Map Amendment for Langley Park, College Park and Greenbelt (Planning Areas 65, 66 and 67) for the sector area only. The plan also amends the 1982 Master Plan of Transportation and the 1975 Countywide Trails Plan. This sector plan was adopted by the Prince George's County Planning Board by Resolution Number 01-200 on October 18, 2001, and was approved by the Prince George's County Council, by Resolution Number CR-18-2002, on April 30, 2002, after advertised joint public hearings held on June 11, 2001 and February 5, 2002.



Arthur Holmes, Jr.
Vice Chairman



Elizabeth M. Hewlett
Chairman

THE MARYLAND-NATIONAL CAPITAL
PARK AND PLANNING COMMISSION



Patricia Colihan Barney
Secretary-Treasurer

ACKNOWLEDGMENTS

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- Michael E. Petrenko, AICP, Deputy Planning Director
- Al Dobbins, AICP, Chief, Community Planning Division

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* Former Staff

