

*Approved*  
**City of Mount Rainier**  
*Mixed-Use Town Center Zone Development Plan*  
November 2010





# Abstract

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**PROJECT:** Approved City of Mount Rainier Mixed-Use Town Center Zone Development Plan

**AUTHOR:** The Maryland-National Capital Park and Planning Commission (M-NCPPC)

**SUBJECT:** Approved City of Mount Rainier Mixed-Use Town Center Zone Development Plan

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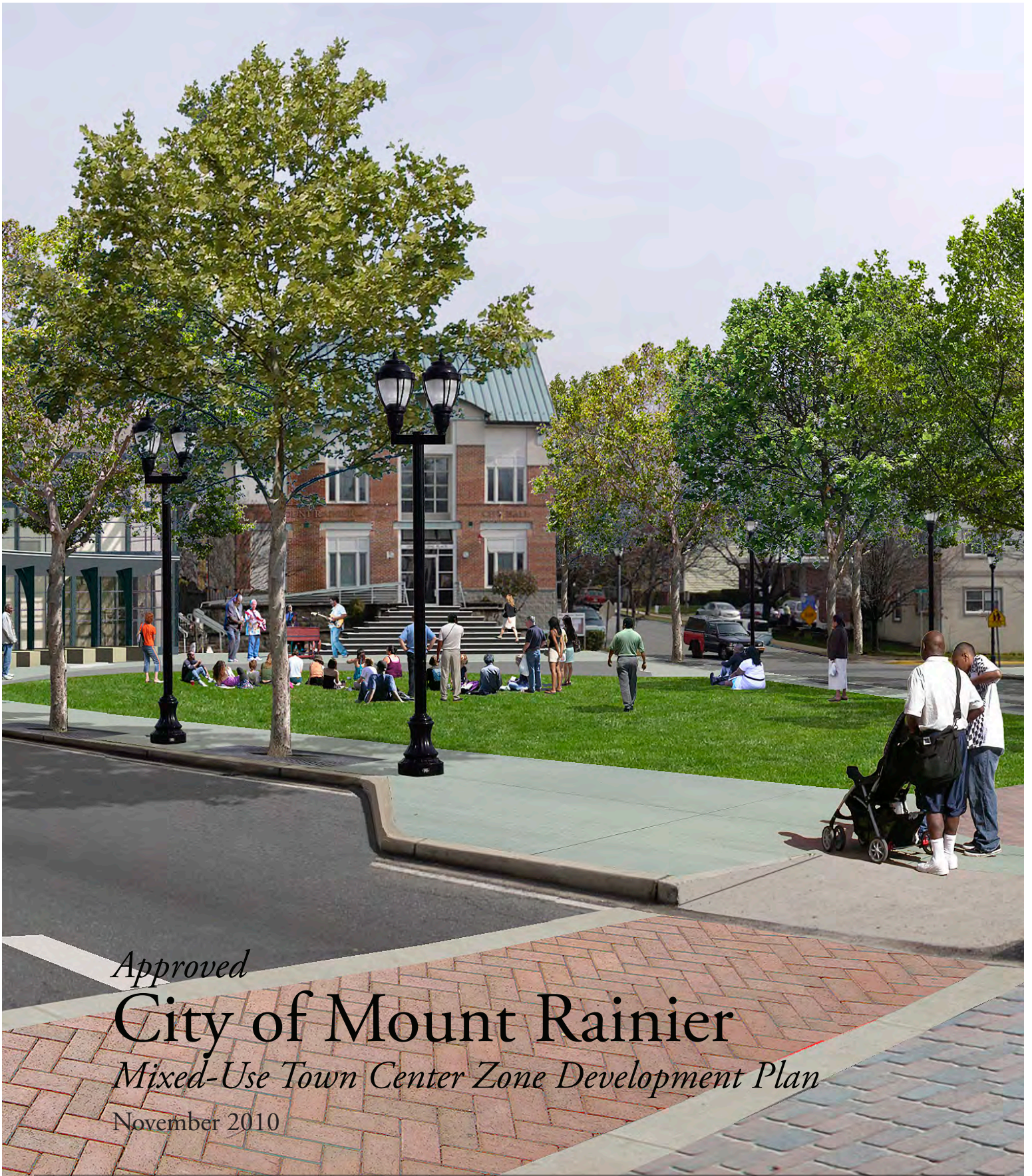
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**ABSTRACT:** The mixed-use town center (M-U-TC) zone district was created in 1994 to promote reinvestment and development in the county's older, more established mixed-use areas. This document updates the July 1994 *Mount Rainier Town Center Development Plan* and revises the development policies and design standards and guidelines that will support a pedestrian-friendly, vibrant mixed-use town center for the City of Mount Rainier.

This plan was developed by M-NCPPC during an extensive 18-month public participation process that involved the input of many residents and public officials. The plan, which takes into account current market and economic conditions, consists of five sections: the Introduction, which defines the project area and outlines the public participation process; the Existing Conditions Analysis, which includes a historical overview and current economic and marketing considerations; the Plan Vision, which describes the envisioned future for the M-U-TC area; the Design Standards and Guidelines, which promote high-quality urban and architectural design; and the Implementation, which provides information on funding resources to encourage, promote, and facilitate near, medium, and long-term project goals.





*Approved*

# City of Mount Rainier

*Mixed-Use Town Center Zone Development Plan*

November 2010

The Maryland-National Capital Park and Planning Commission

[www.mncppc.org](http://www.mncppc.org)



Prince George's County Planning Department

14741 Governor Oden Bowie Drive

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The Commission has three major functions:

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- The acquisition, development, operation, and maintenance of a public park system.
- In Prince George's County only, the operation of the entire county public recreation program.

The Commission operates in each county through a Planning Board appointed by and responsible to the county government. All local plans, recommendations on zoning amendments, administration of subdivision regulations, and general administration of parks are responsibilities of the Planning Boards.

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- Our mission is to help preserve, protect, and manage the county's resources by providing the highest quality planning services and growth management guidance and by facilitating effective intergovernmental and citizen involvement through education and technical assistance.
- Our vision is to be a model planning department of responsive and respected staff who provide superior planning and technical services and work cooperatively with decision-makers, citizens, and other agencies to continuously improve development quality and the environment and act as a catalyst for positive change.

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# Executive Summary

## Introduction

The City of Mount Rainier Mixed-Use Town Center (M-U-TC) Zone Development Plan updates the previously approved 1994 *Mount Rainier Town Center Development Plan*. The new plan takes into account current economic/market conditions. Its revised development policies and design standards will guide future development within the plan area and ensure a pedestrian-friendly, vibrant mixed-use town center.

The project area encompasses the buildings along the Rhode Island Avenue corridor east of the District of Columbia line at Eastern Avenue to just beyond the traffic circle at 34th Street. It also extends from the building frontages along 34th Street just north of Bunker Hill Road to about an eighth of a block south of Rhode Island Avenue.

Several hundred residents and stakeholders within a half-mile radius of the study area participated in the development plan update through more than 12 public meetings, hands-on work sessions, and stakeholder group meetings held between September 2009 and March 2010.

## Existing Conditions Analysis

As a part of the development plan update, a detailed analysis was made of existing conditions in Mount Rainier. This analysis included research and documentation of the following:

- The physical/geographic description of the M-U-TC boundary
- Historic background and development patterns
- Transportation systems
- Market analysis
- Demographics
  - Existing land uses
  - Market demands and development
  - Opportunities

## Plan Vision

The plan vision incorporates urban design and illustrative plans, which provide a framework and visualization of the planned town center, as well as recommendations for the three sub-plan areas:

- Upper 34th Street
- Rhode Island Avenue
- Civic Core/City Center



FIGURE 1: An illustrative showing the sub-plan areas.



## Design Standards and Guidelines

The purpose of the design standards and guidelines is to promote high-quality urban and architectural design within the context of the M-U-TC Zone development plan area and to encourage a cohesive and attractive environment consistent with the plan vision and economic revitalization.

The design standards and guidelines define a design review process that ensures predictability for development or redevelopment from concept, through review, to approval and implementation.

First, the design standards and guidelines are based on an understanding of the Mount Rainier Historic District and an overview of its defining characteristics.

The plan area's three districts are described in detail:

- Upper 34th Street: "Main Street" pedestrian scaled one- to three-story retail.
- Rhode Island Avenue: "Boulevard" three- to five-story larger scale, retail/residential mix.
- Civic Core/City Center: "Civic Center" three- to four-story larger scale, civic/residential mix.

Next, the design standards and guidelines provide graphics and details for the public realm, site design, and architectural elements. Examples of these graphics and details include street sections at 34th Street and Rhode Island Avenue, pedestrian crosswalks, sidewalks, street lighting, building height, setbacks, massing, walls, fences and screening, façades and fenestration, storefronts, material, color, porches and front yards, additions, subtractions, and signage.

Specific recommendations are provided for key buildings within the plan area.

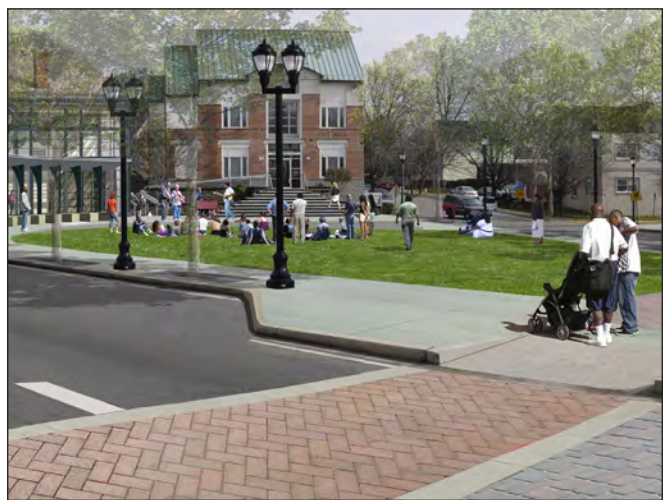
Finally, the design standards and guidelines also address public art, sustainability, parking, and the treatment of historic buildings within the Mount Rainier M-U-TC Zone development plan area.



Plan vision: 34th Street, looking south.



Plan vision: Rhode Island Avenue, looking west.



Plan vision: civic core/city center, looking east.

## Implementation

For the effective implementation of the plan vision, it is suggested that a number of key stakeholders form new working relationships and coordinate initiatives. The M-U-TC Zone development plan provides a framework for implementation, paying special attention to priorities, timing, and possible funding strategies. The key elements to the implementation philosophy are:

- A comprehensive framework
- Building on existing resources
- Priority and focus
- Quality over quantity
- Incremental change

The key implementation priorities are:

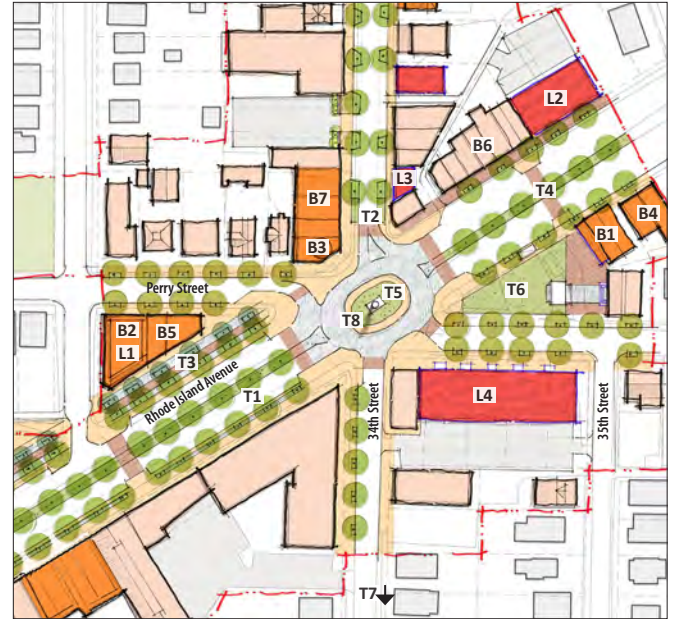
- To reinforce the role of the M-U-TC Zone development plan as the critical planning tool.
- Creation of a “downtown” action plan to concentrate public investment and energy on key plan areas: Upper 34th Street, Rhode Island Avenue, and the Civic Core/City Center.
- Creation of a “Main Street” program.

The M-U-TC Zone development plan provides implementation charts and action timelines for each of the key plan areas.

Implementation precedents such as Barracks Row in Washington, D.C., and Takoma Park Main Street in Maryland provide specific examples of how the recommended strategies worked in other communities.

The section on implementation tools and programs itemizes current successful economic development programs and transportation implementation tools. Economic revitalization programs are outlined at the Prince George’s County, State of Maryland, and federal levels.

Profiles of property/building clusters provide details on existing conditions and development potential for major development or redevelopment opportunity sites highlighted in the plan vision. Information is included on lot area, land use, building details, historic integrity, and ownership. Details are also provided on implementation factors such as costs, benefits, and phasing.



**FIGURE 2: Implementation map—Civic Core.**

*T—Transportation recommendations*

*L—Land use recommendations*

*B—Building recommendations*







# Introduction 1



## Plan Purpose

On June 4, 2009, the Prince George's County District Council initiated the process to update the existing 1994 *Mount Rainier Town Center Development Plan*. The Mixed-Use Town Center (M-U-TC) Zone was created in 1994 to promote reinvestment and redevelopment in the older, more established mixed-use areas in Prince George's County. Mount Rainier was the first jurisdiction in the county to use the M-U-TC Zone.

The City of Mount Rainier Mixed-Use Town Center (M-U-TC) Zone Development Plan updates the previously approved 1994 development plan. The plan takes into account current economic/market conditions. Its revised development policies and design standards will guide future development within the plan area and ensure a pedestrian-friendly, vibrant mixed-use town center.

## Study Area Description

The project area encompasses all parcels along the Rhode Island Avenue corridor east of the District of Columbia line at Eastern Avenue to just beyond the traffic circle at 34th Street. It includes the building frontages along 34th Street just north of Bunker Hill Road to about an eighth of a block south of Rhode Island Avenue. The 2004 *Approved Sector Plan and Sectional Map Amendment for the Prince George's County Gateway Arts District* amended the 1994 M-U-TC boundary. This development plan uses the amended M-U-TC boundary. **(See the aerial photograph and outline below.)**



An aerial photograph of the Mount Rainier M-U-TC Zone.



## Public Outreach Process



Community members present ideas at the October 31, 2009, hands-on community design session.

A critical component of creating a successful, sustainable development plan is ensuring an open and inclusive public participation process. Broad and active community engagement involves the public in plan making from the early stages of determining issues and assets, through visioning, to final plan formulation. This planning process invited community residents, business owners, and government officials to share their ideas. The aspirations of all segments of the community are incorporated into a shared vision for the future.

In September 2009, M-NCPPC mailed several hundred residents and business owners within a half-

mile radius of the Mount Rainier M-U-TC to notify them of the public planning process and community meeting schedule for the project. M-NCPPC staff also hand delivered flyers to all operating businesses within the M-U-TC approximately one week prior to the project kickoff meeting on October 7, 2009. The meeting schedule was also available on the project web site at [www.pgplanning.org/Mount-Rainier.htm](http://www.pgplanning.org/Mount-Rainier.htm), published in the monthly Mount Rainier newsletter, and announced on public access television prior to each meeting date.



## Community Workshops

Eight well-attended community meetings were held at the Bunker Hill Fire Station between October 2009 and March 2010. At these meetings, community members gave their input toward shaping an updated vision for Mount Rainier. For example, during the hands-on community design session on October 31, 2009, participants were divided into three groups to discuss issues related to transportation, land use, streetscape and the pedestrian realm, development infill opportunities, vacant buildings, potential additions and subtractions to existing buildings, and historic preservation. Each group produced and presented drawings to illustrate their findings on the M-U-TC area. This session helped create the vision framework for Mount Rainier.



October 31, 2009, hands-on community design session.

## Community Workshop Outreach Schedule

### Community Kick-off Meeting

October 7, 2009

### Community Workshop 1

October 19, 2009

### Hands-on Community Design Session Workshop 2

October 31, 2009

### Work in Progress Open House Workshop 3

November 10, 2009

### Artists Stakeholder Feedback Session

December 15, 2009

### Vision Plan—Feedback Session Workshop 4

January 12, 2010

### Community Workshop—Presentation of Implementation/Development Plan

February 23, 2010

### Community Workshop—Presentation of Design Guidelines and Standards

March 23, 2010



Sample table drawing from a hands-on community design session.

## Stakeholder Dialogues

Between September and December 2009, the project team also conducted a number of meetings with individual stakeholder groups to solicit their opinions regarding the future vision for the M-U-TC area. These groups included the following:

- District of Columbia government officials representing the Great Streets Initiative (Rhode Island Avenue).
- The Mount Rainier Town Center Design Review Committee.
- Mount Rainier Mayor and City Council.
- Property and local business owners within the M-U-TC.
- Local arts-related non-profits and property management companies.
- Local artists.

The presentations, meeting minutes, and meeting summary handouts from each of the eight community meetings were posted on the M-NCPPC project web site at the conclusion of the meeting. Summaries of each stakeholder meeting were also posted on the same web site in the Existing Conditions Report dated December 2009.



September 21, 2009, meeting with the City of Mount Rainier Mayor and Council.



Open house and displays at the community meeting on November 10, 2009.



December 15, 2009, artists stakeholder feedback session.

**The City of Mount Rainier  
Mixed-Use Town Center  
(M-U-TC) Zone Development Plan**

**Summary of the November 10th Community Meeting**

After the Open House Session, Cunningham Quill Architects (CQ), gave a brief presentation discussing their thoughts concerning these questions. One idea presented was that the M-U-TC area could be subdivided into smaller Districts. Additionally, CQ presented images of public art and facade color of similar scaled projects that may be applicable to the M-U-TC area.

The last portion of the meeting was an open dialogue with the community. The notes from the community dialogue can be found at [www.pgplanning.org/Mount-Rainier.htm](http://www.pgplanning.org/Mount-Rainier.htm).

**Upcoming Community Meetings**

**Feedback Session**  
7pm **Tuesday 1/12/2010**  
Bunker Hill Fire Station

**Draft Development Plan Presentation**  
7:00 p.m. **Tuesday 2/23/2010**  
Bunker Hill Fire Station

**Design Standards & Guidelines Presentation**  
7:00 p.m. **Tuesday 3/23/2010**  
Bunker Hill Fire Station

For more information contact:  
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[www.pgplanning.org/Mount-Rainier.htm](http://www.pgplanning.org/Mount-Rainier.htm)

**Mount Rainier Streetscapes**

**Barracks Row Streetscapes**

**Top:** The streetscapes of the Mount Rainier M-U-TC area are composed of a muted color palette.

**Bottom:** The streetscape of Barracks Row, located along Eighth Street, S.E. in Washington, D.C., is composed of a more vibrant, larger color palette than Mount Rainier.

PRINCE GEORGE'S COUNTY PLANNING DEPARTMENT  
THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

Meeting summary example.







Existing Conditions Analysis **2**



## Context

### Regional

The City of Mount Rainier is a historic community in Prince George's County, Maryland, inside the Capital Beltway along the northeast boundary of Washington, D.C. The city center, situated in the historic district, is located at the intersection of Rhode Island Avenue and 34th Street between the District of Columbia and Hyattsville, Maryland. Mount Rainier is adjacent to several residential neighborhoods, including Woodridge, Langdon, and Michigan Park in the District of Columbia, and Brentwood, Cottage City, and Hyattsville in Prince George's County.

Rhode Island Avenue is an important regional north/south thoroughfare, connecting Washington, D.C., to College Park and Laurel, Maryland. Bladensburg Road, which runs parallel to Rhode Island Avenue, provides an alternative, and at times competing, retail corridor within the area. Going north on US 1, the municipalities of Brentwood, Hyattsville, and College Park also have important nodes of economic activity that serve their adjacent neighborhoods along the corridor.

Additionally, Mount Rainier, along with the municipalities of Brentwood, North Brentwood, and Hyattsville, is located in the Prince George's County Gateway Arts District. This district was established by the 2004 *Approved Sector Plan and Sectional Map Amendment for the Prince George's County Gateway Arts District* with the purpose of building upon the unique characteristics of the four municipalities as arts communities, while preserving the heart of the four small towns. It provides the basis for lively commercial and mixed-use centers that retain and attract new artists to these municipalities.

### Site

The Mount Rainier M-U-TC area is surrounded by tributaries flowing into the Anacostia River. The topography of the area gently slopes downhill from west to east along Rhode Island Avenue, and dramatically slopes downhill from north to south along 34th Street.

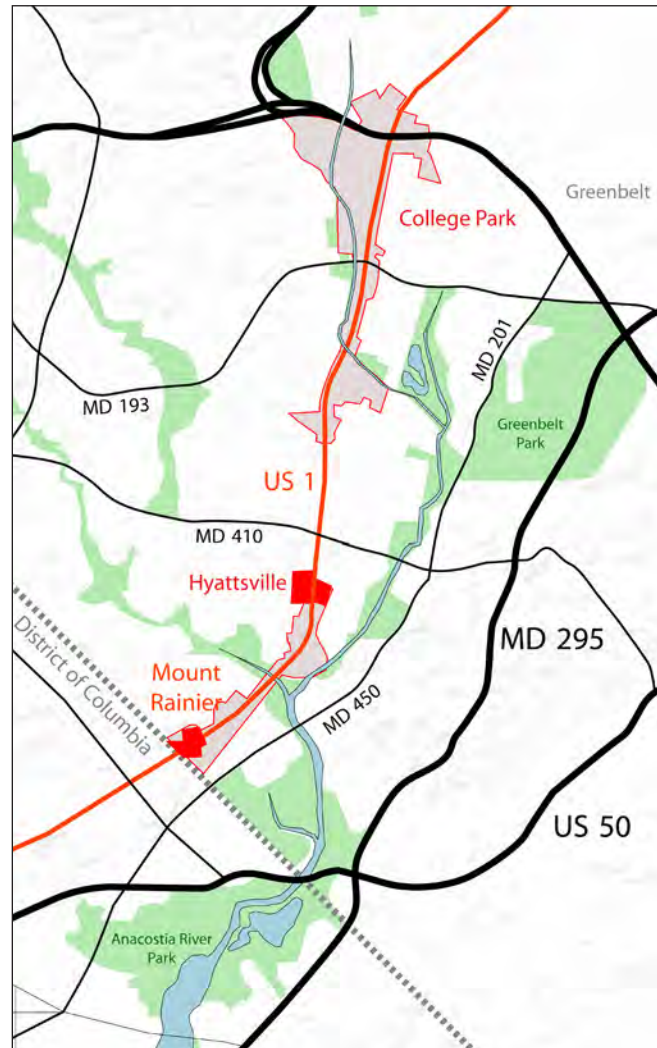


FIGURE 3: REGIONAL CONTEXT MAP.



Looking north up 34th Street from the traffic circle.

Specifically, across the M-U-TC area boundary, there is a 30-foot grade change between the upper and lower portion of 34th Street.

Rhode Island Avenue serves as the regional “Main Street” of the M-U-TC area, but lacks a defined and unified boulevard character. The other primary roadway within the M-U-TC, 34th Street, serves as the local “Main Street” for the immediate surrounding residential neighborhoods.

The entire mixed-use town center is located within a five-minute walk of the intersection of Rhode Island Avenue and 34th Street, which is the “symbolic center” of the M-U-TC. The area is accessible via multiple major bus routes operated by the Washington Metropolitan Area Transit Authority (WMATA) and Prince George’s County. Additionally, a potential MARC station, referenced in the 2009 *Approved Port Towns Sector Plan and Sectional Map Amendment*, may be sited along the CSX railroad right-of-way in the future and would be within walking distance of the M-U-TC and surrounding residential areas.

The M-U-TC contains approximately 2,400 linear feet of frontage along both sides of Rhode Island Avenue (running from Eastern Avenue to the Mount Rainier Public Library) and approximately 2,000 linear feet of frontage along both sides of 34th Street.

Typical of many small town “Main Streets,” the M-U-TC originally contained a mixture of both mixed-use and solely residential buildings. The mixed-use buildings contained ground-floor retail with a residential use above. Over time, many residential structures were re-adapted for commercial uses, such as the buildings located on the east side of 34th Street. Additionally, many buildings were torn down and replaced (and/or modified) with lower-scaled buildings and surface parking lots. The continuous street wall frontages once found along both commercial streets of the M-U-TC no longer exist.

The Mount Rainier M-U-TC area is smaller in scale relative to other local redeveloping main streets, such as 8th Street, S.E., Washington, D.C. The modest scale of the mixed-use town center area creates a unique character and development opportunities for Mount Rainier along Rhode Island Avenue.



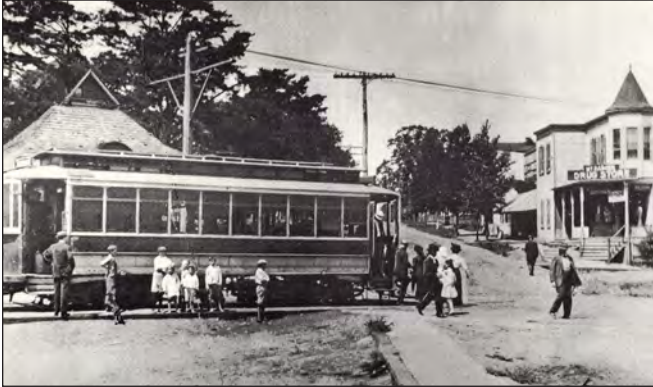
FIGURE 4: M-U-TC DEVELOPMENT PLAN AREA.



FIGURE 5: SCALE COMPARISON—8TH STREET, S.E., WASHINGTON, D.C.



## Historical Analysis



Rhode Island Avenue and 34th Street, circa 1910.



The scene of a fire in 34th Street, circa 1955.

### Early History of the City: 1897 to 1945

The Maryland and Washington Railways started operations connecting what is now the area encompassing the City of Mount Rainier to the District of Columbia in 1897, with a stop located at the intersection of Rhode Island Avenue and 34th Street called the District Station. In 1902, local developers purchased 1,090 acres of the Clemson family farm, surveyed the land, and renamed it “Mount Rainier.” City residents petitioned and achieved incorporation of the City of Mount Rainier in 1910. At the time, the city had approximately 1,242 residents and contained 163 buildings.

During the period between its incorporation and the end of 1945, the city experienced growth in population, land area, and development. In 1929, the city annexed areas where the Queenstown and Queens Manor Garden Apartments are now situated. An additional annexation occurred in 1943. In 1934, upper 34th Street was extended through to Chillum Road making it a connector street to downtown Mount Rainier. Detached, one story, wood frame vernacular “bungalow style” homes became the predominant single-family residential architectural style built throughout the city between the 1920s and 1940s. The development patterns of the areas north and outside of the Mount Rainier M-U-TC became more suburban in character with the completion of the Kaywood Gardens, Queenstown, and Queens Manor Garden apartment complexes and their nearby strip shopping centers in 1943. At that time, the population of Mount Rainier grew to approximately 4,830 residents.

### Post-1945 History: A Time of Expansion and Change

In 1955, the City of Mount Rainier annexed its final parcels and reached its highest recorded population at approximately 11,000 residents. Three years later, in 1958, the streetcar line servicing Mount Rainier closed and was replaced by various bus lines including some that are currently in operation. A period of gradual population decline followed and, in 1980, the population of Mount Rainier dipped to a post-1945 low of approximately 7,361 residents.

However since 1980, Mount Rainier has experienced a resurgence as the general public has become aware of the city’s charm, due in part, to the quantity of architecturally distinctive bungalow housing stock, its proximity to the District of Columbia, and a new, burgeoning arts district.

In 1994 the Mount Rainier M-U-TC zone was established to help revitalize the commercial areas along Rhode Island Avenue and 34th Street, and in 1998 the Gateway Community Development Corporation was founded to develop an arts district to support the artists living in the municipalities of Mount Rainier, Brentwood, and Hyattsville. Today, Mount Rainier boasts one of the largest and most vibrant artist communities in the Washington metropolitan area. The works of prominent artists, based in Mount Rainier, are exhibited both locally and nationally.

## Property Analysis

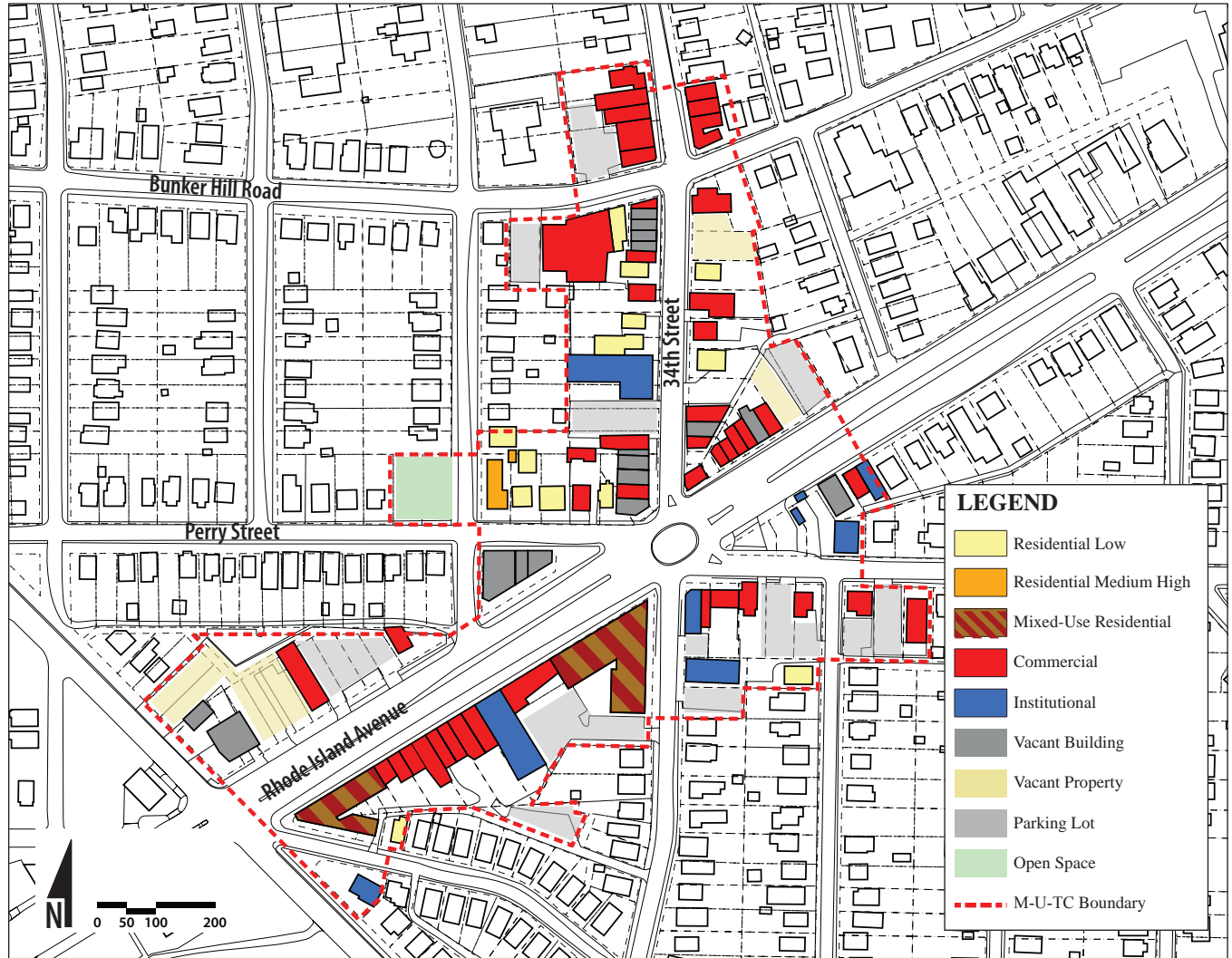


FIGURE 6: EXISTING PROPERTY MAP.

### Existing Property Survey

There are approximately 90 properties, which contain a total of 86 buildings, within the boundary of the Mount Rainier M-U-TC. As of fall 2009, 70 buildings are occupied with various uses and the remaining 16 buildings are vacant. Additionally, there are a number of significant vacant parcels of land within the boundary. A total of approximately 350,000 square feet of residential, commercial, and institutional uses are contained within the existing occupied and vacant buildings. (For additional information regarding land uses within the M-U-TC, please refer to pages 20 and 21.)

Floor area ratio (FAR) is the calculation of the total building square footage, or building area, divided by the site size square footage. Rhode Island Avenue has an average density of 0.92 FAR, and 34th Street has an overall density of 0.49 FAR. It should be noted that the Artscape Lofts building, at 3311 Rhode Island Avenue, has a density of 1.71 FAR.

### Building Heights

Building heights range from approximately 12–20 feet for one-story structures to approximately 40–50 feet for four-story structures.



## Transportation Analysis



Study area roadway network and Rhode Island Avenue (AADT = annual average daily traffic volume).

### Roadways

Rhode Island Avenue is classified as a collector road by the Prince George's County 2009 *Approved Countywide Master Plan of Transportation*. Within the M-U-TC area, Rhode Island Avenue has two marked 11-foot wide travel lanes and one wide outside travel lane with marked on-street parking in each direction. A raised median strip/turn lane, which is approximately 13 feet wide, divides the eastbound and westbound travel lanes.

The county classifies 34th Street as a local street. It is an important north-south street within the M-U-TC

area and connects Chillum Road (MD 501) to Queens Chapel Road. The 34th Street roadway consists of two lanes with on-street parallel parking in both directions.

An oval-shaped traffic circle, measuring approximately 80 feet by 65 feet, marks the intersection of Rhode Island Avenue and 34th Street. Although two speed limit signs are posted for 25 miles per hour (MPH) and 35 MPH along Rhode Island Avenue, motorists typically exceed this limit, even through the circle.

## Transit Connections

The Mount Rainier Town Center area still functions as a transit connection hub. Eight Metro stations and two MARC stations are located within five miles travel distance of the traffic circle at Rhode Island Avenue and 34th Street. Eight Metrobus lines and one county bus line connect the study area to the surrounding rail stations. The average weekday morning and evening peak period ridership in the Mount Rainier area is approximately 800 people.

## Parking Downtown

On-street metered parking is provided along most of Rhode Island Avenue and 34th Street inside the M-U-TC area. Various commercial uses and churches have off-street parking behind or adjacent to their buildings. Many of the off-street parking lots are

unpaved and underutilized. Businesses on upper 34th Street and along Rhode Island Avenue have expressed concern about a lack of adequate or easily accessible parking.

## Pedestrian Conditions

A number of the intersections within the M-U-TC area are difficult for pedestrians to traverse. Cars entering the traffic circle tend to speed up, rather than slow down and yield to pedestrians. The intersection of Rhode Island Avenue and Eastern Avenue lacks a pedestrian crossing on the Prince George’s County side. Some sidewalks along 34th Street are extremely narrow and are not wheelchair accessible due to their width and the lack of handicap ramps. Additionally, a number of curbs within the M-U-TC area are deteriorating. The entire area is dominated by vehicular traffic rather than pedestrians.

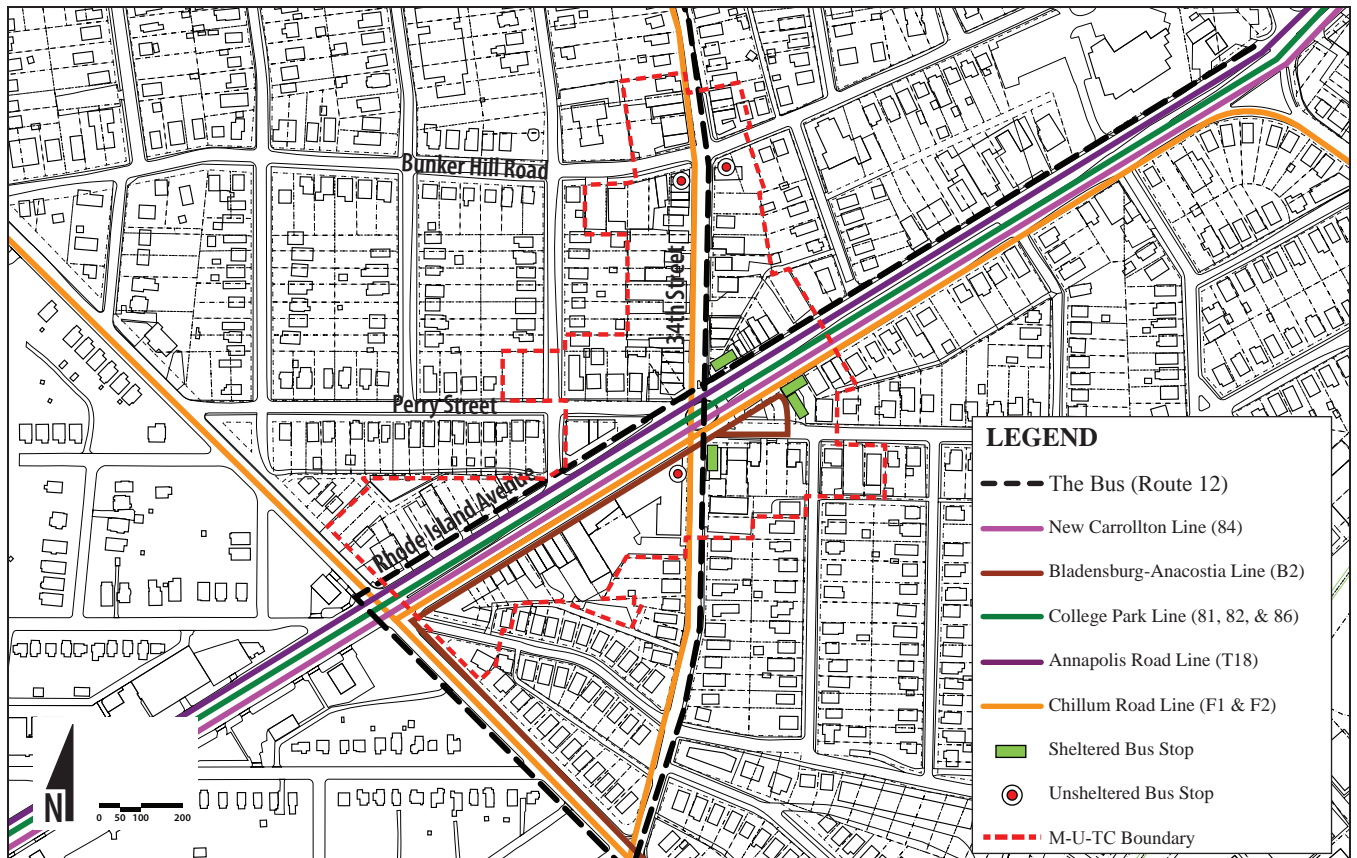


FIGURE 7: BUS LINES SERVING THE STUDY AREA.



# Market Analysis

## Primary Trade Area

The greatest demand for commercial use can be found in the immediate areas surrounding the M-U-TC boundary. This area, which is defined as the primary trade area (PTA), is a customized geographic area within a one-mile radius from the intersection of 34th Street and Rhode Island Avenue. The one-mile radius represents a 15- to 20-minute walk to downtown Mount Rainier. The specific boundary of this PTA is Michigan Avenue to Arundel Road on the north; open space and parks on the east; New York Avenue/US 50 on the south; and 13th Street, N.E., in the District of Columbia, on the west.

## Current Demographic Profile of the PTA

There are approximately 25,700 residents and 10,400 households within the PTA boundary, which equates to 2.4 residents per household. The home ownership rate is approximately 60 percent. Approximately 45 percent of the population owns one vehicle and 21 percent owns two vehicles. Twenty-four percent of the population does not own a vehicle.

Additionally, an estimated 23 percent of working age residents use public transportation to get to work. **Table 1** compares a number of local and regional demographic profiles with the demographic profiles of the PTA.

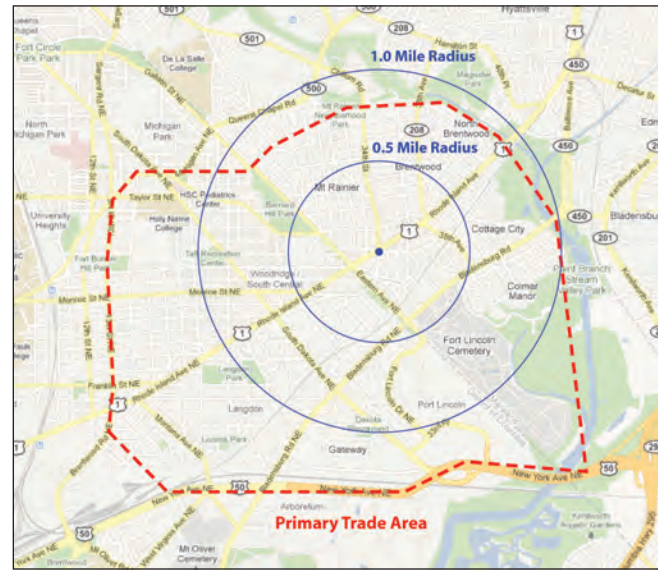


FIGURE 8: PRIMARY TRADE AREA.

TABLE 1: MOUNT RAINIER M-U-TC STUDY—2009 ESTIMATED DEMOGRAPHIC SUMMARY

	0.5 MILE RADIUS	CITY OF MOUNT RAINIER	PRIMARY TRADE AREA	PRINCE GEORGE'S COUNTY	DISTRICT OF COLUMBIA	WASHINGTON, D.C., MSA*
Population	5,400	8,400	25,700	827,400	591,700	5,400,000
<b>RACE</b>						
Black or African-American	56.3%	53.9%	72.5%	53.9%	53.5%	25.8%
White	17.9%	18.1%	12.7%	18.1%	35.9%	56.4%
Other <sup>1</sup>	25.8%	28.0%	14.8%	28.0%	10.6%	17.8%
Households	1,900	3,400	10,400	297,100	260,750	2,000,000
Average household size	2.8	2.4	2.4	2.7	2.1	2.6
Average age	38	35	41	35	38	37
Median household income	\$55,160	\$40,680	\$52,120	\$69,280	\$55,600	\$84,500
Average household income	\$70,220	\$49,070	\$66,670	\$81,690	\$86,200	\$106,500
Percentage homes owner occupied	64.7%	25.8%	60.1%	25.8%	40.3%	64.9%
Median home value	\$256,700	\$246,300	\$289,000	\$283,800	\$358,300	\$366,000
Percentage of family households	62.7%	52.8%	55.9%	69.1%	46.0%	62.5%
Percentage with high school degree or higher	67.5%	72.8%	73.1%	85.2%	77.8%	87.2%
Average number of vehicles per household	1.3	1.1	1.2	1.7	0.9	1.7

<sup>1</sup> The majority of "other" are Hispanic and Latino.

Source: US Census, Claritas, 10/09

\* Metropolitan Statistical Area

## Population and Household Growth Trends

Until 2005, the overall population and household growth within the PTA was relatively static. However, since that date, there has been an incremental population increase with aging households being replaced by younger families and new infill residential development and redevelopment. It is expected that in the long term there may be a 5–10 percent growth in population and number of households in the PTA (see Table 2).

The overall residential profile of the PTA is 72 percent African-American, 13 percent Caucasian, and 15 percent “other,” predominately made up of Hispanic people. The average age of residents is 41. However, 20 percent of the population consists of children under the age of 18. Persons 65 years or older make up 17 percent of the population. An estimated 73 percent of the population over the age of 25 have attained at least a high school degree.

The average income is approximately \$66,670, and the median household income is approximately \$52,100. The data for household income indicates that approximately 23 percent of the households within the PTA earn less than \$50,000 per year, and approximately one fifth of the households earn between \$50,000 and \$75,000 per

TABLE 2: POPULATION AND HOUSEHOLD GROWTH

TIMEFRAME	POPULATION	HOUSEHOLDS
1990–2000*	-5.4%	-2.4%
2000–2009*	-1.4%	0.3%
2009–2014 (projected)**	-0.0%	0.0%
2014+ (potential long term)***	5-10%	5-10%

SOURCE: \*U.S. Census, \*\*Claritas, \*\*\*Bolan Smart 10/09

year. On a comparative basis, within a half-mile radius of the intersection of 34th Street and Rhode Island Avenue, the population base is more diverse, with higher home-ownership rates and incomes than those found in the overall PTA.

## Employment Characteristics

Table 3 summarizes employment within a half-mile radius of the intersection of 34th Street and Rhode Island Avenue. While there is a large employment base at the periphery of the PTA, this job base has had minimal impact on the retail environment of downtown Mount Rainier.

## Housing Stock Characteristics

Most residential units within the PTA are roughly 60 years old and approximately 66 percent are single-family homes. Small multifamily buildings (between 3 and 19 units) make up approximately 19 percent of the housing stock. Another 11 percent of the residential units are large-scale multifamily buildings (50 plus units). Other housing types comprise the remaining three percent of housing stock within the PTA. A majority of the homes closer to downtown Mount Rainier are owner occupied single-family residences. The median home value within the PTA is estimated at \$289,000 compared to \$283,800 in greater Prince George’s County.

TABLE 3: HALF-MILE RADIUS EMPLOYMENT SECTOR (2009 ESTIMATE)

BUSINESS / LABOR SECTOR	FULL-TIME EMPLOYEES	PERCENTAGE OF TOTAL
Retail	454	22.9
Construction	241	12.1
Finance	220	11.1
Personal services	159	8.0
Government and education	151	7.6
Wholesale trade	139	7.0
Manufacturing	138	7.0
Social services	118	5.9
Business services	93	4.7
Transportation and utilities	93	4.7
Other	181	9.0
TOTAL	1,987	100.0

SOURCE: U.S. Census, Claritas, Bolan Smart 10/09



## Market Analysis: Existing Land Uses

### Overview

The buildings within the M-U-TC boundary total approximately 350,000 square feet. An estimated 25–30 percent of these buildings are vacant or under-utilized. Additionally, vacant lots and/or surface parking lots account for approximately two acres within the M-U-TC area.

### Commercial Uses

**Table 4** gives a breakdown of the commercial uses by category located within the M-U-TC. There is approximately 178,000 square feet of commercial space within the boundary of the M-U-TC, of which approximately 64,800 square feet—or 36 percent of the built commercial square footage—was vacant as of December 2009. Of the approximately 113,200 square feet of occupied commercial space within the area, consumer goods and personal care services comprise almost 57,000 square feet, or 50 percent of

that occupied space. Another ±34,600 square feet of commercial within the M-U-TC consists primarily of small office spaces, and ±20,160 square feet of commercial consists of cultural/arts related uses. A majority of the retail tenants are sole proprietors, and they are not representative of regional or national franchises/chains. By comparison, the commercial area surrounding the M-U-TC contains 1.5 million square feet of retail space comprised of numerous franchises/chains.

Even with the various retail businesses outside of the PTA, the Mount Rainier M-U-TC area offers opportunities for new commercial uses, such as a sit down restaurant and/or bank. **Table 5**, which is a commercial market summary, provides the strengths, weaknesses, and opportunities for future commercial uses within the M-U-TC area.

TABLE 4: EXISTING COMMERCIAL LAND USES BY CATEGORY

CATEGORY	NUMBER OF BUSINESSES	SQUARE FEET	COMMERCIAL USE PERCENTAGE
<b>RETAIL</b>			
Food and Drink	10	18,745	16.6
Beauty Salon	11	12,811	11.3
Grocery / Convenience / Variety	5	12,670	11.2
Flowers / Gifts / Boutiques / Thrift	6	9,670	8.5
Art / Galleries	1	1,700	1.5
Services / Laundry	1	1,220	1.1
<b>SUBTOTAL RETAIL:</b>	<b>34</b>	<b>56,816</b>	<b>50.2</b>
<b>OTHER COMMERCIAL / OFFICE</b>	<b>14</b>	<b>34,589</b>	<b>30.6</b>
<b>CULTURAL &amp; ARTS</b>	<b>2</b>	<b>21,778</b>	<b>19.2</b>
<b>Totals:</b>	<b>50</b>	<b>113,183</b>	<b>100.0</b>

SOURCE: Market Data, Mount Rainier Business Association and Bolan Smart, 10/09.

TABLE 5: COMMERCIAL MARKET SUMMARY

STRENGTHS	WEAKNESSES	OPPORTUNITIES
<ul style="list-style-type: none"> <li>• Two distinct commercial streets.</li> <li>• Historic character.</li> <li>• Regional demand draw for patrons of Joe’s Movement Emporium and Glut Food Co-op.</li> <li>• Pedestrian scale/neighborhood serving.</li> <li>• Arts/environment/health niche.</li> <li>• Parking generally available.</li> <li>• Relatively low rents.</li> </ul>	<ul style="list-style-type: none"> <li>• Building storefronts/appearances.</li> <li>• Perceived crime and safety issues.</li> <li>• Vacant buildings, some obsolete space, and institutional street level uses.</li> <li>• Rhode Island Avenue pedestrian access and safety.</li> <li>• No cohesive streetscape or signage.</li> <li>• Parking is not always available directly in front of businesses.</li> <li>• Redundancy of uses.</li> <li>• Inconsistent hours of operation.</li> <li>• Proximate competitive retail.</li> </ul>	<ul style="list-style-type: none"> <li>• Diversification of retail offerings.</li> <li>• Household and income growth.</li> <li>• Improve visual appearances (i.e., signage, buildings, etc.).</li> <li>• More pedestrian-friendly streetscapes and amenities.</li> <li>• Assistance/incentives for future investors.</li> </ul>

### Civic and Institutional Uses

Public related land uses comprise ±36,100 square feet, or approximately 10 percent of the existing total estimated 350,000 square feet in the M-U-TC area.

### Residential Uses

Residential land uses comprise ±135,600 square feet, or 39 percent of the total square footage within the M-U TC area. Approximately 75 percent of this residential square footage is distributed between the Mount Rainier Artist Lofts at 3311 Rhode Island Avenue, the Newton Square Apartment at 3201 Rhode Island Avenue, and HIPP Artist Housing at 3801 33rd Street. The remainder of the residential units within the M-U-TC are smaller one- to four-unit apartment buildings scattered throughout the area.

The residential neighborhoods immediately surrounding the M-U-TC, within the PTA boundary, are

predominantly low density single-family bungalow style homes. Moving outside of the PTA, there are a number of rental apartment complexes, including Queenstown Apartments, Queens Manor Garden, and Kaywood Gardens Apartments.

New residential development and redevelopment within and just outside of the M-U-TC represents a key opportunity with many benefits. While financing is a challenge, efforts need to be directed towards capturing new market-rate ownership housing.







Plan Vision **3**



## Plan Principles

The plan vision incorporates Mount Rainier’s existing positive attributes and potential opportunities to create a revitalized mixed-use town center. The M-U-TC’s physical character, land uses, transportation connections, and cultural assets inform the plan vision to transform the area into a walkable, vibrant, and attractive locale for residents and visitors. An analysis of the area considered the streetscapes, the visual composition of buildings, and the unique elements that define Mount Rainier. Three distinct districts were identified within the M-U-TC: 34th Street, Rhode Island Avenue, and the Civic Core/City Center. Each district has a distinct role within the plan vision.

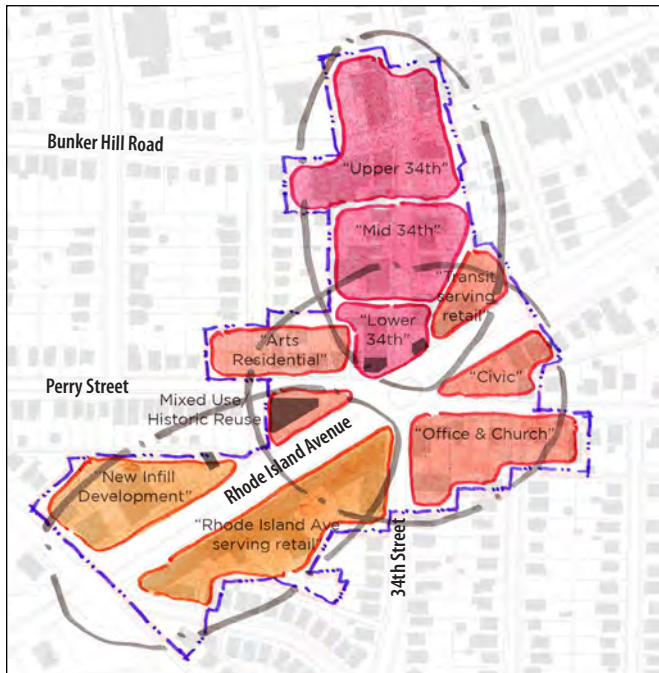


FIGURE 9: DIAGRAM OF THE INITIAL DISTRICTS CONCEPT.

### 34th Street: The “Neighborhood Main Street”

This district consists of parcels with primary frontage on 34th Street north of the Rhode Island Avenue/34th Street traffic circle. The area, which includes local retail businesses and cultural venues, is defined by one- to three-story structures placed close to the street. This creates an intimate streetscape. This local main street will be the primary focus area for façade improvements and

the preferred location for local neighborhood-serving retail tenants. The intersection of 34th Street and Bunker Hill Road is the cultural center for community activity. Existing retail, new businesses, and cultural festivities will enhance the street. The intersection of 34th Street and Bunker Hill Road will be made a more inviting space for pedestrian activity with the introduction of special paving in the street right-of-way. A medium-scale single row of trees will align each side of the street and provide shade to pedestrians.

### Rhode Island Avenue: The “Boulevard/Gateway”

This district, which consists of parcels with frontage on Rhode Island Avenue, is the main transportation corridor through the M-U-TC area from points east and west. The avenue will be a wide tree lined boulevard aligned with primarily four- to five-story buildings. It will be the preferred location for new development and destination retail with a regional attraction. The pedestrian experience will be enhanced by wide sidewalks with special paving patterns, outdoor café seating, and a double row of large street trees extending the full length of the avenue within the M-U-TC area on the north side. Rhode Island Avenue will also feature new dedicated bicycle lanes that will serve as another mode of transportation. A landmark building at the northeast corner of Eastern and Rhode Island Avenues on the city-owned redevelopment parcel, coupled with the building across Rhode Island Avenue, will create a formal gateway into Mount Rainier from the west.

### The Civic Core: The “City Center”

This district is composed of parcels with primary frontage adjacent to the traffic circle at 34th Street and Rhode Island Avenue. It includes retail and institutional uses, such as City Hall, the public library, and a transportation hub. This area, which is the symbolic center of the M-U-TC, will feature one- to three-story buildings, a transit hub, and new gathering space in front of City Hall. A new landmark sculpture/monument will anchor the center of the traffic circle.

# Urban Design Plan

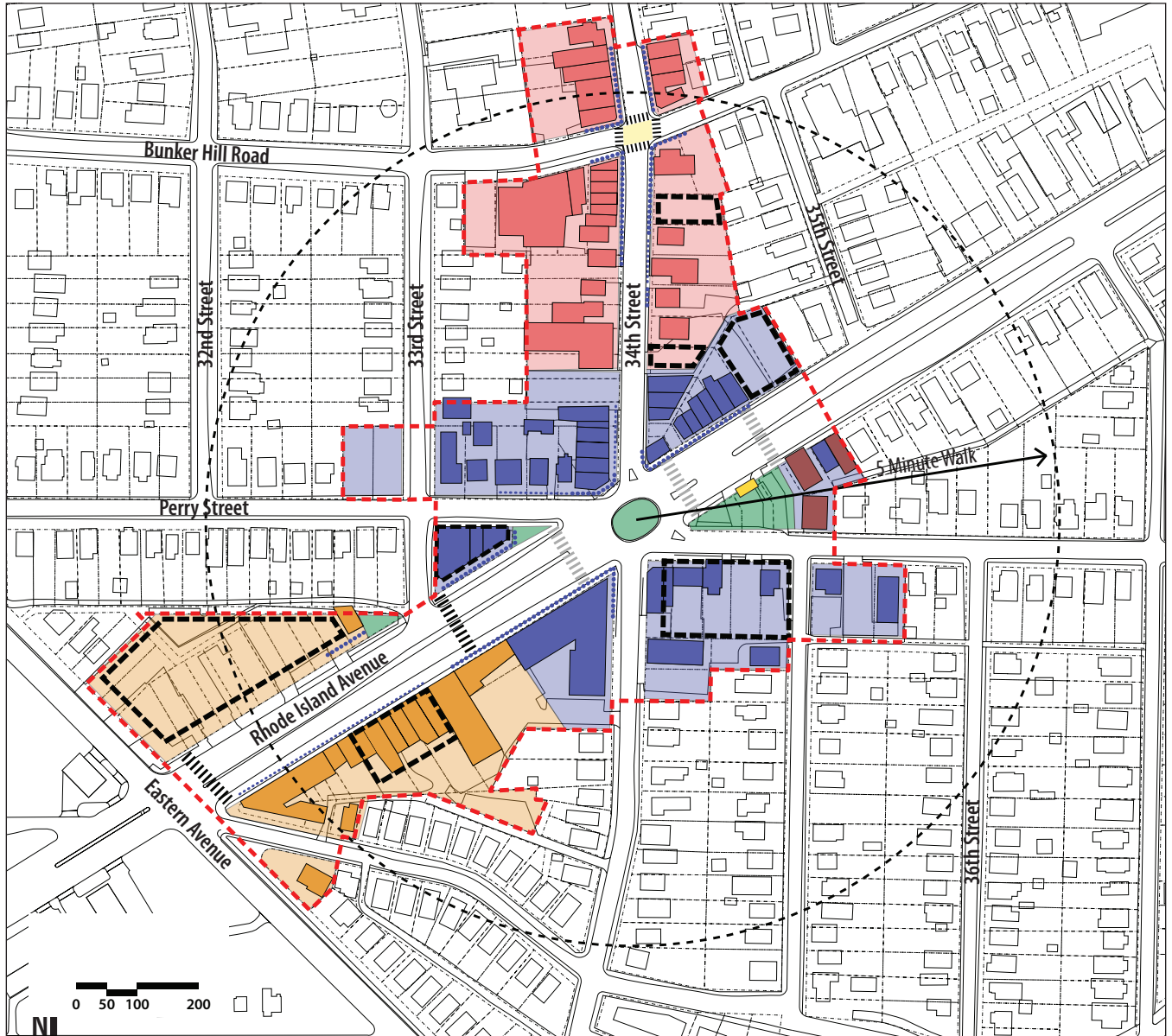


FIGURE 10: THE URBAN DESIGN PLAN.

The urban design plan describes the framework for interrelated plan elements such as the three districts, potential land-use, retail focus, pedestrian improvements, bus hub, significant civic buildings, and infill opportunities. The urban design plan provides detailed boundaries for district areas, retail storefront focus zones, open space, major development opportunities, and infrastructure improvements within the M-U-TC area.

## LEGEND

- District—Rhode Island Avenue
- District—Upper 34th Street
- District—Civic Core/City Center
- Open space
- New infill/major renovation opportunity sites
- Parcel
- Civic buildings
- Retail storefront focus
- Commercial/office optional focus
- Existing crosswalk
- New crosswalk
- Five-minute walk
- Bus transit stop
- M-U-TC Boundary



# Illustrative Plan



FIGURE 11: ILLUSTRATIVE PLAN FOR THE M-U-TC AREA.

The illustrative plan for the M-U-TC area depicts key design recommendations for specific locations, such as 3200–3220 Rhode Island Avenue, Upper 34th Street, and 3300–3310 Rhode Island Avenue. The plan represents future development patterns based on the plan vision framework described in this chapter. The illustrative plan is not an exact depiction of future development, rather, it represents one of many possible future development scenarios.

The core components of this illustrative plan, in terms of new infill, minor renovations, and major renovations, reflect the recommended directions future development and redevelopment should take as the development plan is implemented.

## District Recommendations

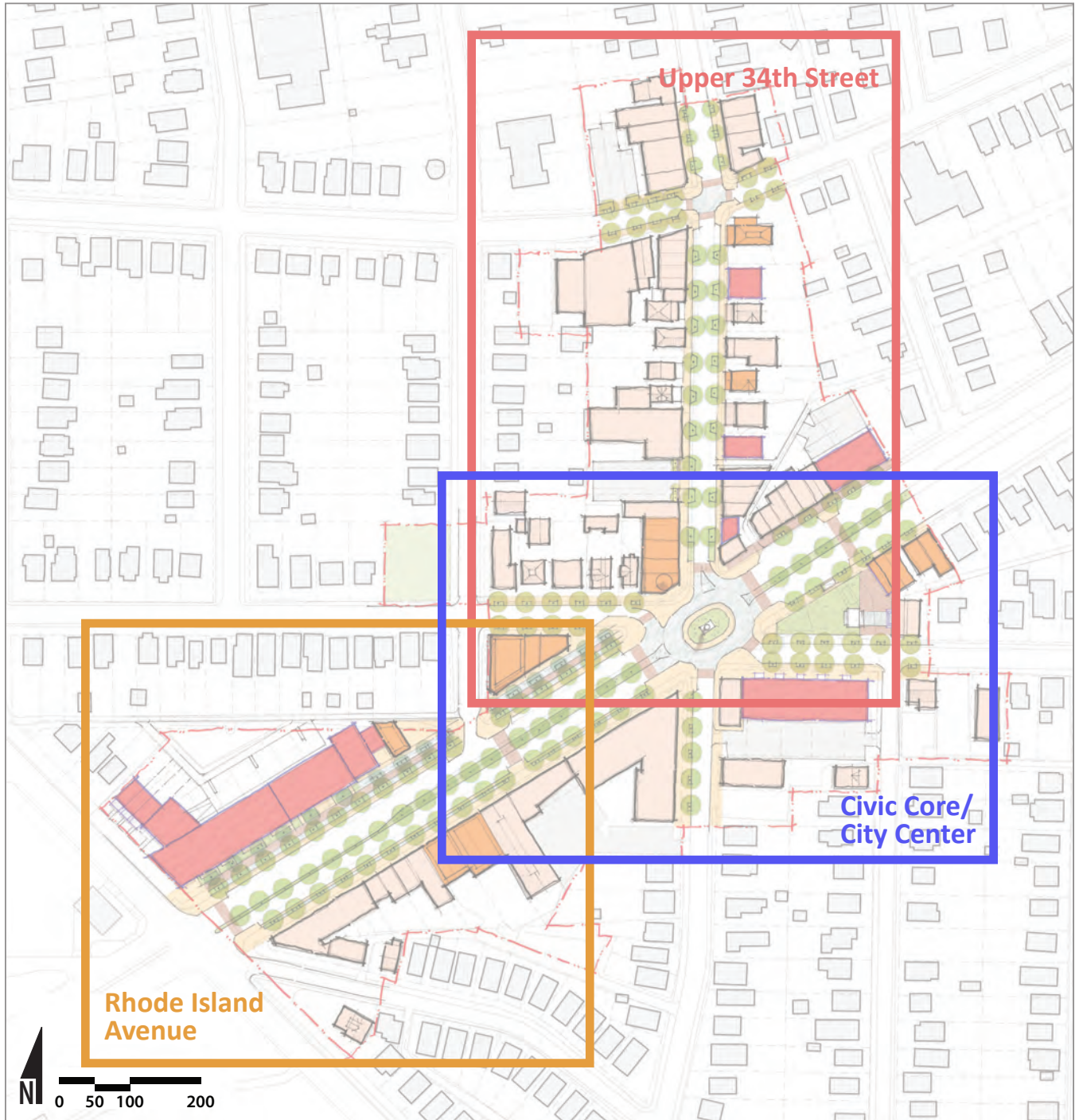


FIGURE 12: RECOMMENDATIONS FOR THE THREE DISTRICTS.

This section details specific design and plan recommendations for the following three districts:

- Upper 34th Street
- Rhode Island Avenue
- Civic Core/City Center



## Upper 34th Street: The Neighborhood-Serving Main Street



EXISTING CONDITIONS: 34TH STREET, LOOKING SOUTH TOWARD THE LIGHTBOWN BUILDING (SE CORNER).

34th Street is a revitalized, neighborhood-serving main street with the following:

- **Neighborhood-serving retail:** Small-scaled, two- and three-story retail buildings serve the surrounding community.
- **Retail storefronts:** New pedestrian-scaled signage, attractive and simple fenestration, colorful facades, awnings and porches, and outdoor seating create vibrant retail storefronts.
- **On-street parking:** Parallel parking slows down and protects pedestrians from traffic.
- **Parking lots:** Shared-use parking lots serve multiple users.
- **Historic character:** Façades of most buildings are restored to their historic character.
- **Mix of retail:** Various types of retail, including restaurants, are evident all along the street.
- **Mixed-use:** Residential uses are found on the second and third floors above retail.
- **Bicycle route:** Shared-lane markings and bikeway signage identify 34th Street as a bicycle route.



A MAIN STREET IN THE DEL REY COMMUNITY, ALEXANDRIA, VA.



MAIN STREET—OUTDOOR SEATING AND COLORFUL RETAIL STOREFRONTS.

## Upper 34th Street: Existing and Future Street Sections

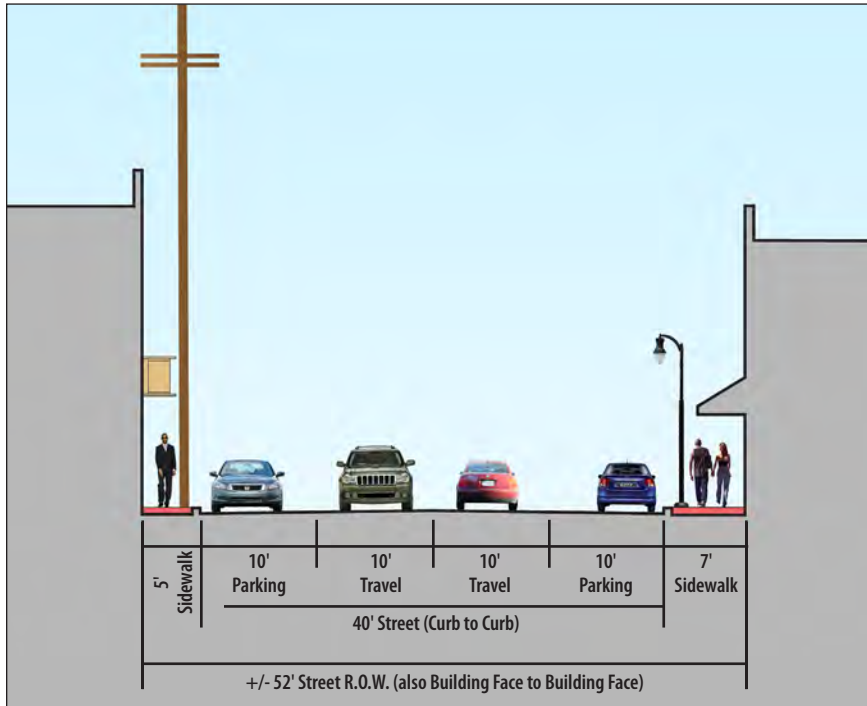


FIGURE 13: EXISTING CONDITIONS—34TH STREET, LOOKING SOUTH.

In order to create pedestrian precincts along both sides of 34th Street, the current location of curb lines and sidewalk profiles will be changed through the implementation of the proposed design standards. The proposed street section for 34th Street between the circle and its intersection with Bunker Hill Road will maintain the existing travel lane width of ten feet in each direction and will extend both curb lines two feet into the parking lane in order to provide wider sidewalks. Additionally, street trees will be planted on islands between parking spaces to complement the existing streetscape. Shared lane markings and bikeway signage will designate the bicycle lanes.

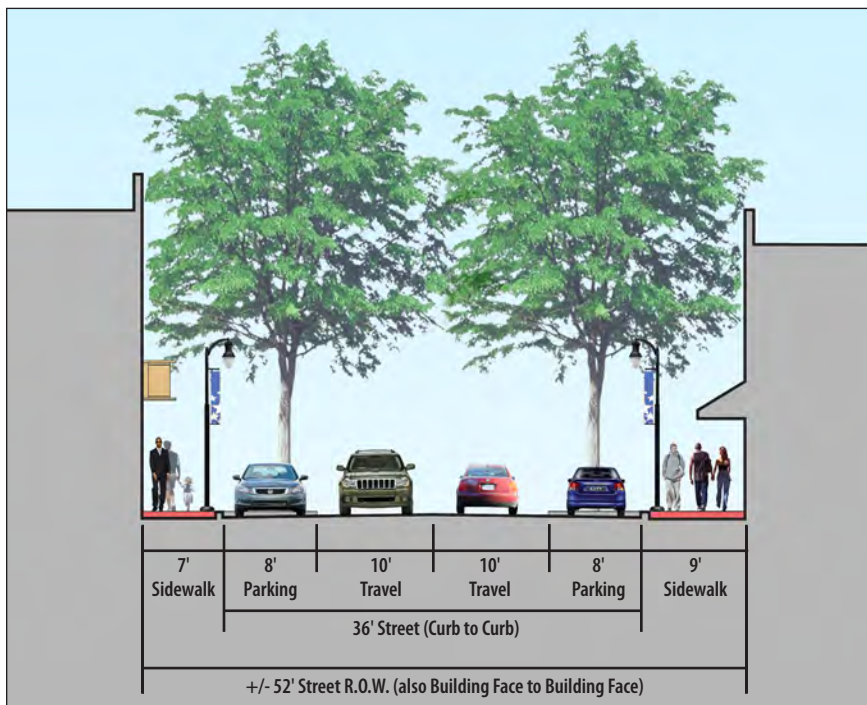
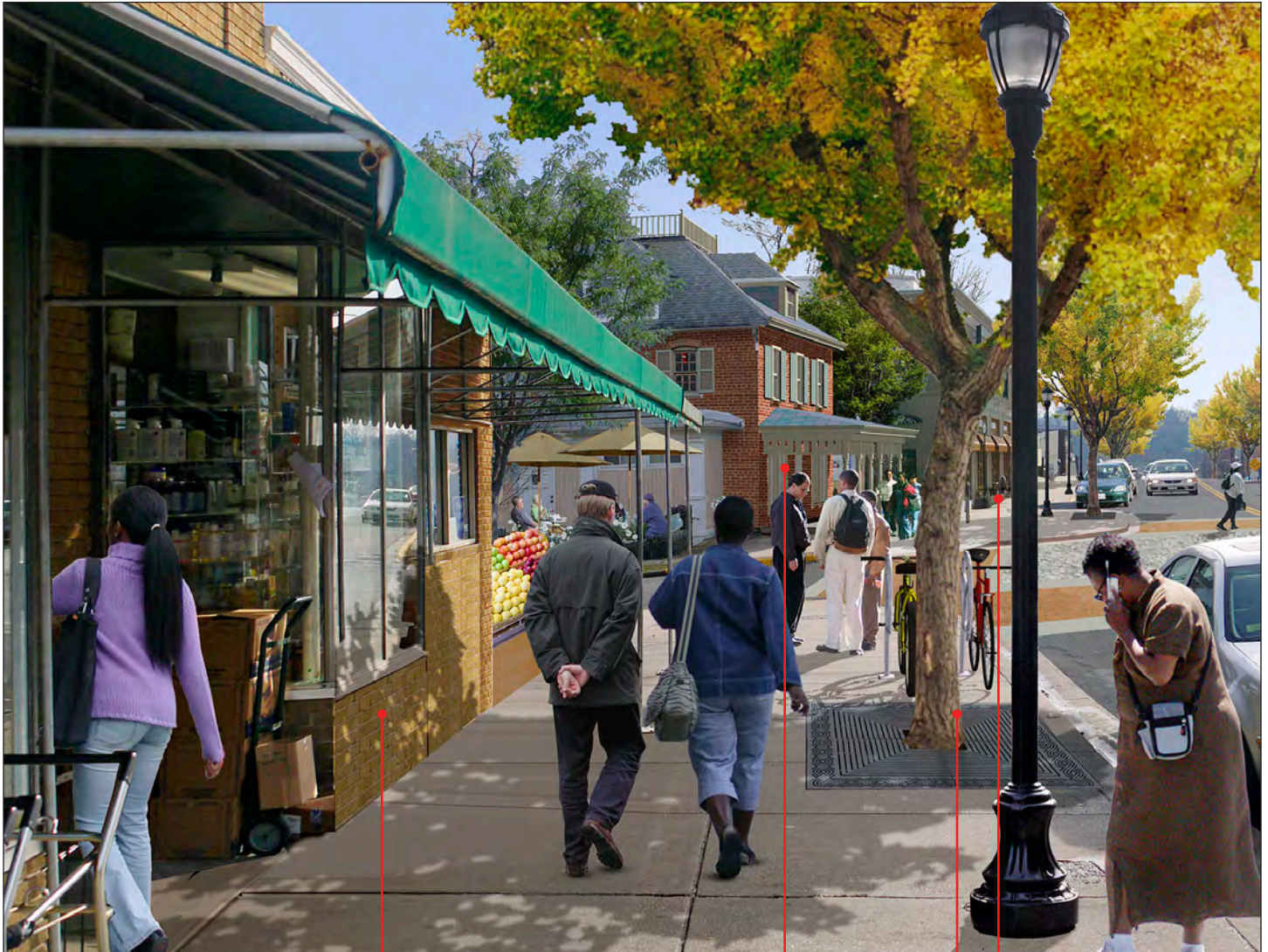


FIGURE 14: PROPOSED CONDITIONS—34TH STREET SOUTH OF BUNKER HILL ROAD, LOOKING SOUTH.



## Upper 34th Street: Future Vision



Refurbish the storefront for 4005 34th Street. Set up vegetable/fruit display on sidewalk.

Renovate 3401 Bunker Hill to original façade for new retail or restaurant.

Build mixed-use infill building with ground-floor retail space.

Introduce new street trees in grates and new bike racks.



KLEIN'S BAKERY, CIRCA 1905.





Introduce new crosswalks, curb extensions, and special paving at the intersection of 34th Street and Bunker Hill Road.

Expand pedestrian sidewalks with street trees located on both sides of the street.

Refurbish façade for new ground-floor retail tenant/restaurant and café seating.

Introduce new retail signage.

Restore retail storefronts to its original character.

Introduce new pay-on-foot parking meters.

Introduce shared lane for bicycle traffic.



## Upper 34th Street: Recommendations



1. Add new pedestrian crosswalks with special paving at the intersection of 34th Street and Bunker Hill Road.

2. Add tree boxes in curb extensions within the parking lane from the traffic circle to Bunker Hill Road. Place trees in tree boxes within the sidewalk area north of Bunker Hill Road.

3. Renovate existing parking lots to include landscaping, permeable paving, and low walls along the street frontage.

4. Create areas for a “low-impact development” stormwater drainage rain garden at the lower end of 34th Street, north of the Rhode Island Avenue traffic circle.

5. Renovate existing buildings such as Klein’s Bakery (see historic photo on page 30).

6. Create places for vibrant street activity by using the existing building setback areas.

7. Create places for historically-sensitive front-porch additions to existing buildings up to the building setback.

8. Build new mixed-use infill buildings with ground floor retail space.

9. Move overhead utility lines to an underground location.

FIGURE 15: RECOMMENDATIONS FOR UPPER 34TH STREET.

## Rhode Island Avenue: The Tree-Lined Boulevard



EXISTING CONDITIONS: RHODE ISLAND AVENUE, LOOKING WEST TOWARDS THE SINGER BUILDING.

The plan vision imagines Rhode Island Avenue as a vibrant, grand boulevard that will identify and reflect the Mount Rainier community by employing the following elements:

- **Wide, multilane thoroughfare:** Design a pedestrian-friendly streetscape that will encourage slower auto speeds and yet still allow for adequate traffic flow.
- **Landscaped median:** Enhance an existing feature that will be unique to the Mount Rainier section of Rhode Island Avenue.
- **Pedestrian sidewalks:** Widen sidewalks to create places for social interaction and outdoor retail seating. Add special paving designs and patterns to give the boulevard a unique *“sense of place.”*
- **Larger, mixed-use buildings:** Renovate existing buildings and design infill buildings to meet the street with proper orientation and land use. Create spaces to support retail/restaurants with outdoor seating.
- **Urban furniture:** Use functional, durable street lamps, benches, bike racks, chairs, and tables that will enhance and promote the pedestrian experience.



BOULEVARD: MIXED-USE/PUBLIC SPACE CONDITION.

- **Street trees:** Plant trees and other landscaping elements to provide a sense of enclosure, protect pedestrians and cyclists, and help slow down traffic.
- **Bicycle paths:** Add a cycle track and widen sidewalks to accommodate for bicycle enthusiasts, commuters and casual cyclists.



## Rhode Island Avenue: Existing and Future Street Sections

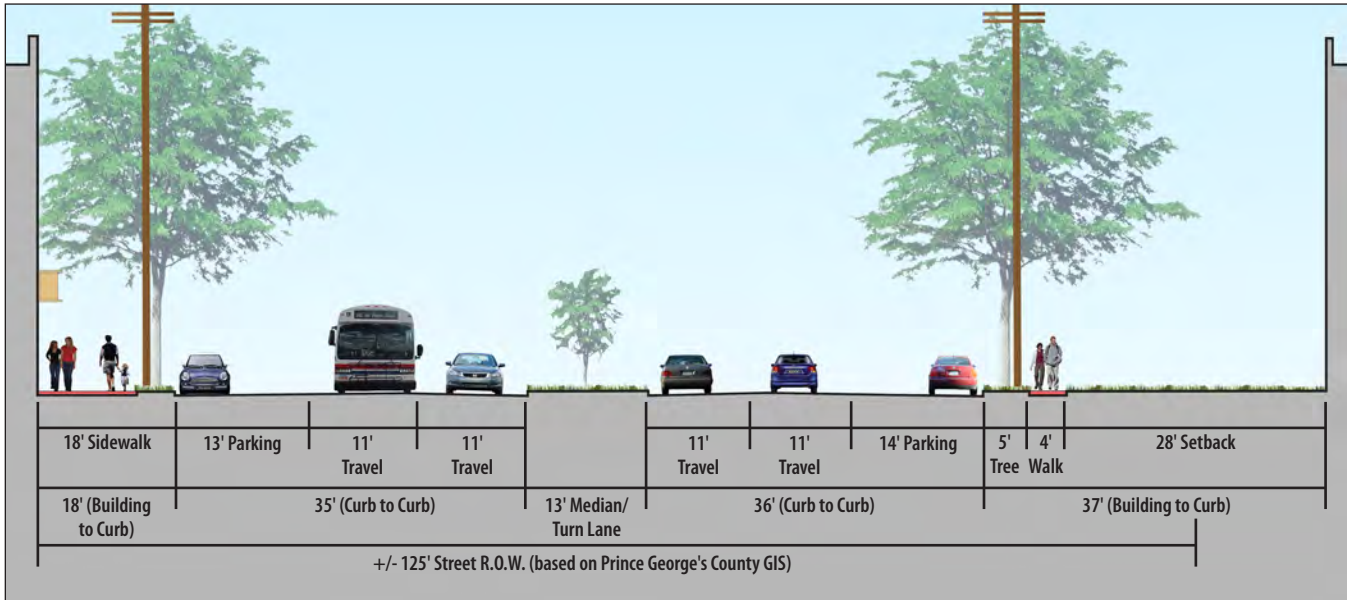


FIGURE 16: EXISTING STREET SECTION: RHODE ISLAND AVENUE, WEST OF TRAFFIC CIRCLE, LOOKING WEST.

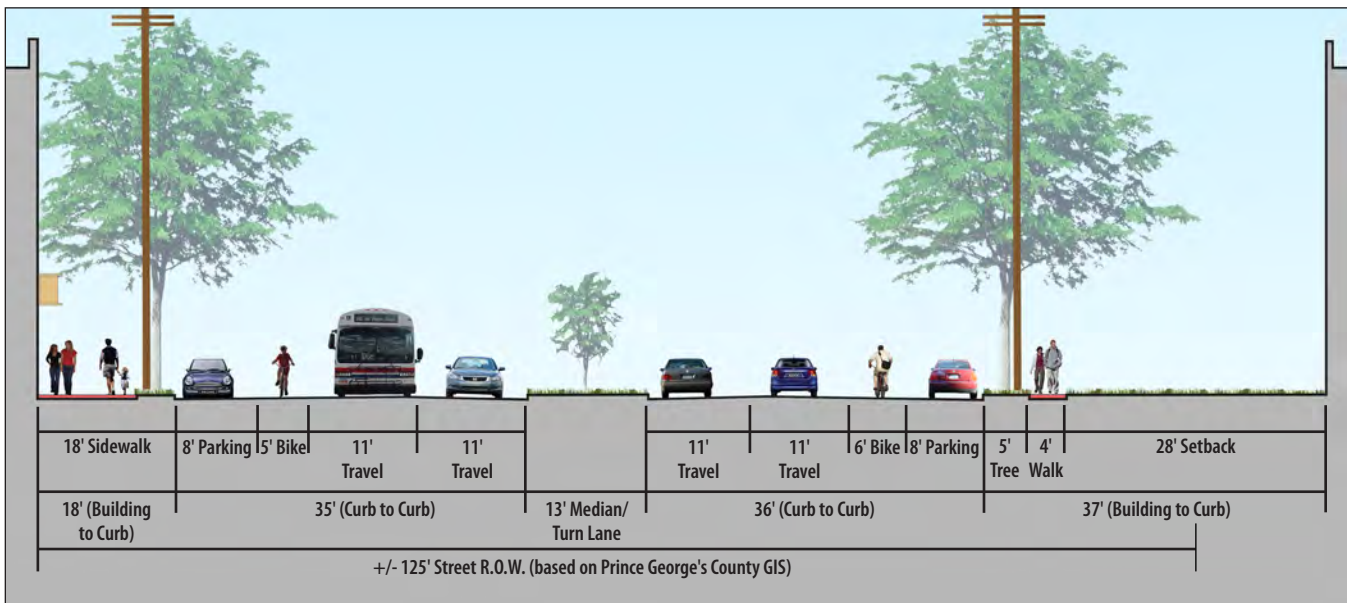


FIGURE 17: PROPOSED SHORT-TERM STREET SECTION—RHODE ISLAND AVENUE, WEST OF TRAFFIC CIRCLE, LOOKING WEST.

In order to create pedestrian precincts along both sides of Rhode Island Avenue, the current location of curb lines and sidewalk profiles will be changed through the implementation of the proposed design standards.

The proposed street configuration for Rhode Island Avenue maintains the State Highway Administration (SHA) designed 11-foot travel lanes (two in each direction, four in total) and the existing 13-foot median.

The north curb line of Rhode Island Avenue will be extended by six feet, and the south curb line by three feet to allow for two 11-foot travel lanes and one eight-foot parking lane in each direction. A new 6-foot wide cycle track, which is an exclusive right-of-way for bicycles, will be located along the new curb lines.

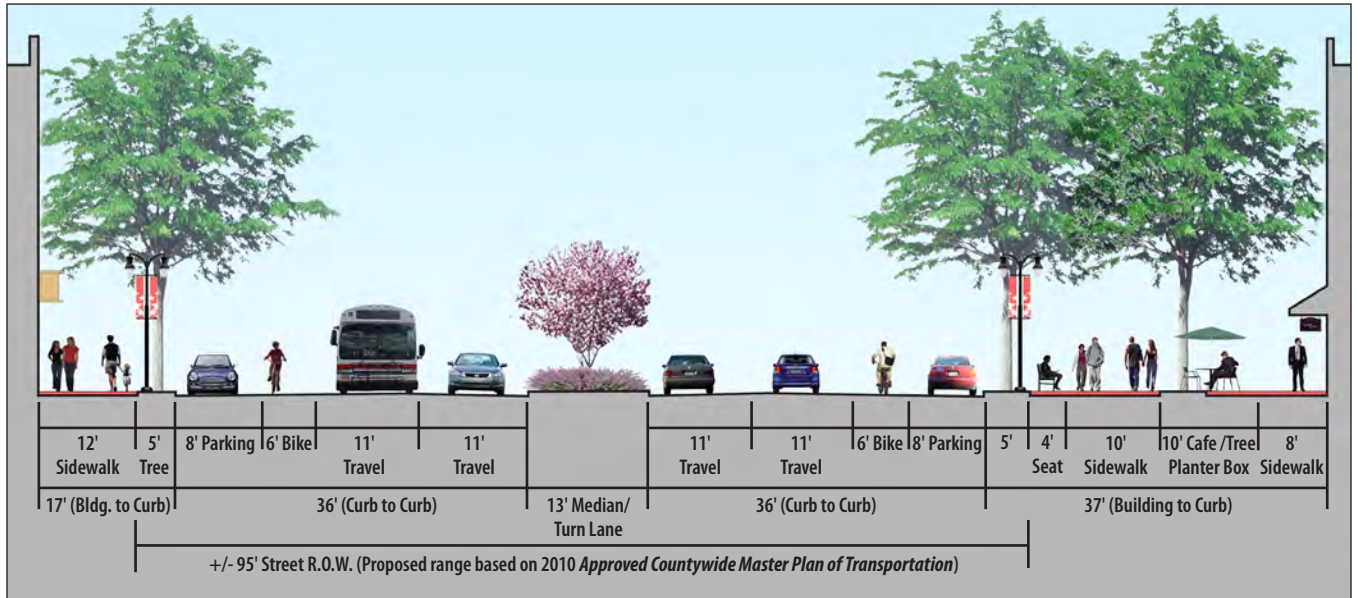


FIGURE 18: PROPOSED MID-TERM STREET SECTION—RHODE ISLAND AVENUE, WEST OF TRAFFIC CIRCLE, LOOKING WEST.

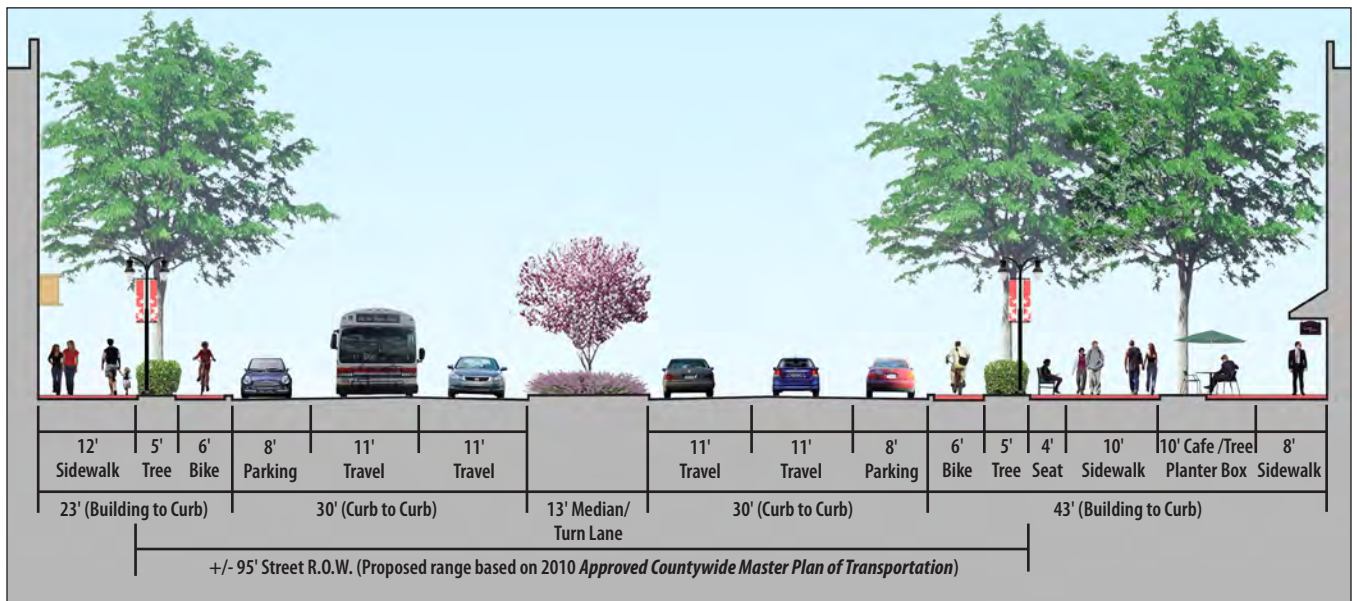
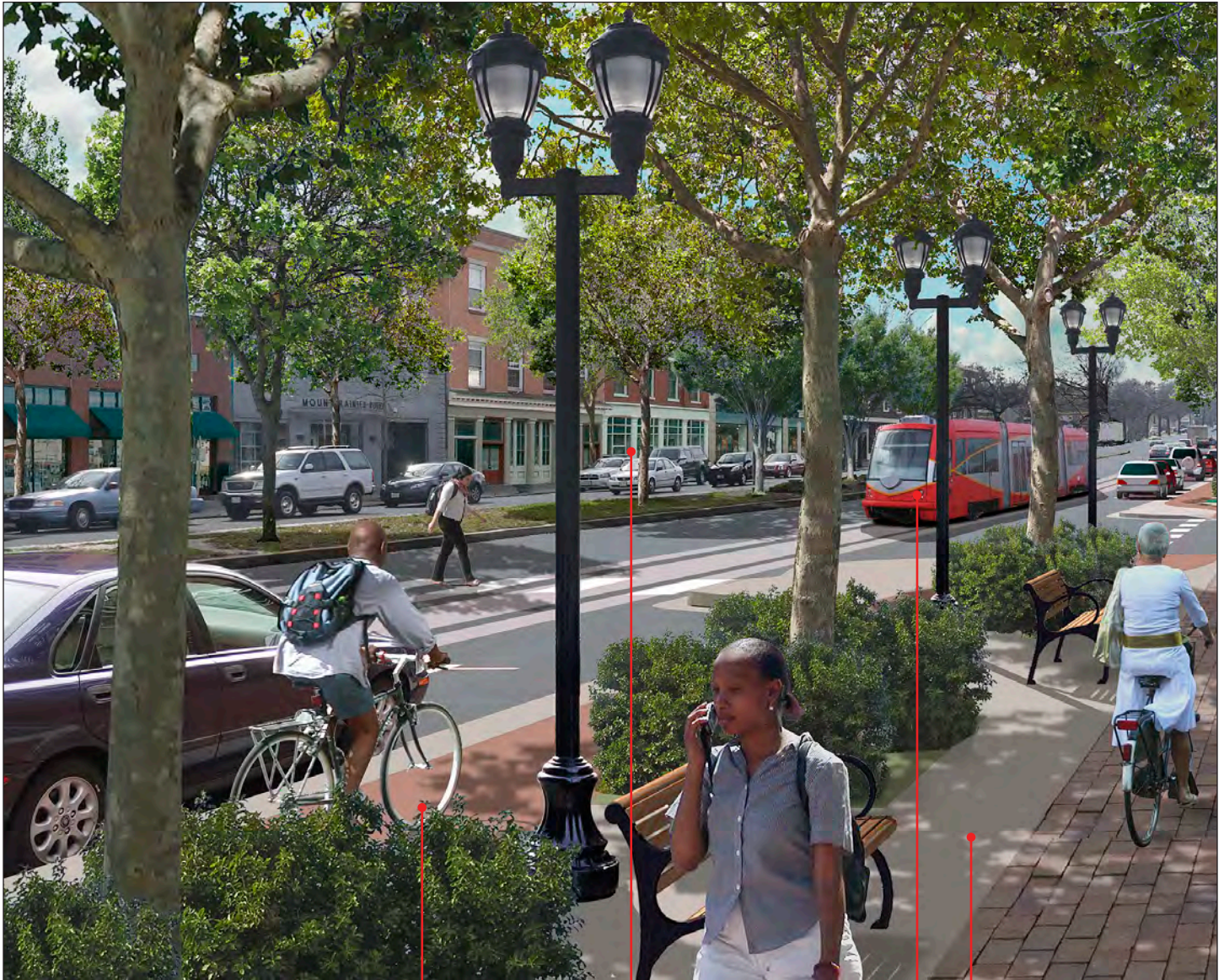


FIGURE 19: PROPOSED LONG-TERM STREET SECTION—RHODE ISLAND AVENUE, WEST OF TRAFFIC CIRCLE, LOOKING WEST.

Additionally, new crosswalks with special textured features—such as stamped concrete—that emphasize the pedestrian crossings, will be installed on Rhode Island Avenue at Eastern Avenue and 33rd Street.



## Rhode Island Avenue: Future Vision



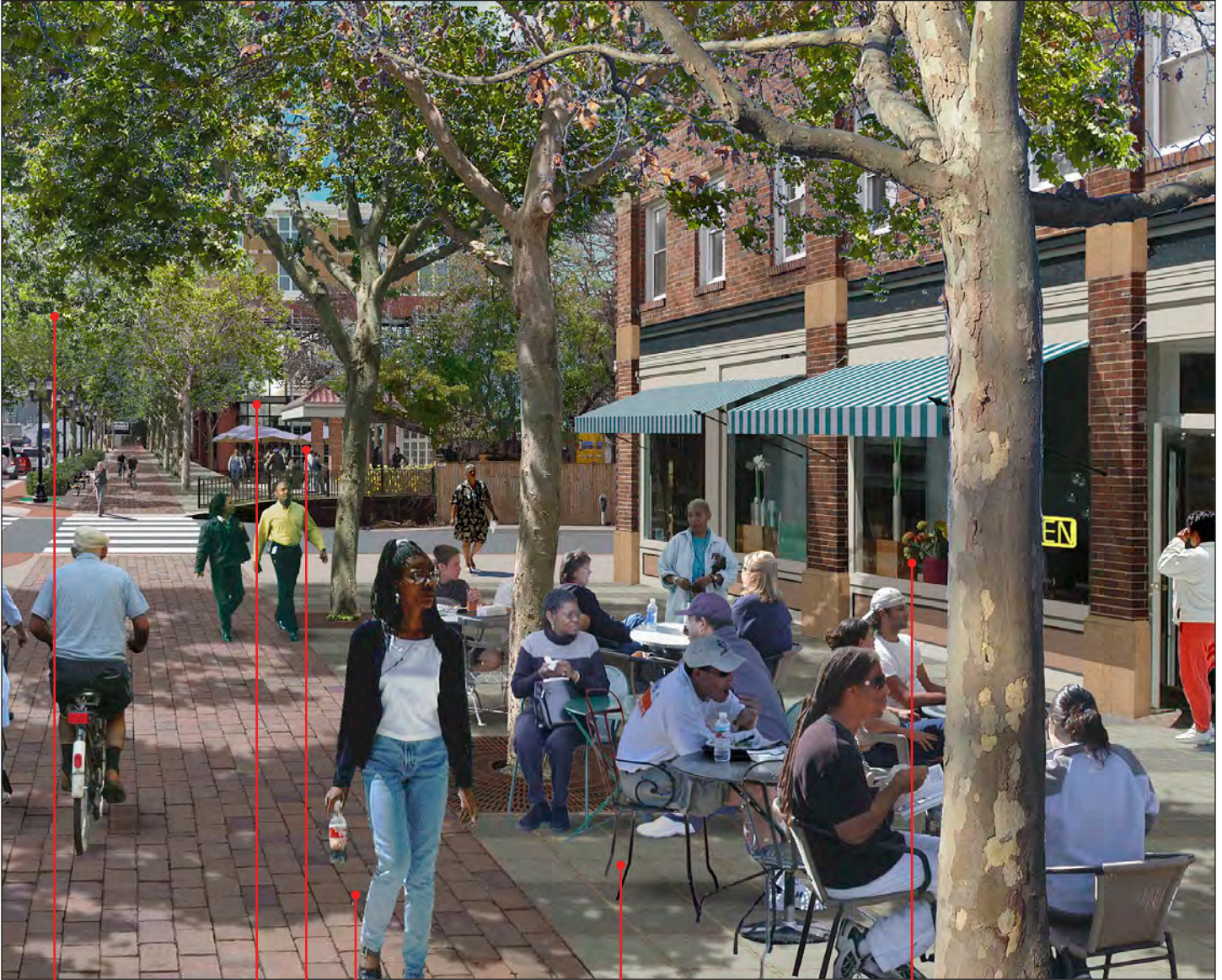
Introduce new cycle track zone.

Refurbish building façades.

Install street trees and pedestrian seating in the streetscape zone.

Consider incorporating design elements into the newly redesigned boulevard that will support the potential DC street car line.





Install a double row of street trees.

Install new primary sidewalk streetscape zone.

Create an area, designed with special paving, for café seating in the streetscape zone.

Refurbish 3300 Rhode Island Avenue with new ground-floor retail tenants.

Construct multipurpose community flex space.

Renovate 3220 Rhode Island Avenue and convert to retail use with outdoor seating.



## Rhode Island Avenue: North Infill Opportunity Site



FIGURE 20: SCHEMATIC SITE PLAN.

The combined 3200–3220 Rhode Island Avenue sites will be redeveloped into a signature, mixed-use building and will serve as the northern part of the gateway into Mount Rainier. A four- to five-story infill residential building will face Rhode Island Avenue. Large townhouses will face Eastern Avenue. The former gas station site, 3220 Rhode Island Avenue, will be adaptively reused as small-scale café/retail space with a landscaped public plaza, and integrated as part of a new community flex space. This two- to three-story community flex space will measure approximately 75 feet by 40 feet and will serve multiple community functions. Office and commercial space will be located above the flexible space.

This new mixed-use building will be designed in a manner that is sensitive to the existing surrounding residential fabric. Its highest point will be located at the intersection of Rhode Island and Eastern Avenues. The building will step down away from this intersection in order to acknowledge the scale of—and serve as a transition to—the adjacent existing single-family neighborhood context. This building configuration will reduce the impact of shadows on the surrounding residential neighborhood.

A solar study was conducted to show how shadows might be cast from the new mixed-use building at two hour increments during the day. This study was conducted in order to assess the impact of the redevelopment on the adjacent residential neighborhood.



FIGURE 21: BUILDING SOLAR STUDY: MAY 21ST.

## Rhode Island Avenue: Recommendations



FIGURE 22: RECOMMENDATIONS FOR RHODE ISLAND AVENUE.

1. Include areas for café seating and bike lanes along the newly designed Rhode Island Avenue streetscape.
2. Create a mid-block pedestrian crossing.
3. Renovate existing buildings with the addition of rooftop floors.
4. Renovate existing buildings.
5. Construct a small-scale community flex space.
6. Build infill multifamily residential buildings at the gateway to the M-U-TC area.
7. Build townhouses adjacent to existing single-family homes.
8. Move overhead utility lines to underground locations.
9. Provide AASHTO designated bike lanes along Rhode Island in the short- and mid-term by using extra space currently allocated for on-street parking.



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## Civic Core: The City Center



EXISTING CONDITIONS: RHODE ISLAND AVENUE, LOOKING EAST TOWARDS CITY HALL.

Mount Rainier’s civic buildings serve as a focal point and anchor to the community. The Mount Rainier City Hall, library, Eastern Star Building, and police station reinforce downtown as being a special place through the following proposed improvements:

- **City Hall:** Construct a new community presentation platform and staircase.
- **Civic green and plaza:** Transform the bus turnaround into a green civic space.
- **Eastern Star Building:** Restore the historic exterior, renovate the interior with a grand hall, exploit the potential for a part-time gallery/arts space, create a new façade facing the green.
- **Mount Rainier Library:** Continue to develop designs for a new/renovated library.
- **Transit Center:** Create a modern transit structure with opportunity for collaboration with artists.



PROPOSED CIVIC GREEN AT BUS TURNAROUND.



TRAFFIC CIRCLE, CIVIC GREEN AND MUNICIPAL CENTER.



## Civic Core/City Center: Future Vision



Renovate the Mount Rainier Library.

Create a modern transit structure.

Renovate 3405 Rhode Island Avenue, a historic building, with a modern addition facing the civic green.

Construct an elevated landscape terrace/performance space.

Install special paving for pedestrian crosswalk.





Construct public gathering space in the new civic green.

Restore landscaping and urban seating area.

Construct infill development along Perry Street East (as a temporary use, add parking on open lots).

Install special paving around traffic circle.



## Civic Core/City Center Recommendations

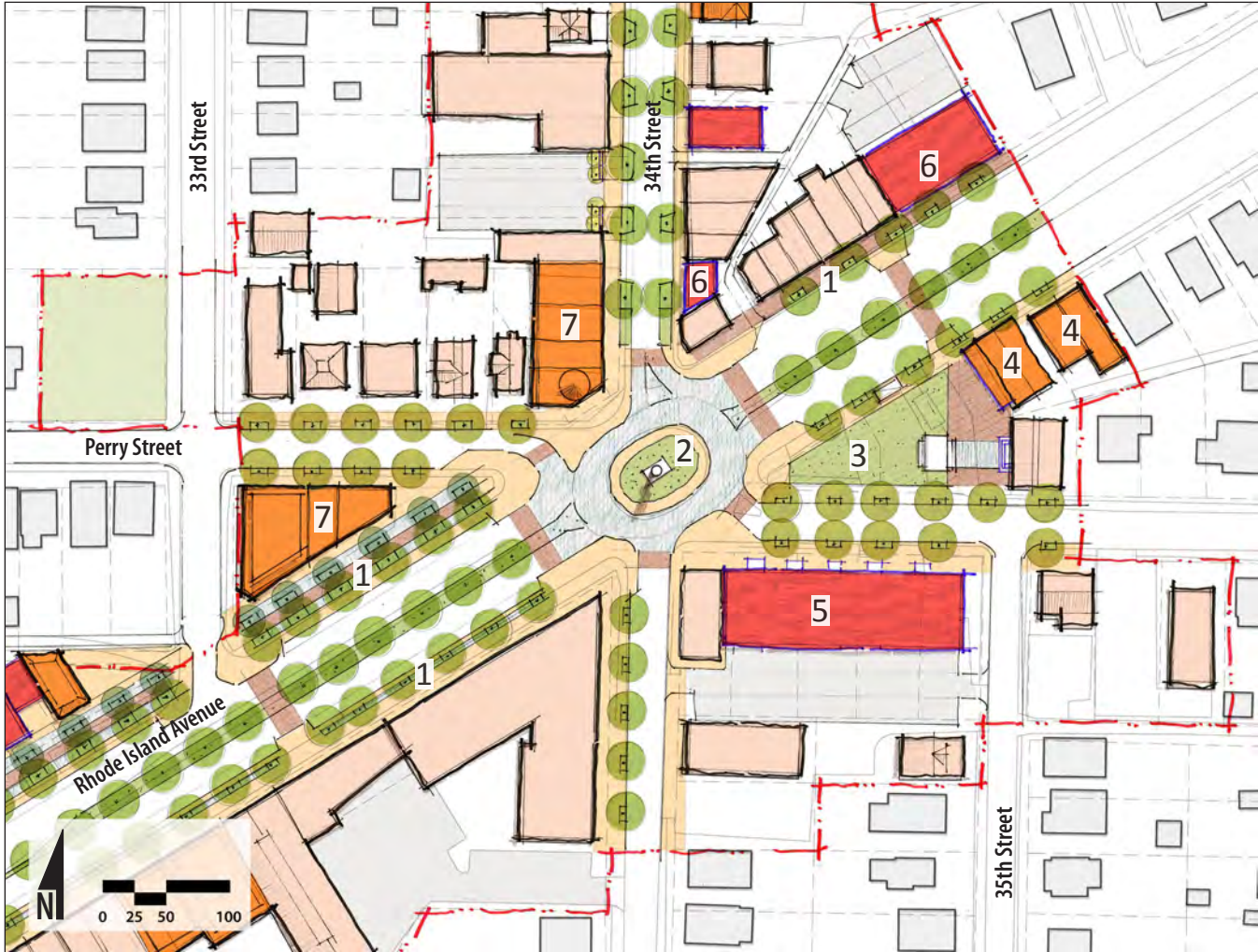


FIGURE 23: RECOMMENDATIONS FOR THE CIVIC CORE/CITY CENTER.

1. Include areas for café seating and bike lanes along the new Rhode Island Avenue streetscape.
2. Create special paving and a new landmark monument at the Rhode Island Avenue circle.
3. Create a new civic green and plaza adjacent to the City Hall.
4. Renovate the municipal buildings.
5. Build infill multifamily residential or commercial office buildings.
6. Build mixed-use infill buildings.
7. Renovate major interior and exterior architectural elements.

## Land Use Recommendations

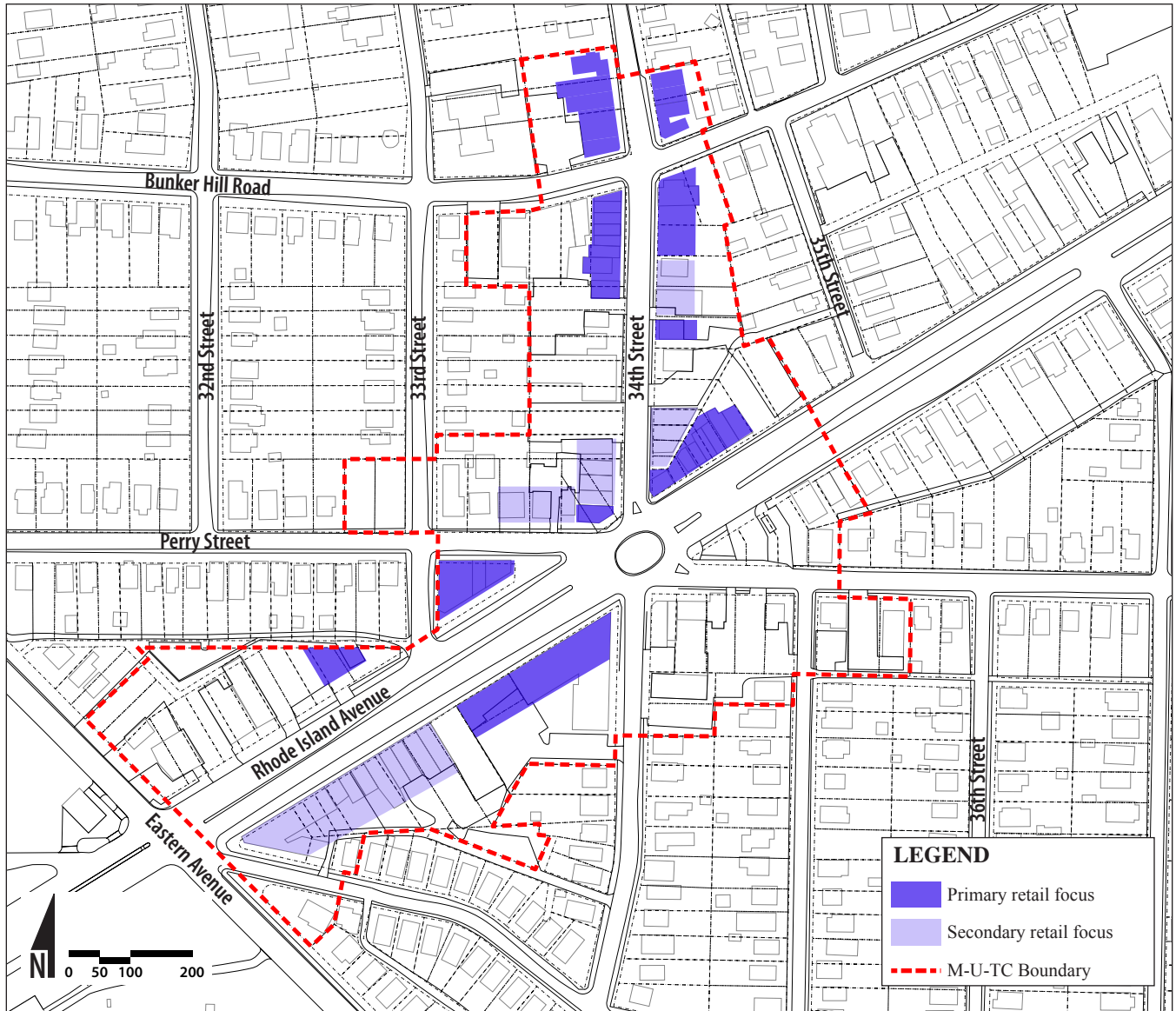


FIGURE 24: PRIMARY AND SECONDARY RETAIL FOCUS AREAS.

### Retail Uses

In many ways, retail land uses represent the principal face for the mixed-use town center area. The interaction between retail space, consumers, passing traffic, and the surrounding community impact the M-U-TC’s image and its prospects for future economic activity and development. Beyond active occupied retail spaces, the M-U-TC is heavily influenced by the number and character of vacant or underutilized commercial spaces in the area.

The quantity of existing retail space (approximately 90,000 square feet) exceeds today’s market demand of approximately 60,000 square feet. However, long-term growth is anticipated. In order to be beneficial to the area, retail properties and operating businesses should be of a sufficient size and mix and also self-sustaining, even if surrounded by vacant storefront space. Additionally, retail plays a vital role in attracting investment in other land uses, such as residential.



With respect to the plan vision and its implementation, retail land uses should be supported by considering the following principles:

- Provide an attractive and contiguous retail street frontage at economically sustainable locations to advance the pedestrian precinct. Utilize Mount Rainier’s large inventory of existing retail space that can easily accommodate future storefronts.
- Maintain a critical mass of different types of retailers adjacent or proximate to each other in order to promote combined patronage, which helps to re-tenant vacated space, and encourages the ongoing investment of retailers in a given location.
- Focus retail uses at the intersections of 34th Street and Bunker Hill Road and along Rhode Island Avenue, extending from the traffic circle.
- Attract new retailers to help elevate the level of activity and image of the entire mixed-use town center area and support existing retail users.

## Other Commercial Uses

Aside from traditional retail uses, the M-U-TC area has a variety of other existing commercial uses, most notably professional office and contractor services firms. These other commercial uses are located on the ground floor of retail-oriented properties. Other similar uses may occupy second level space of buildings or former residential buildings. These alternative land uses perform a vital role in drawing customers and clients to the M-U-TC area, and provide some amount of employee-related economic support of other M-U-TC businesses during the day. While sustaining and growing these smaller-scale commercial land uses should occur in tandem with advancing the goals of the plan vision, their impact is limited to incremental gains in economic development.

## Residential Uses

Residential land uses are interspersed within the area and help to define the character of the M-U-TC and the greater Mount Rainier area. The plan vision foresees both residential-related redevelopment and infill development as major development opportunities for the M-U-TC area. Future development may benefit from incorporating the following strategies and characteristics:

- Add new housing and residents to provide a broad spectrum of benefits, including shoring up retail vitality, reconnecting the streetscape, helping to alleviate crime and safety concerns, and adding to the tax base.
- Create opportunities for alternative market-based ownership housing types in the M-U-TC area, such as rowhouses, flats, and other forms of multifamily housing that can draw on the local and regional market for support.

## Historic Preservation/Adaptive Reuse Recommendations

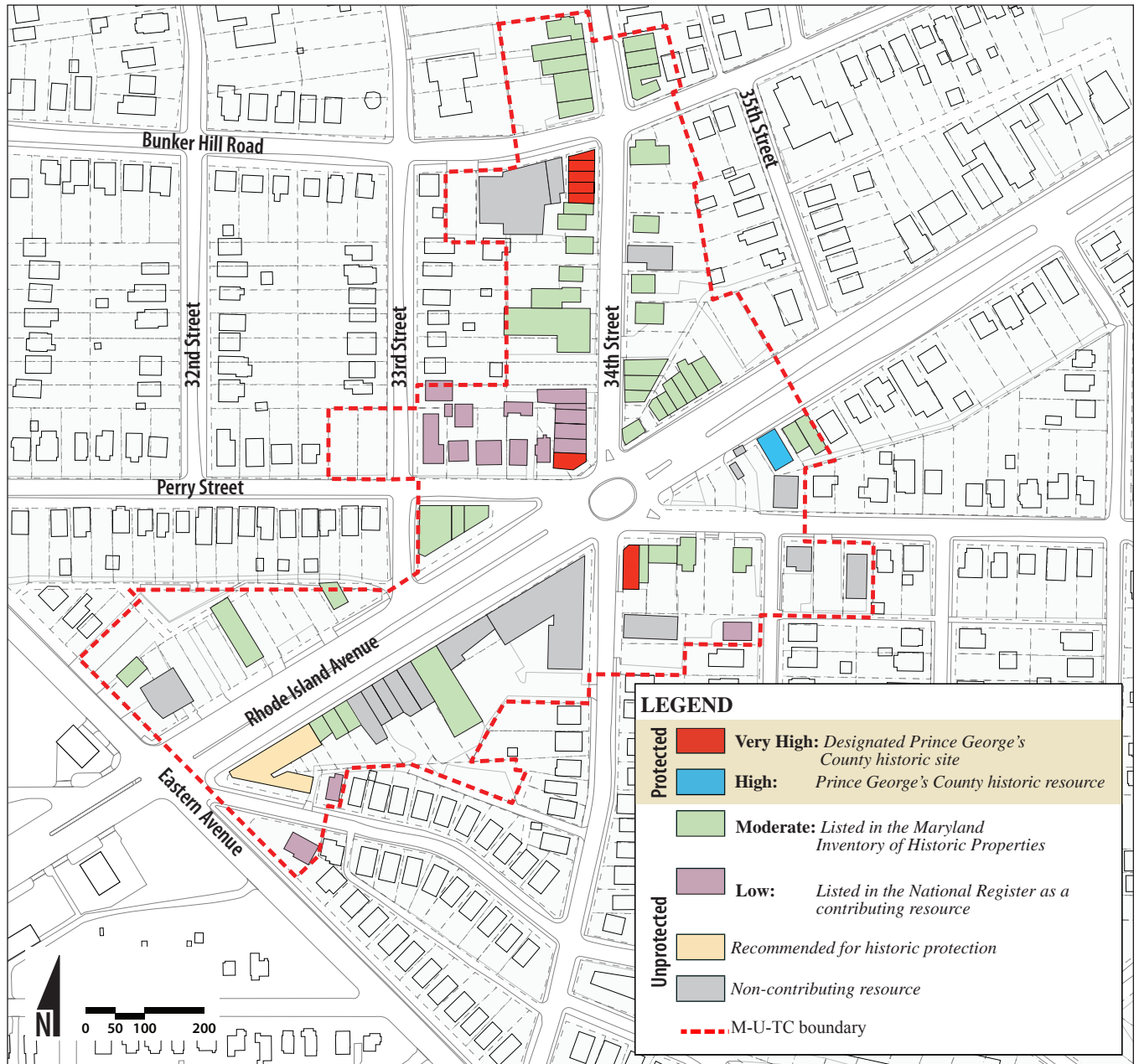


FIGURE 25: PROTECTED AND UNPROTECTED HISTORIC PROPERTIES.

### General Historic Improvement Significance and Actions

The Mount Rainier Historic District is a prime example of a community that changed the character of western Prince George's County during the early twentieth century. The historic district is also important for its expansive and diverse collection of vernacular inspired buildings erected during the period between 1900 and 1940.

The mixed-use town center falls within the boundaries of the Mount Rainier National Register Historic District. Identified as the community's downtown from its early stages of development, the mixed-use town center has historically contained an eclectic collection of residential, commercial, and mixed use buildings that maintained the character of a small town "Main Street." In support of the vision of the plan, preserving and restoring the "Main Street" character will require



the implementation of distinct, yet vital, historic improvement actions:

- Restore historic property by removing non-contributing additions.
- Maintain and repair historic materials and retain the property’s form, details, and existing historic character.
- Alter and add to a historic property to meet continuing or changing uses while retaining the property’s historic character.
- Build new structures that are sympathetic in massing to the historic character of the area.

## General Improvement Areas

### 34th Street

The intersection of Bunker Hill Road and 34th Street is an important focal point for restoring the “Main Street” feel. The buildings typically have high integrity and will require basic maintenance and repair of existing windows, storefronts and porches, as well as repairs to masonry and other minor façade improvements. A few buildings—including 3401 34th Street and the flat fronts, 3807–3815 34th Street—will require restoration of their façades, the removal of non-contributing additions, and the replacement of non-contributing building materials. Vacant lots suitable for construction should follow the scale, massing, and styles reflective of the area.

### Civic Core/City Center

Most of the structures in the Civic Core have a high level of integrity. Adaptive reuse should be more prevalent, especially for properties south and east of the traffic circle, including properties along east Perry Street and Star/Potts Hall (3405 Rhode Island Avenue). The Prince George’s Bank (3800 34th Street) and the Sanitary Grocery Company Building (3401 Perry Street) have been designated as Prince George’s County historic sites. Both sites will require restoration and maintenance and repair because of their high integrity. Other buildings, such as 3300 Rhode Island Avenue and its adjacent properties, as well as the group of transit

oriented retail (3410–3248 Rhode Island Avenue) will require similar restoration and repair.

### Rhode Island Avenue

Because of the large portions of vacant/developable land and the lack of historic integrity of the properties within the Rhode Island Avenue district of the M-U-TC, specific improvement actions will be oriented to adaptive reuse and new construction. Infill construction shall be sympathetic to the nature of the surrounding buildings in scale, massing, and quality. The character of existing buildings shall be retained even with the addition of floors or changing of uses. An exception is the historic gas filling station (3220 Rhode Island Avenue), which is a proposed Prince George’s County historic site. Restoration, maintenance, and repair of the gas filling station will be required because of its poor condition in order to preserve an important landmark in the development of Historic Mount Rainier.

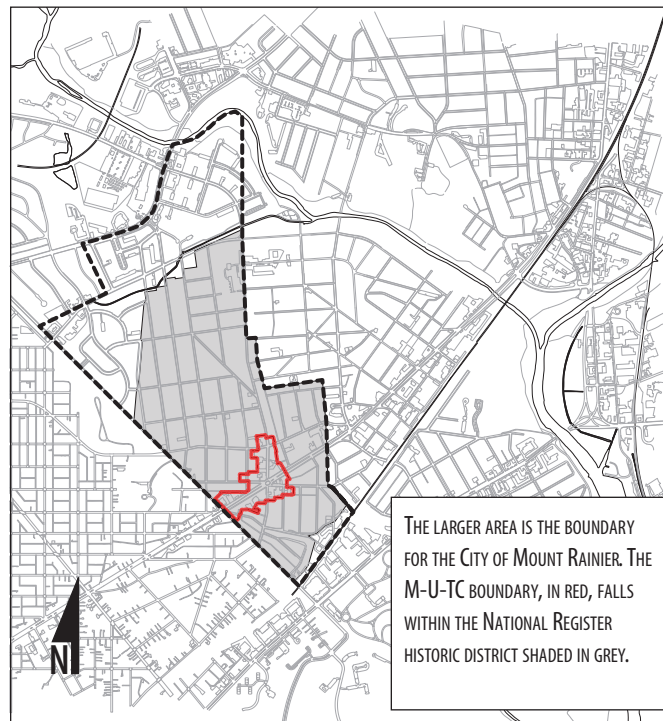


FIGURE 26: M-U-TC, MOUNT RAINIER, AND HISTORIC DISTRICT BOUNDARIES.

## Environmental/Sustainability Recommendations



FIGURE 27: LOCATION OF PERMEABLE PARKING AREAS AND RAIN GARDENS/GREEN STREETS PROTOTYPE.

### Sustainable Strategies

The M-U-TC area is located within the greater Anacostia River watershed. The water quality in the Anacostia River has been compromised and degraded over many years and redevelopment should consider the inclusion of urban environmental design elements that will have a low impact on natural resources within the community.

Redevelopment practices will need to incorporate strategies that help to improve water quality, reduce flooding in neighboring communities, manage stormwater quantities, and diminish erosion and siltation

in the river. These strategies integrate, the most current methods for sustainability in urban environmental site design. Rain gardens, infiltration trenches, urban tree canopy, permeable pavers, bioretention ponds, and green roofs are all appropriate site design details that may, in combination, create a more sustainable community and help mitigate water degradation and associated stormwater run-off problems.

With respect to stormwater management, the plan vision calls for the construction of two rain gardens, with native plant species, on both sides of 34th Street, and the use of permeable paving in any anticipated future share parking lots within the M-U-TC. Both of these measures will help to capture and clean water runoff from 34th Street.



## Transportation Recommendations

The plan vision's primary transportation objective is to develop a district that is pedestrian-friendly along both Rhode Island Avenue and 34th Street. The implementation of several transportation improvements such as the enhancement of pedestrian crossings, the provision of curb extensions and wider sidewalks, the installation of pedestrian signals, and the conversion of the existing bus turnaround depot into a public park, will accomplish this objective.

### Pedestrian Precinct

#### *Rhode Island Avenue Elements*

The plan recommends the installation of curb extensions at the intersection corners of Rhode Island Avenue with Eastern Avenue and with 33rd Street along the north and south sides of Rhode Island Avenue from Eastern Avenue to the circle. The introduction of curb extensions minimizes the crossing distances for pedestrians while maintaining the existing width of the travel lanes along Rhode Island Avenue. The plan vision also recommends a raised off-street bicycle track on both sides of the existing Rhode Island Avenue curb between Eastern Avenue and the traffic circle. In this street configuration, an independent travel way will be separated from the proposed sidewalk with landscaping along that same section of Rhode Island Avenue.

Additionally, the plan recommends the installation of a crosswalk on the Prince George's County side of Rhode Island Avenue at its intersection with Eastern Avenue. This modification will require additional pedestrian signals, as well as adjustment of the existing traffic-signal timing.

The plan recommends a new pedestrian crosswalk on Rhode Island Avenue, just east of its intersection with 33rd Street, with an accompanying pedestrian-activated crosswalk signal. Pedestrians will be able to activate this signal to stop vehicular traffic along Rhode Island Avenue, allowing them to cross safely. When the signal is not activated, traffic moves without having to stop.

Another recommendation supports the installation of a pedestrian-activated crosswalk signal located just west of the circle on Rhode Island Avenue. This signal will

activate the traffic signal just east of the circle and will allow pedestrians to cross Rhode Island Avenue with greater ease.

#### *34th Street Elements*

The plan recommends the widening of sidewalks on both sides of 34th Street so that they extend further into the roadway, while still retaining two ten-foot travel lanes on 34th Street from the circle to Bunker Hill Road. Widening the sidewalk to provide for improved pedestrian circulation on 34th Street is a key component of making this street a more attractive retail destination for prospective patrons. In addition to widening the sidewalk, the plan proposes the installation of curb extensions along both sides of 34th Street to allow for the placement of trees along the street between the circle and Bunker Hill Road.

The plan also proposes curb extensions at the intersection of 34th Street and Bunker Hill Road for the southeast, southwest, and northeast corners of the intersection. This improvement will facilitate pedestrian crossings at this four-way stop intersection. In addition, it is recommended that textured crosswalks be installed and that the interior of the intersection be patterned to reinforce it as the center of the retail activity on upper 34th Street.

### Shared Parking

The need for additional parking to support existing and new retail uses and infill development can be accommodated through the use of shared parking strategies. Potential locations for shared parking include:

- The lot behind 4000 34th Street (potential to provide 24 spaces).
- The lot south of 3841 34th Street (potential to provide 16 spaces).
- The lots 3802–3820 34th Street south of the Metropolitan Church of God during times when there are no church services.
- The northern portions of 3424–3428 Rhode Island Avenue.
- 3411 Perry Street
- 3503 Perry Street

## Civic Green and WMATA

Historically, Mount Rainier was established as a street car community adjacent to the District of Columbia. The turnaround area within the Civic Core served as the terminus for that street car system. The existing Metro bus turnaround area currently serves as a transit hub for the residents and businesses of Mount Rainier. The three bus lines using this facility provide connections for the immediate surrounding neighborhood to other modes of transportation, such as the Metrorail system. It is envisioned in the future that the Civic Core, which is centrally located adjacent to a major thoroughfare, will support additional modes of transportation, including either the extension of the proposed District of Columbia's Rhode Island Avenue street car system or a bus rapid transit (BRT) system. The *Approved Countywide Master Plan of Transportation* recommends extension of the BRT system from the District of Columbia to the planned circumferential fixed guideway transit (Purple Line) between Bethesda and New Carrollton.

In the near- and medium terms, the plan recommends the retention of the existing bus turnaround operations

that support the transit facility adjacent to the Mount Rainier City Hall. As a long-term vision, the development plan recommends the creation of a civic green adjacent to the Mount Rainier City Hall that will create a sense of place by enhancing the Civic Core. This investment will also support future development opportunities along the Rhode Island Avenue transit corridor. This future civic green may accommodate the existing bus lines through a modified bus turnaround area and potential future new modes of transportation. A more detailed evaluation of feasible options should be considered as part of a subsequent corridor study in order to ensure quality and efficient transit service, which would support the plan's ultimate vision for the area. This study should consider the prioritization of bus circulation based on future additional modes of transportation, and the location, design, and maintenance of future bus stops, bus lay over areas, shelters, lighting, and passenger amenities. Partners in the study of the corridor should include WMATA, Prince George's County, the City of Mount Rainier, and the District of Columbia.



ILLUSTRATIVE SKETCH OF THE CIVIC GREEN.



**RIGHT: THE PROPOSED LOCATION FOR A MARC STATION.**  
**BELOW: THERE IS POTENTIAL FOR EXTENSION OF THE FUTURE DC STREETCAR SYSTEM INTO PRINCE GEORGE'S COUNTY.**



## Proposed MARC Station

Together with the 2009 *Approved Port Towns Sector Plan and Sectional Map Amendment*, this plan recognized the potential for a MARC station that will serve as a local mode of transportation enabling nearby residents to access Union Station to the south or points north such as Baltimore and the Baltimore Washington International Thurgood Marshall Airport. The main route from the Mount Rainier Civic Core to the potential station has been identified as 34th Street to Wells Avenue by way of Otis Street.

The plan recommends that limited parking be made available to support the station on the north side of the tracks. Any major parking facilities to support this station should be located on the south side of the train tracks in Cottage City, given its proximate access to Bladensburg Road and the surrounding industrial land uses. The implementation of a new MARC station would ultimately require coordination with the Maryland Transit Administration (MTA), the county, and the city.

## Extension of the DC Streetcar System

The District of Columbia Department of Transportation (DDOT) has started construction of a new streetcar line in Anacostia, tracks are currently

being laid on H Street and Benning Road for a second line, and planning is underway for additional segments across the city. The entire 37-mile proposed streetcar system will take at least 7-10 years to build, which fits into the medium-term time frame established by this plan (see the Implementation Chapter).

Ultimately, the DC Streetcar system will include a line along Rhode Island Avenue extending from Florida Avenue to the District Line at Eastern Avenue. The implementation of the first phase of the DC Streetcar system does not include the Rhode Island Avenue line, which is preliminarily programmed in Phases 2 and 3 of the streetcar development, pending funding. The Rhode Island Avenue line has not been designed at this point; nor have cross-sections or alignments been determined.

This plan envisions extending the streetcar system into Prince George's County through the M-U-TC area. This extended streetcar system should operate along the curb travel lane and have curb extensions that allow patrons to access the streetcars at the designated stops. The plan vision incorporates this type of operation into its overall roadway geometric requirements. A future DC Streetcar extension will make travel to and from the District of Columbia much easier for Mount Rainier residents, workers, and visitors, and it will complement existing transit options.

## Arts and Arts Community Recommendations

### Role of the Arts/Gateway Arts District

This plan envisions that both the Mount Rainier artists and their art act as key catalysts for revitalization of the M-U-TC area. The plan creates new opportunities for artists to engage with the local community.

The City of Mount Rainier is an important part of the Gateway Arts District. The city's arts identity is a significant resource for marketing and promoting development in the mixed-use town center area. An existing arts community helps distinguish the M-U-TC area from other locations and attracts outside interest in the community. Arts targeted projects may help leverage external funding sources, and the artists, themselves, may help expand the visibility of the arts in the M-U-TC area in order to implement the plan vision. Because of its established arts community and previous efforts, Mount Rainier is well positioned to explore new opportunities to visibly promote its art community and exhibit its art in new development projects.

### Pop-up Galleries

As an interim arts strategy for the M-U-TC area, the plan proposes that the city and county make available selected venues, called "pop-up" galleries, in city-owned and county-owned vacant or underutilized storefront properties that, with minimal operational costs, may energize and activate art spaces along the M-U-TC's commercial corridors. These spaces may be used for visual and/or performing arts activities.

The City of Mount Rainier would play a key role in the implementation of this strategy by selecting the storefronts. The city would also manage the general maintenance and operations activities of the spaces for limited periods of time. Depending on the type of arts activities, the city would determine the associated fees for events held in the storefronts. For example, non-profit activities might be charged a minimal or subsidized fee for use of the venue. Events that charge admission or sell art could contribute a percentage of sales towards building maintenance costs. Artists would be responsible for setting up and preparing the spaces for exhibits or performances.

### Public and Utilitarian Art Program

In the proposed Public and Utilitarian Arts Program, the city will identify key open spaces, public parks, and/or publicly-visible privately owned sites to display or provide venues for work by local artists. This will feature art (graphics, signage, sculpture, metalwork, fabric art, etc.) in public places. Artists will compete for city sponsored sites and opportunities to feature their art at city-sponsored events or in city-generated media, such as the graphic design of the city newsletter, web site, and logo. Art locations would be "leased" for a designated period. At the end of a specified time period, new artists and art would be selected for the city art spaces. Proposed exhibit spaces could be small or large but should be able to accommodate various types of art such as: permanent or temporary murals, insets and bas relief on walls, metal work, or performance art (dance or repertory theater).

The program details would include a city appointed jury to select visual and performance art; an inventory of predetermined venues, including Gateway Arts events and outdoor art spaces; and the establishment of a municipal fund to cover the city's portion of the shared general maintenance, insurance, and security costs with the Gateway CDC, and artists.

### Signage and Wayfinding Program

The proposed Signage and Wayfinding Program combines the talents of local artists and historians to create a Mount Rainier heritage trail signage system. Local artist-inspired and designed signage may be used to create heritage trails and/or direct pedestrian traffic through the M-U-TC area. Heritage trail signage will feature information about places and events that have historical significance to Mount Rainier. Additionally, wayfinding signage will help visitors locate city services and merchants within the M-U-TC and act to "brand" the mixed-use town center area.







Design Standards and Guidelines **4**



## Design Review Process

### Goals

The Town Center (or M-U-TC) Design Review Committee, the City of Mount Rainier, the county, and the public (including building owners, business owners, architects and other design professionals) shall use these design standards and guidelines when considering or designing new development and property improvement within the M-U-TC. The purpose of the standards and guidelines is to promote high quality urban and architectural design within the M-U-TC area and to encourage the creation of a cohesive and attractive environment consistent with the plan vision. These standards and guidelines help to protect and reinforce the existing fabric of the historic district as well as the character of the town center area. They also give direction to revitalization efforts.

The standards and guidelines shall be used alongside the other components of the M-U-TC Development Plan—such as the Plan Vision and the Implementation—in order to gain a full understanding of the goals of the development plan.

### Applicability

The design standards and guidelines apply to all new development and exterior improvements, beyond routine maintenance and repair, on properties located within the M-U-TC Zone. Applicants may deviate from the design standards and guidelines through the detailed site plan process provided they can demonstrate how their proposal advances the vision for the M-U-TC Zone.

As set forth in Section 27-108.01 of the Zoning Ordinance, “the words ‘shall,’ ‘must,’ ‘may only,’ or ‘may not’ are always mandatory standards and not discretionary. The word ‘may’ and ‘should’ are permissive. The words ‘including’ and ‘such as’ do not limit a term to the specified examples, but are intended to extend its meaning to all other instances or circumstances of like kind or character.

### Relationship to the Gateway Arts District

The 2004 *Approved Sector Plan and Sectional Map Amendment for the Prince George’s County Gateway Arts District* determines the uses that are permitted in the Mount Rainier M-U-TC. The use table for the Gateway Arts District Sector Plan can be found on pages 167-199 of that plan and in the appendix of this plan (pages 158–178). If the proposed use is not listed in the table, it is prohibited. Changes to the use table must be approved by the District Council through a detailed site plan process.

The town center character area development district standards found in the Gateway Arts District Sector Plan do not apply to the Mount Rainier M-U-TC Zone. The development plan design standards and guidelines found in this chapter prevail.

### Economic Revitalization

Attractive and inviting streetscapes and building façades help advance opportunities for economic development in Mount Rainier. A positive physical image impacts a broad base of visitors, business owners, property owners, and potential investors, including financial institutions. While there may be limited need for some property owners to use the design standards and guidelines, for such things as signage and fencing, the design standards and guidelines are essential to improving the overall marketability of the M-U-TC area.

Well conceived and implemented design standards and guidelines that respect the unique character of Mount Rainier will benefit all properties. The assurance that design standards and guidelines are applied in a consistent and comprehensive manner will attract the necessary investment and capital, both in the near term and the long term. This assurance will achieve the following:

- Enhance patronage of the M-U-TC area.
- Heighten business and property-owner confidence.
- Give reassurance to new investors regarding return on their investment.
- Advance economic sustainability.

## Code Enforcement

All new construction and improvements requiring a building permit will be subject to review by the Department of Environmental Resources (DER).

New construction and improvements that are subject to the City of Mount Rainier’s municipal ordinances are enforceable by the city. Applicants are responsible for obtaining any required municipal permits for exterior alterations to properties.

## M-U-TC Design Review Committee Composition

The City of Mount Rainier solicits and advertises for potential candidates to the M-U-TC Design Review Committee. The City Council makes its selection from this group of candidates and submits its recommendations to The Maryland-National Capital Park and Planning Commission (M-NCPPC) for consideration and approval by the Prince George’s County Planning Board.

During the advertisement and selection process, efforts are made to ensure that at least one of the residents, business or property owners, or government representatives on the committee is a practicing artist or design professional with experience in planning, architecture, landscape architecture, or historic preservation. Solicitation of the design professional member is done regionally and is not limited to candidates from the city.

The appointed M-U-TC Design Review Committee consists of seven members, with staggered terms, to ensure that there is not a full turnover of the committee at any given time. The composition of the M-U-TC review committee is as follows:

- Two residents from the city.
- Two business or property owners within the M-U-TC area.
- Two municipal government representatives.
- One design professional.

Each member serves a two-year term and may be reappointed to subsequent terms by the Planning

Board. Additionally, a designated member of the M-NCPPC’s Prince George’s County Planning Department serves as non-voting staff person that advises the committee and attends all its meetings.

## Use of the Design Standards and Guidelines by the M-U-TC Design Review Committee

The M-U-TC Design Review Committee reviews all exterior renovations and new development projects within the Mount Rainier M-U-TC Zone District to ensure that the applications comply with the intent of the plan vision, the design standards and guidelines, the implementation strategies of this development plan, and the Secretary of Interior’s Standards for Rehabilitation. Use of the guidelines will assist the committee in making consistent and fair recommendations that are based on sound urban design and preservation practices.

All infill development projects, exterior renovations, signage, awnings, fencing, screening, exterior seating and display, surface lot parking, site lighting, or other site improvements and/or changes within the M-U-TC area shall be reviewed by the M-U-TC Design Review Committee.

## Application Process

### Prior to Submission

- The applicant should review the 2004 *Approved Sector Plan and Sectional Map Amendment for the Prince George’s County Gateway Arts District* to determine if the proposed use is permitted within the town center character area.
- The applicant should meet with the designated M-NCPPC staff member to review the proposal for compliance with the plan vision, design standards and guidelines, and implementation components of the development plan. Staff shall review the application for completeness in accordance with the submission requirements and offer guidance regarding the “permit” and “detailed site plan/special permit” processes.



**Submission**

- The applicant shall submit eight copies of the application and the applicable documents, as listed on page 59 under Application Submission Requirements, to M-NCPPC for staff review and distribution to the M-U-TC Design Review Committee and the City of Mount Rainier.
- The designated M-NCPPC staff member shall produce a staff report documenting the facts of the application. The staff member shall make recommendations in the staff report.
- The staff report, the application, and a copy of the applicant’s submitted documents shall be transmitted to each member of the M-U-TC Design Review Committee, the City of Mount Rainier, and the applicant.

**M-U-TC Design Review Committee Meeting**

- M-U-TC Design Review Committee meetings shall be held within thirty days of M-NCPPC’s receipt of a complete application. The applicant shall be notified of the time, date, and location of the committee meeting. The applicant or applicant’s agent shall be required to attend the committee meeting to present the application.
- The M-U-TC Design Review Committee shall review and make recommendations on all building permits or detailed site plan/special permit applications submitted to the county for work within the M-U-TC study area. All committee members should be familiar with the City of Mount Rainier Historic District and the plan vision, design standards and guidelines, and implementation strategies of this document.
- The applicant shall present the proposal to the M-U TC Design Review Committee. Failure to attend and present the application shall result in the automatic deferral of the project to a subsequent M-U-TC committee meeting.
- The designated M-NCPPC staff member shall verbally highlight key elements from the staff report and make any recommendations. Staff shall also offer supporting comments and answer questions from the committee.

- The M-U-TC committee shall review the application and recommend either:
  - Approval as submitted.
  - Approval with conditions and/or recommendations.
  - Denial with clarifications as to the M-U-TC Design Review Committee’s action.
  - Deferral for resubmission with explanation and recommendations.

**After M-U-TC Review Committee Meeting**

- The designated M-NCPPC staff member shall prepare meeting minutes summarizing the discussions, recommendations, and actions taken by the committee on each application reviewed during the meeting.
- Approved applications shall be stamped by the M-U-TC Design Review Committee to signify that the plans are in compliance with the plan vision, design standards and guidelines, and implementation strategies. Applications approved with conditions or recommendations will need to be revised prior to being stamped by the committee to show that the updated proposal complies with the plan vision, design standards and guidelines, and implementation strategies.
- The applicant may appeal M-U-TC Design Review Committee recommendations to the Prince George’s County Planning Board through the detailed site plan/special permit process.

## Application Submission Requirements

The applicant shall submit eight copies of all relevant building, site plan and/or signage information required for the submission to show compliance with the plan vision, design standards and guidelines, and implementation strategies. The following information is recommended as possibly being necessary for the type of project work proposed:

### Photographs

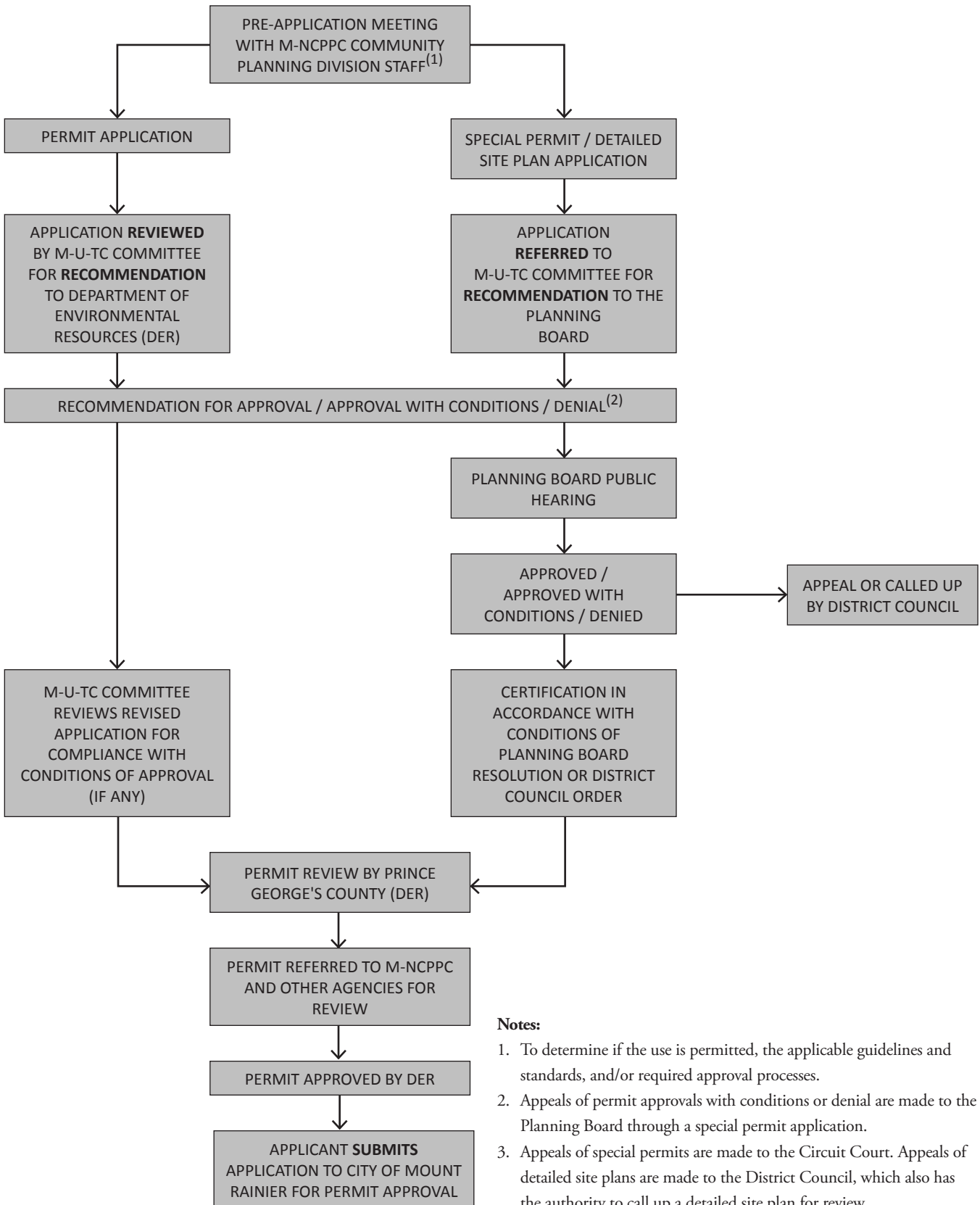
The application shall include comprehensive exterior photographs of the building, structure, or site in question. Since the building, structure, or site is in a historic district, the application shall also include photographs of the adjacent buildings and the immediate neighborhood. All photographs shall be good quality and clearly show the area affected by the proposed work.

### Architectural Drawings

- *Existing Conditions Plans*  
Property lines, adjacent structures, and existing buildings shall be drawn to an architectural or engineering scale.
- *Extent of Work*  
The application shall clearly show the extent to which the existing building or structure is to be preserved, altered, or demolished. An application for demolition shall include architectural plans or diagrams clearly showing the extent of proposed demolition.
- *Site Plan*  
The site plan shall include dimensioned property lines, building locations, parking, vehicular and pedestrian access points, and all other proposed improvements.
- *Massing Drawings*  
As necessary, architectural drawings or model photographs indicating the general massing, composition, or design of the proposed work within its surroundings shall be provided. These drawings shall be drawn to scale and dimensioned with important measurements, including overall width, length, and height.
- *Elevation Drawings*  
Elevations shall show all façades, including information such as building height, floor-to-floor height, architectural features, detailing, openings (including windows and doors), façade materials, and color. Drawings shall clearly show the exterior design of the proposed work within its surrounding and adjacent context. The application shall clearly show the proposed color and/or finish and texture of materials to be used for all new elements. Finish, color, and textures of adjacent existing materials must be provided (in photographs or drawings) for comparison to the proposed work.
- *Parking Plan and Layout*  
Where applicable, a parking plan and layout shall be submitted as well as documentation illustrating how proposed shared parking or proposed reductions in parking numbers may be implemented.
- *Landscape Plan*  
All development within the M-U-TC shall provide landscape plans showing compliance with the applicable provisions of the *Prince George's County Landscape Manual*.
- *Signage*  
Signage submissions shall include proposed dimensional locations, materials, colors, and any lighting information.
- *Other Components*  
Additional components, such as proposed site lighting, building lighting, fencing, screening, low screen walls, and other site or building components not noted above, shall be submitted with the application so that a full understanding of the new development, addition, renovation, alteration, or signage may be apparent during the review process.



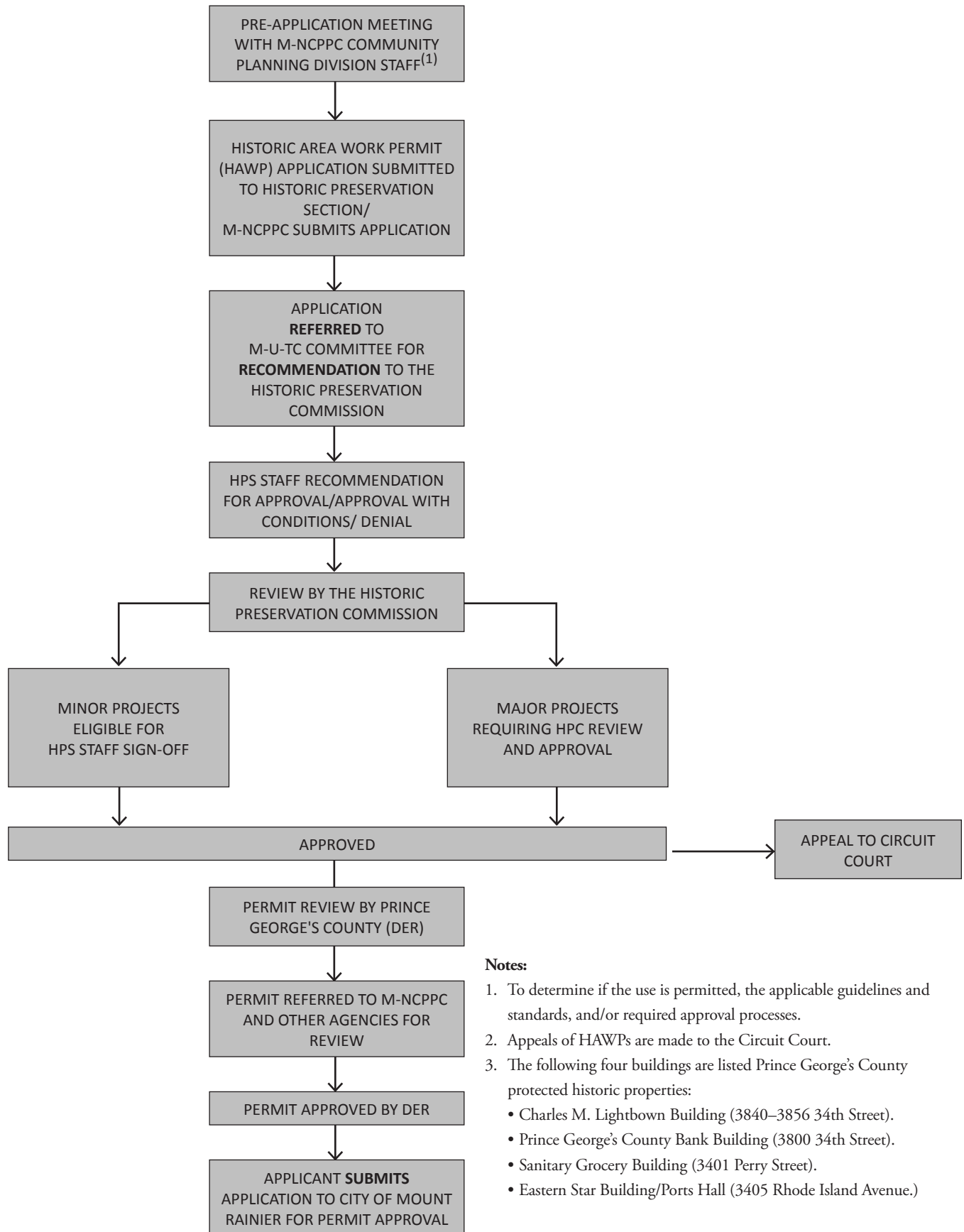
## Design Review Process | *Non-Historic Properties*



**Notes:**

1. To determine if the use is permitted, the applicable guidelines and standards, and/or required approval processes.
2. Appeals of permit approvals with conditions or denial are made to the Planning Board through a special permit application.
3. Appeals of special permits are made to the Circuit Court. Appeals of detailed site plans are made to the District Council, which also has the authority to call up a detailed site plan for review.

## Design Review Process | *Historic Properties*





## Plan Vision Overview

### The Historic District

The entire M-U-TC area is contained within the Mount Rainier Historic District, which has been listed in the National Register of Historic Places since 1990.

The M-U-TC area contains approximately ninety properties; forty-six are listed on the Maryland Historical Trust’s Inventory of Historic Properties and four of these are Prince George’s County protected historic properties. The four properties are:

- Charles M. Lightbown Building (3840–3856 34th Street)
- Prince George’s County Bank Building (3800 34th Street)
- Sanitary Grocery Building (3401 Perry Street)
- Eastern Star Building/Ports Hall (3405 Rhode Island Avenue)

Additionally, sixteen properties are “contributing” historic properties to the National Register district.

A large number of “contributing” properties constructed within the period of significance (late 1890s to early 1940s) for the Mount Rainier Historic District still retain varying degrees of architectural and historic integrity. The low-scale, one- to three-story mixture of mixed-use residential and commercial structures is a defining characteristic of the Mount Rainier town center area. Detached, closely sited, single-family homes make up the bulk of the structures bordering and surrounding the M-U-TC area.

Most of the commercial buildings in the Mount Rainier Historic District are located in the city’s historic downtown area, which traditionally contained a mixture of both residential and mixed-used buildings, just as it does today.

The downtown area includes Rhode Island Avenue beginning at the District of Columbia boundary line east to the Town of Brentwood and the block of Perry Street, east of 34th Street. It also includes the two blocks of 34th Street north of Rhode Island Avenue. Today the commercial area also extends a half a block of 34th Street south of Rhode Island Avenue.

The low level of integrity of many structures will allow for sensitive and carefully crafted development of selected sites. This development may include historically compatible additions and new replacement buildings. The guidance included herein is designed to assist the M-U-TC Design Review Committee with analysis and review of new work, repair, and maintenance activities throughout the M-U-TC area. The guidance should foster new development, while maintaining the character and small town feel of the City of Mount Rainier.

The Mount Rainier Historic District is integral to the design character of three districts within the M-U-TC area:

- Upper 34th Street
- Rhode Island Avenue
- Civic Core/City Center

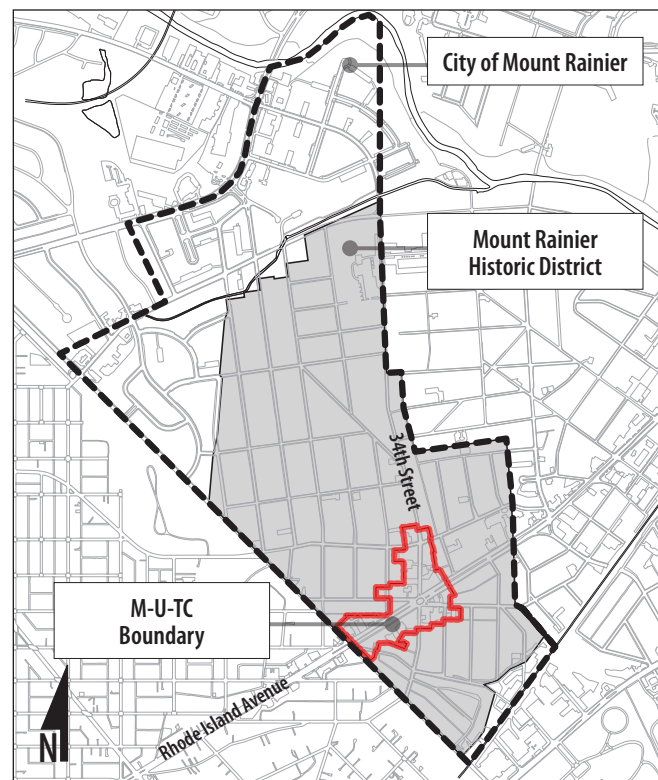


FIGURE 28: CITY OF MOUNT RAINIER.



### District Character: Upper 34th Street

The intersection of 34th Street and Bunker Hill Road is a focal point to downtown Mount Rainier that contains a mix of regional and local neighborhood-serving commercial uses. This four-way intersection has direct access to the surrounding neighborhood. The intimate pedestrian and vehicular environment of 34th Street is reinforced by its relatively narrow street width and its generally contiguous street frontages of improved and occupied properties. The surrounding area features supporting cultural and faith-based institutional uses. The street corners are anchored by low-scale historic buildings that have retained much of their original character. The existing building fabric provides subtle clues to what 34th Street was like in the mid-twentieth century. With some noted exceptions, the existing buildings are appropriate for mixed-use retail and should be preserved.

Market support for the location does not call for substantially increased intensity of use. There is limited reason to consider replacing the existing structures with larger structures. While there are longer-term opportunities for some infill on current vacant sites, the vision of the future for this area should be mostly focused on improving the existing architectural fabric.

#### Design Summary

- Maintain a “Main Street” character for 34th Street.
- Build new buildings on the property line facing public streets with no setbacks except where grandfathered.
- Concentrate retail storefronts near the intersection of Bunker Hill Road and 34th Street and encourage mixed-use retail, office, and residential elsewhere.
- Maintain a general one- to three-story height for buildings.
- Convert some residential buildings to small-scale retail and office.
- Use natural materials for buildings.
- Install pedestrian-scaled signage.



### District Character: Rhode Island Avenue

In contrast to the focused neighborhood character of Upper 34th Street, Rhode Island Avenue serves multiple and, at times, conflicting purposes. It is a critical transportation corridor that has great market potential, but can also negatively impact the adjacent residential neighborhood. With its ample right-of-way, the avenue tends to divide the north and south sides of the street. This characteristic is vividly evident by the profoundly different levels of economic activity on the two sides of Rhode Island Avenue. As we look to implement the plan vision for Rhode Island Avenue, this seemingly negative wide expanse to the avenue can provide opportunities to facilitate the construction of taller buildings at the property line and provide space to accommodate on-street parking, bike lanes, pedestrians, outdoor retail, and other open space uses.

Mount Rainier, which serves as a gateway into the county from Washington, D.C., has the potential for substantial growth. The building at 3413 Otis Street sets the tone at the District of Columbia line, and the traffic circle provides a constant visual and functional regional focal point for Mount Rainier.

With its historical elements and existing businesses, Rhode Island Avenue has the contributing factors of location, visibility and existing economic base to help propel needed new development. The mixture of older and newer buildings along Rhode Island Avenue represents a diverse design backdrop for development on infill sites. It also provides opportunities for existing structures ready to be renovated as new infill takes hold. The scale and orientation of the possible redevelopment sites are particularly well suited to medium-sized residential uses.

#### Design Summary

- Maintain a boulevard character for Rhode Island Avenue.



- Construct most new buildings on the property line and retain a setback along the north side of Rhode Island Avenue.
- Concentrate retail storefronts on or near 3300 Rhode Island Avenue and on the south side of Rhode Island Avenue; encourage new residential buildings elsewhere.
- Construct three- to five-story infill buildings.
- Use modern materials for new civic buildings.

### District Character: Civic Core/City Center

The Civic Core/City Center area of Mount Rainier is multifunctional. The intersection of 34th Street and Rhode Island Avenue is distinguished by prominent public spaces and a variety of buildings representative of Mount Rainier’s historic past. This central area, which is a natural focal point for civic identity of the community, includes the city hall, the public library, an historic assembly building, faith-based institutions, a mix of commercial businesses, some residential buildings, and an existing small bus transportation hub.

The central spine of public space of the Civic Core/City Center extends from the existing library and Mount Rainier City Hall westward through the traffic circle to the block containing 3300 Rhode Island Avenue and a corner park space facing back toward city hall. The public streetscape of the Civic Core/City Center is somewhat established with the notable exception of the bus waiting area. The high profile of the civic core with

its visible location along Rhode Island Avenue demands that priority be given to its most critical components.

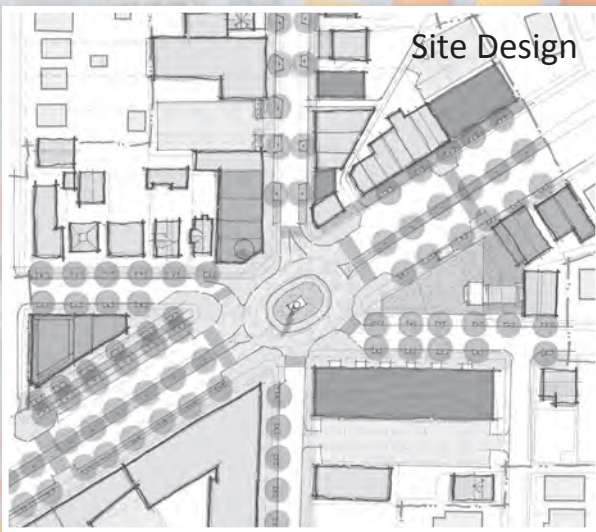
### Design Summary

- Maintain the civic center character that exists around the city hall.
- Construct new buildings on the property line with no setbacks except where grandfathered.
- Encourage civic and residential uses.
- Build three- to four-story infill buildings.
- Use modern materials for additions and transit buildings.



## Mount Rainier M-U-TC Design Standards and Guidelines

The plan vision for the Mount Rainier M-U-TC area provides the framework to create a pedestrian-friendly, vibrant mixed-use town center that serves both residents and visitors. The Public Space, Site Design, and Architectural Elements are the important components that define the streetscapes and open spaces of the M-U-TC. The Design Standards and Guidelines on the following pages address each of these vital components in order to promote the creation of a cohesive and attractive environment consistent with the plan vision.



Numbered items in the Design Standards and Guidelines in **bold type face** are standards and in regular type face are guidelines.



# Public Space Standards and Guidelines

## Public Space | *Street Section—Upper 34th Street*

### Guidelines (South of Bunker Hill Road)

1. Along 34th Street between the traffic circle at the intersection of 34th Street and Rhode Island Avenue and Bunker Hill Road, the sidewalks should be extended by two feet into the parking lane on both sides of the street in order to reduce the parking lanes widths to eight feet.
2. On-street parking should be reconfigured to accommodate street trees. Specifically, street trees should be installed in eight-foot wide tree planters, 52 feet on center to allow for two parallel parking spaces between each planter.
3. Shared lane markings and bikeway signage should designate bicycle routes.

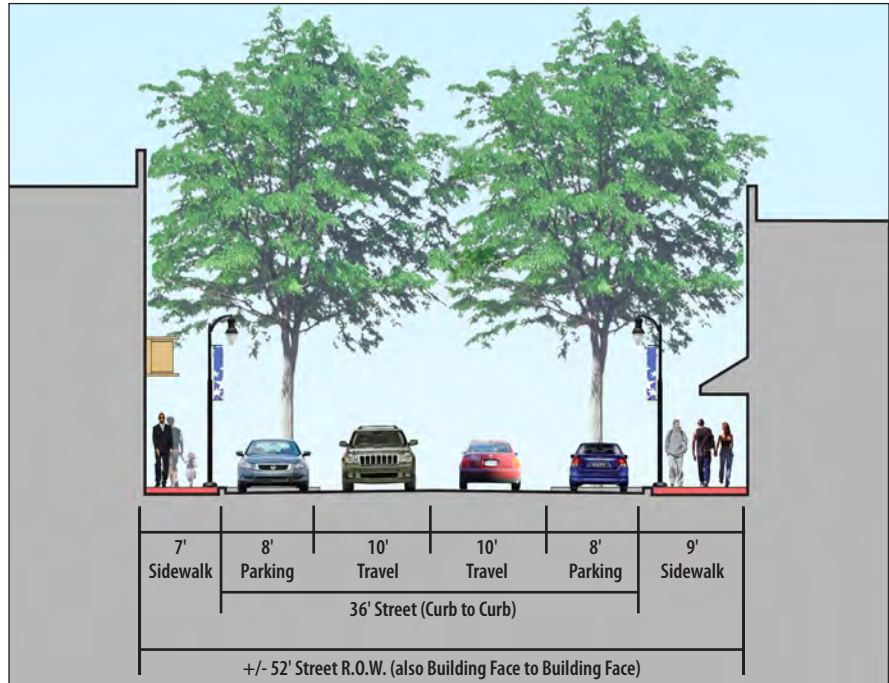


FIGURE 29: PROPOSED STREET SECTION—34TH STREET, SOUTH OF BUNKER HILL ROAD, LOOKING SOUTH.

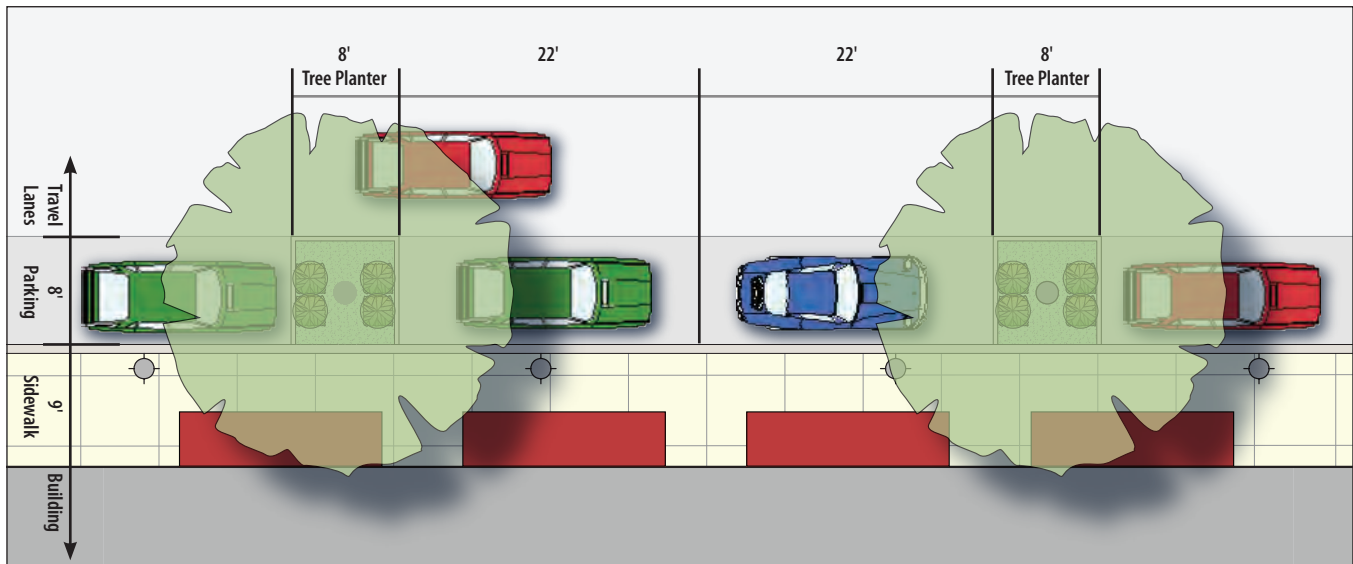


FIGURE 30: PROPOSED STREET PLAN—34TH STREET, SOUTH OF BUNKER HILL ROAD.

## Public Space | *Street Section—Upper 34th Street*

### Guidelines (North of Bunker Hill Road)

1. The existing curb-to-curb cross section of 34th Street, north of Bunker Hill Road, is 38 feet and consists of an eight-foot wide southbound parking lane, two ten-foot wide travel lanes in each direction, and a ten-foot wide northbound parking lane. This cross section of 34th Street, north of Bunker Hill Road, should remain as is.
2. Curb extensions should be installed at all of the corners of the intersection of 34th Street and Bunker Hill Road, except the northwest corner, to minimize the pedestrian crossing distances.
3. Shared lane markings and bikeway signage should designate the bicycle routes.



PROPOSED VISION: UPPER 34TH STREET.

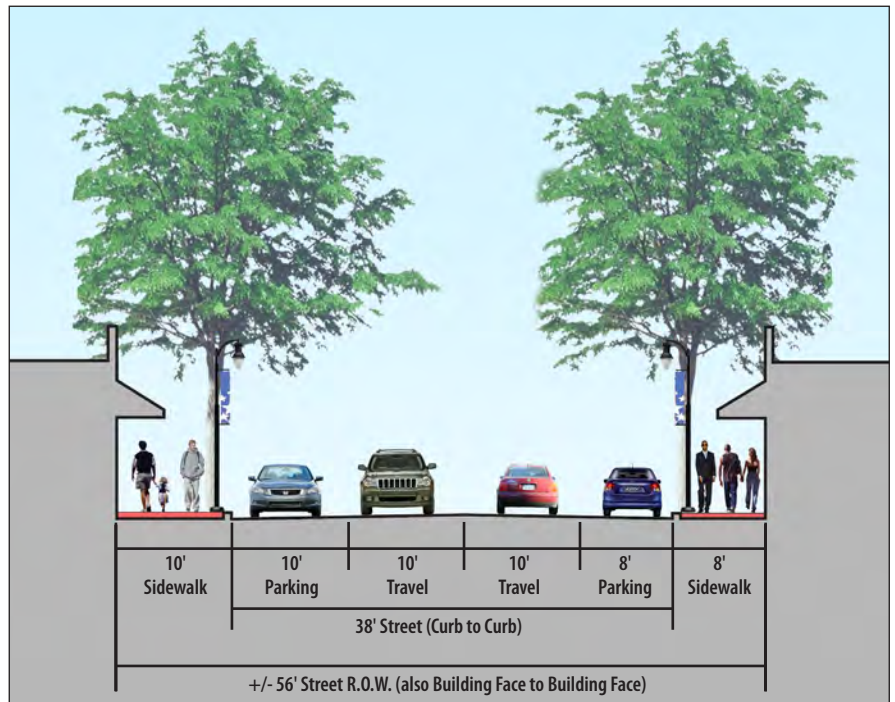


FIGURE 31: PROPOSED STREET SECTION—34TH STREET, NORTH OF BUNKER HILL ROAD, LOOKING SOUTH.

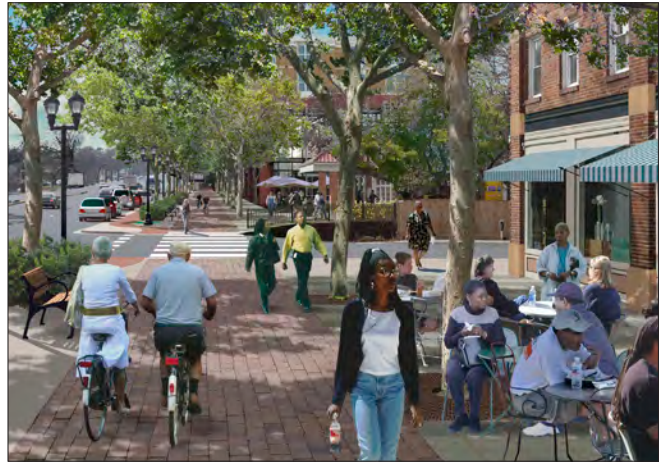


## Public Space | *Street Section—Rhode Island Avenue*

### Guidelines

1. The travel lanes and median width of the proposed Rhode Island Street section between Eastern Avenue and the traffic circle at the intersection of 34th Street and Rhode Island Avenue should not be modified.
2. The proposed curb-to-curb cross section should be 75 feet. It should include one eight-foot wide eastbound parking lane, two 11-foot wide eastbound travel lanes, one 13-foot wide landscaped median, one ten-foot wide westbound parking lane, and two 11-foot wide westbound travel lanes.
3. The long term vision for the corridor is for dedicated, off-road cycle tracks. A buffered, separated facility is desirable along Rhode Island Avenue due to traffic volumes, speeds, and the amount of truck traffic in the corridor. However, cycle tracks will only be implemented after all safety

and design concerns have been addressed to the satisfaction of the SHA Office of Traffic and Safety and consistent with future AASHTO design and construction standards.



PROPOSED VISION: RHODE ISLAND AVENUE.

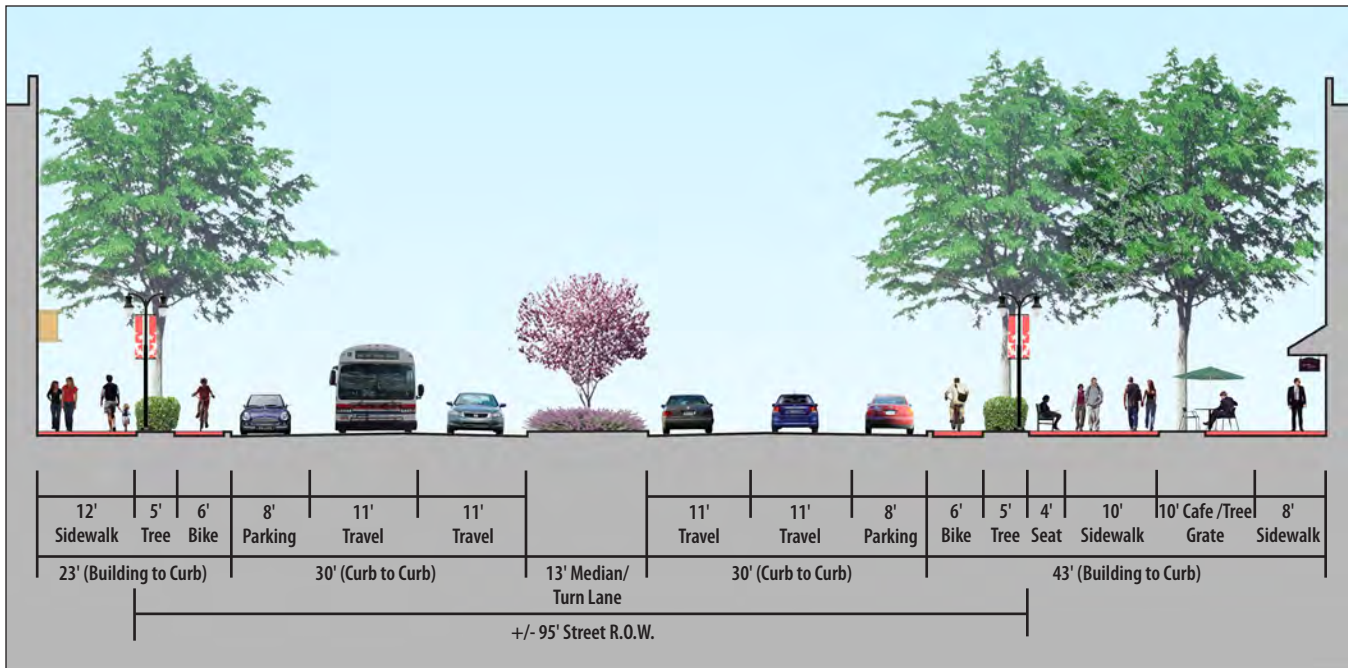


FIGURE 32: PROPOSED STREET SECTION—RHODE ISLAND AVENUE, WEST OF THE TRAFFIC CIRCLE, LOOKING WEST.

## Public Space | *Street Section—Rhode Island Avenue*

### Guidelines (North Side of Rhode Island Avenue)

1. The new street section on the north side of Rhode Island Avenue should modify the parking lanes, public rights-of-way, sidewalks and setbacks on private property. Specifically, the sidewalk should be widened by six feet, and the parking lane should be reduced to eight feet.
2. A six-foot wide cycle track should be built adjacent to the parking lane, leaving space for a five-foot wide planting zone and a 14-foot seating/sidewalk area.
3. Street trees and street lamps should be placed alternately at a spacing of 15 feet on center.

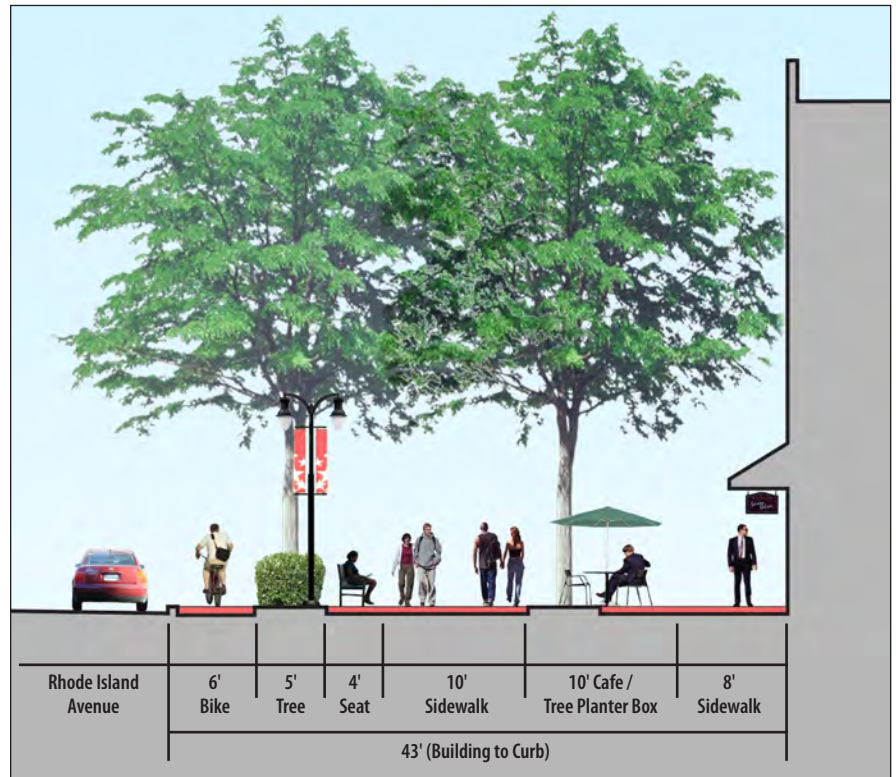


FIGURE 33: PROPOSED CONDITIONS—NORTH SIDE OF RHODE ISLAND AVENUE, LOOKING WEST.

### Guidelines (South Side of Rhode Island Avenue)

1. The parking lane width should be reduced to eight feet in order to make room for a six-foot wide cycle track and a five-foot wide planting zone.
2. At all of the corners of the intersection of 33rd Street and Rhode Island Avenue, curb extensions should be installed to minimize pedestrian crossing distances.

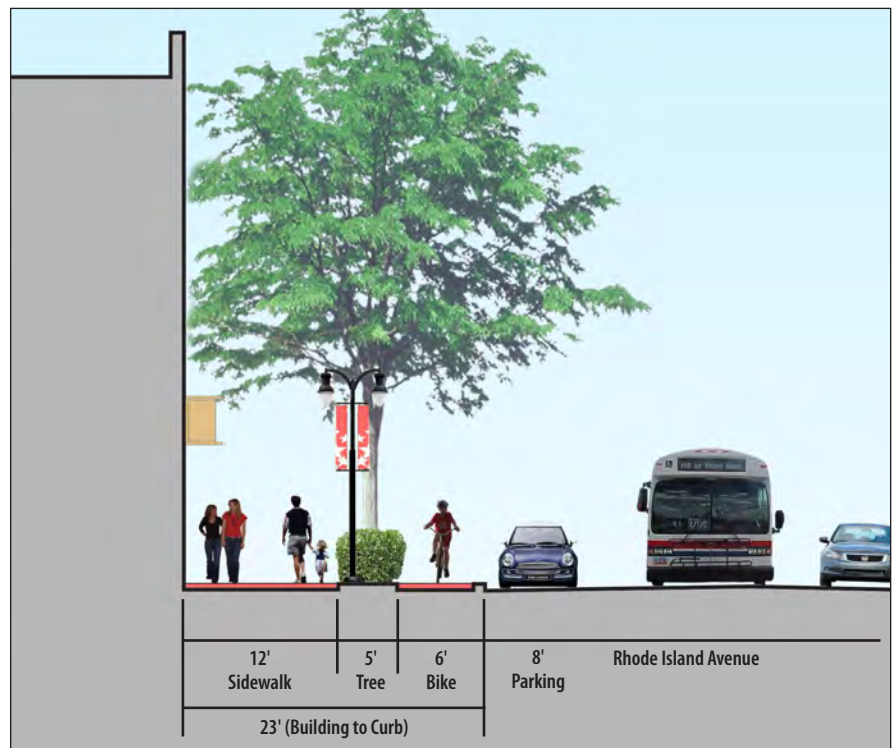


FIGURE 34: PROPOSED CONDITIONS—SOUTH SIDE OF RHODE ISLAND AVENUE, LOOKING WEST.



Public Space | *Street Plan—Rhode Island Avenue*

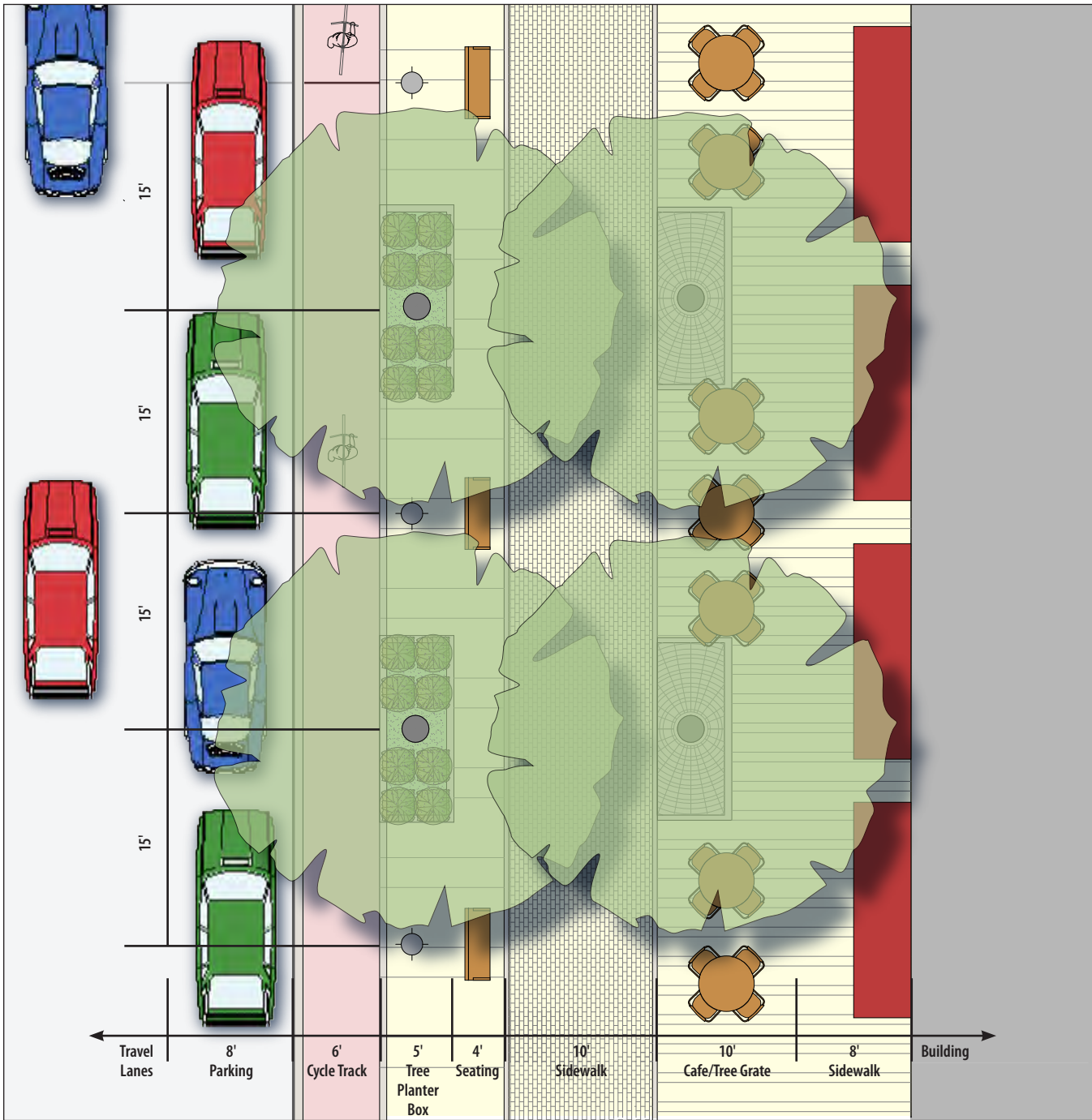


FIGURE 35: PROPOSED STREET PLAN—RHODE ISLAND AVENUE, NORTH SIDE.

## Public Space | *Pedestrian Crosswalks and Zones*

### Guidelines

1. A new crosswalk should be installed on Rhode Island Avenue, just east of Eastern Avenue, in order to complete the network of crosswalks at this intersection.
2. A crosswalk should be installed on Rhode Island Avenue, just east of 33rd Street, along with a pedestrian-activated crosswalk signal. The design of this particular crosswalk should incorporate stamped concrete that provides texture and color to emphasize the new pedestrian crossing location.
3. The reconfigured intersection of 34th Street and Bunker Hill Road should have a series of stamped concrete crosswalks on all approaches to the intersection in order to identify it as a pedestrian-friendly location. An alternative reconfiguration may consist of a raised intersection where the crosswalks and the center of the intersection are located at a higher level than the streets approaching the intersection. This treatment may cause the motorists to slow down through the intersection due to the change in grade.

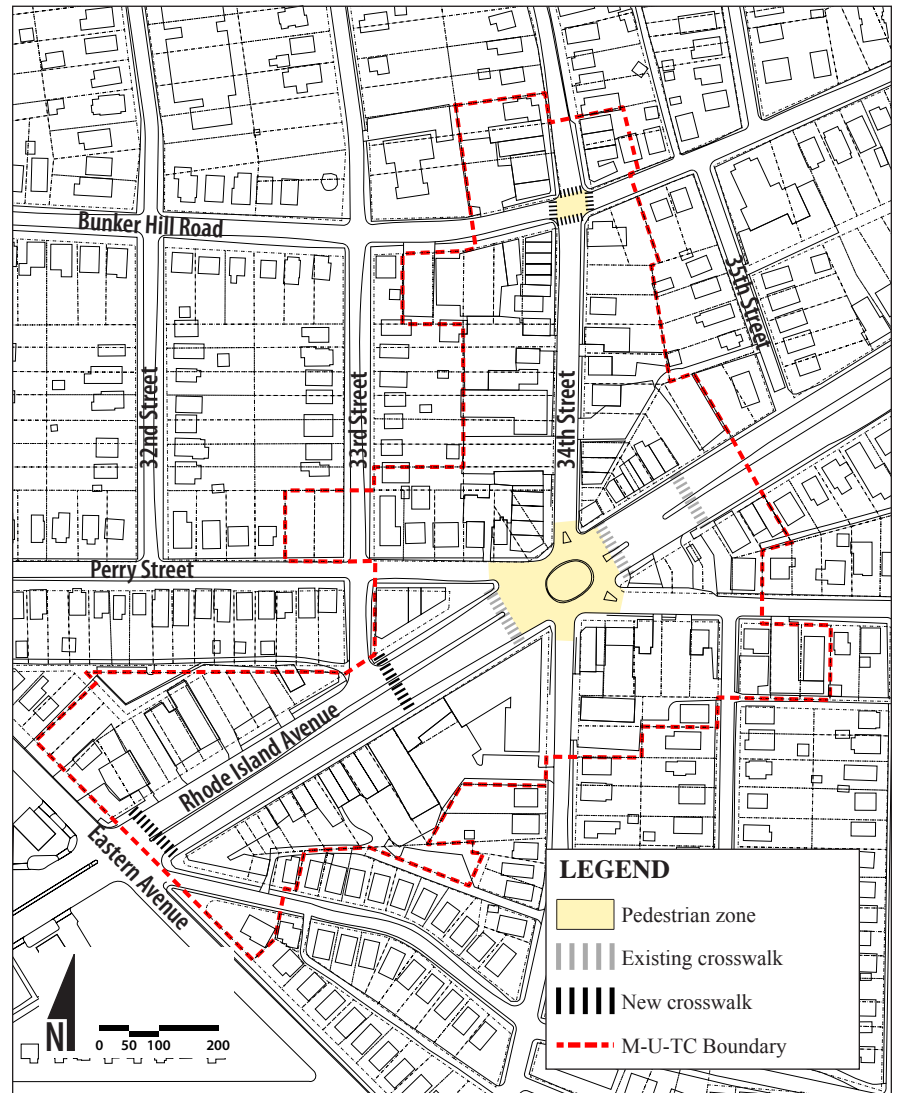


FIGURE 36: M-U-TC PEDESTRIAN ZONES AND CROSSWALKS.



EXAMPLE OF A PEDESTRIAN ZONE.



EXAMPLE OF A PEDESTRIAN CROSSWALK.



## Public Space | *Parking*

### Standards and Guidelines

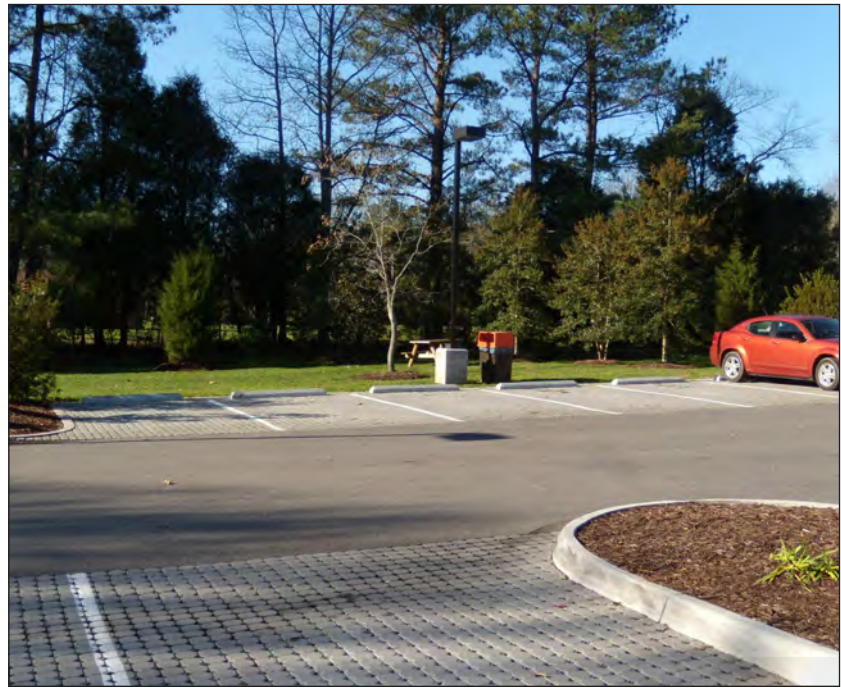
**1. *Multifamily Residential Development***

The minimum number of off-street parking spaces shall be 1.0 spaces per new dwelling unit plus .75 spaces per bedroom in excess of one per unit.

**2. *Non-residential Development***

The minimum number of off-street parking spaces shall be 50 percent of the minimum number of required off-street parking spaces in accordance with Section 27-568(a) of the Zoning Ordinance.

**3. Parking shall not be located between buildings and the street, including buildings on corner lots.**



AN EXAMPLE OF A SURFACE PARKING LOT WITH PERMEABLE PAVING.



FENCE AND LANDSCAPE STREET SCREENING FOR PARKING AREAS.

Public Space | *Parking*

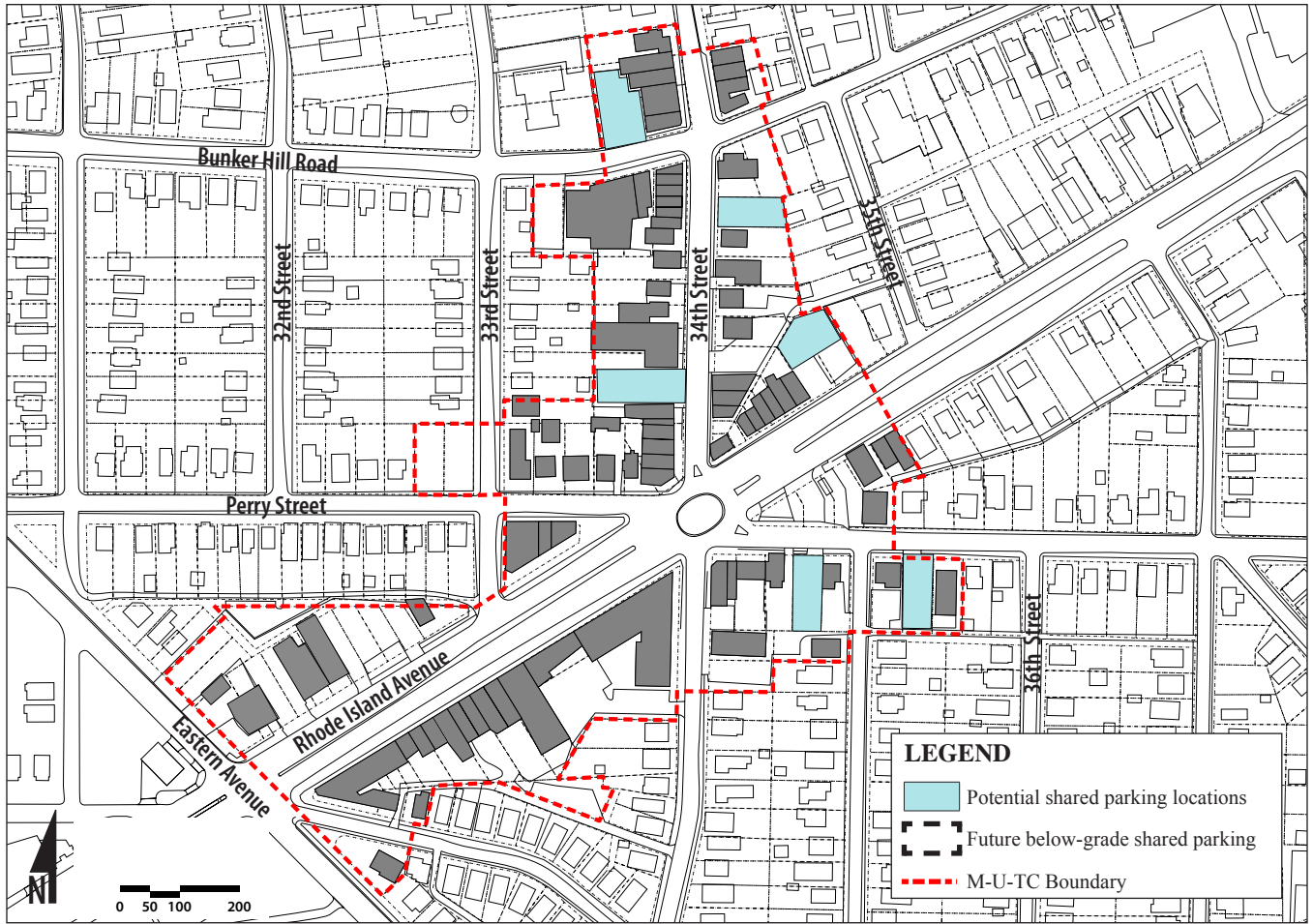


FIGURE 37: POTENTIAL SHARED PARKING LOCATIONS.

4. Loading and service areas shall not be visible from the street.
5. Parking surfaces shall be paved with high-quality, durable, and permeable pavers.
6. Surface parking lots shall be screened with walls, fences or hedges. Walls and/or fences must be a minimum of three and maximum of four feet in height. Hedges must be a maximum of four feet in height. Hedges must be trimmed to allow drivers to have clear visibility. If landscaping is used, hedges must be at least two feet in height at the time of planting.
7. Visible, easy-to-use wayfinding signs shall be used to direct drivers to shared parking lots.
8. Existing parking meters should be replaced with multispace pay-on-foot parking meters. Parking meters that incorporate solar power as an energy source may be used.
9. The City of Mount Rainier should work with M-NCPPC staff and property owners within the M-U-TC to analyze future parking needs in the M-U-TC area and to develop a shared parking strategy for those needs.
10. Drive aisles may be paved with asphalt (optional).



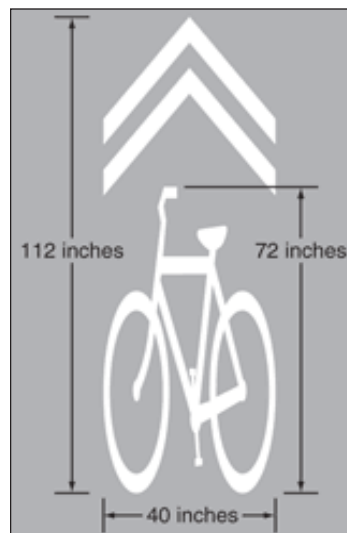
## Public Space | *Bike Facilities*

### Guidelines

1. Planned bicycle facilities should be incorporated in the circulation patterns on 34th Street north of the Rhode Island Avenue traffic circle and on Rhode Island Avenue between the traffic circle and Eastern Avenue.
2. The bicycle route for 34th Street should be shared with vehicles in both directions, given right-of-way constraints. Appropriate pavement markings and associated signage should be placed along 34th Street and should be in compliance with the Manual on Uniform Traffic Control Devices (MUTCD).
3. Bicyclists should be accommodated through the traffic circle with the latest State Highway Administration guidelines, standards, and safety practices.
4. On both sides of Rhode Island Avenue, the bicycle facility should be incorporated into the sidewalk as a separate six-foot wide bicycle track adjacent to the curb and on-street parallel parking.



EXAMPLE OF A CYCLE TRACK.



SHARROW MARKING SPECIFICATIONS.



R4-11

## Public Space | *Sidewalks and Street Trees*

### Sidewalk Standards and Guidelines

1. **Sidewalks shall be made of durable, attractive materials.**
2. Decorative paving materials, such as brick, precast, and granite pavers, should be used.
3. Specialty art paving patterns should be installed on sidewalks in areas with high visibility.

### Street Tree Standards

1. **Street trees shall be located along both 34th Street and Rhode Island Avenue. (Please refer to the street plans on pages 66 and 70 for appropriate spacing.)**
2. **Low maintenance tree species with large canopies shall be used. The following chart outlines recommended maple, ash, oak, and linden trees. More trees can be found in the 2010 *Prince George’s County Landscape Manual*.**



SIDEWALK AND STREET TREE ILLUSTRATION.

RECOMMENDED STREET TREES (SHADE)	
Species Type/Name	Scientific Name
Maple Trees: <ul style="list-style-type: none"> <li>• Red Maple</li> </ul>	<ul style="list-style-type: none"> <li>• <i>Acer rubrum</i> cultivars</li> </ul>
Ash Trees: <ul style="list-style-type: none"> <li>• Autumn Purple White Ash</li> <li>• Blue Ash</li> </ul>	<ul style="list-style-type: none"> <li>• <i>Fraxinus americana</i></li> <li>• <i>Fraxinus excelsior</i></li> </ul>
Oak Trees: <ul style="list-style-type: none"> <li>• Scarlet Oak</li> <li>• Willow Oak</li> <li>• Red Oak</li> <li>• Black Oak</li> </ul>	<ul style="list-style-type: none"> <li>• <i>Quercus coccinea</i></li> <li>• <i>Quercus phellos</i></li> <li>• <i>Quercus rubra</i></li> <li>• <i>Quercus veluntina</i></li> </ul>
Linden Trees: <ul style="list-style-type: none"> <li>• American Linden</li> <li>• Greenspire Littleleaf</li> <li>• Crimean Linden</li> <li>• Silver Linden</li> </ul>	<ul style="list-style-type: none"> <li>• <i>Tilia americana</i></li> <li>• <i>Tilia cordata</i> “Greenspire”</li> <li>• <i>Tilia euchlora</i></li> <li>• <i>Tilia tomentosa</i></li> </ul>



## Public Space | *Street Lamps*



34TH STREET.



RHODE ISLAND AVENUE.



CIVIC CORE/CITY CENTER.

### Standards and Guidelines

1. **New street lamps on 34th Street shall use the existing Mount Rainier lamp standard:**

**Pole—Phillips HADCO P20 or P21 series, with fluted column (black).**

**Lamp—Phillips HADCO Hagerstown or equivalent.**

2. **A double-headed lamp design, based on the existing lamp standard, shall be used along Rhode Island Avenue and around the civic core.**
3. **New lamps shall not exceed 15 feet in height.**
4. New lamps should provide appropriate levels of illumination (see street plans).
5. Energy-efficient lights should be used for all public lighting in order to conserve energy and reduce long-term costs.

## Public Space | *Public Art*



### Guidelines

The City of Mount Rainier is one of the four municipalities that comprise the Gateway Arts District. Public art enhances the experience of our public spaces and promotes the identity of a community.

1. Proposed infill development should provide public spaces for the inclusion of public art amenities. If public art or art space is included as an element of a project, then the M-U-TC Design Review Committee should review the art proposal, incorporating input from Mount Rainier area artists, and make recommendations to the M-NCPPC's Prince George's County Planning Department and the Planning Director regarding the appropriateness of the new art-related proposal as a part of the project.
2. Public art should be included as an integral part of any new Capital Improvement Program (CIP) projects by the city, the county, and the state within the M-U-TC area. Infrastructure projects, such as new bus shelters, a civic green stage, and/or a sculpture within the traffic circle may be a few potential projects that could be initiated through creative collaboration. City, county, and state staffs should work with the M-U-TC Design Review Committee and the community to identify appropriate locations for public art in the early stages of the CIP process.
3. The process to select artists' work may include open competition, competition by invitation, or direct selection based on a body of work and credentials.



# Site Design Standards and Guidelines

## Site Design | Uses and Retail Focus

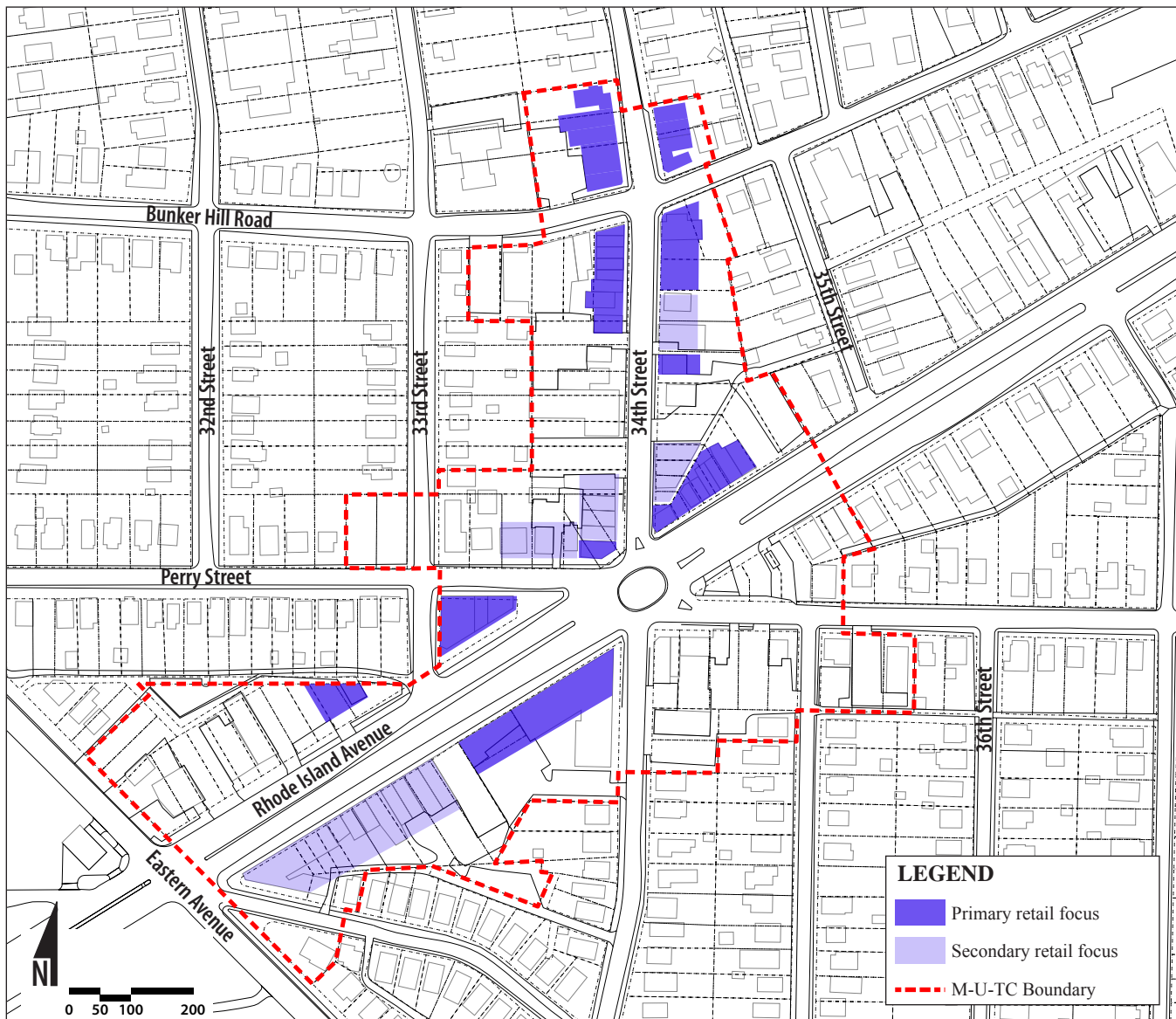


FIGURE 38: PRIMARY AND SECONDARY RETAIL FOCUS AREAS.

### Standards and Guidelines

1. Buildings in the M-U-TC area shall have appropriate uses, as listed under “uses permitted” in the 2004 *Approved Sector Plan and Sectional Map Amendment for the Prince George’s County Gateway Arts District* (see Appendix pages 158–178). The M-U-TC area is located in the “town center” character area classification of that plan.
2. Properties with a primary retail focus should have pedestrian-scale storefronts facing the public realm and are recommended for active retail uses, such as restaurants and stores.
3. Properties with a secondary retail focus areas should have small scale offices and shops and may not necessarily require active storefronts.

## Site Design | Sustainability



EXAMPLES OF RAIN GARDENS.

### Guidelines

1. Roof-mounted photovoltaic or solar hot water panels should be encouraged to reduce grid energy use. Photovoltaic panels should be installed as close as possible to the ideal angle for maximum solar gain. As feasible, panels shall not be visible from the street.
2. All lighting should use high-performance or LED lighting systems.
3. Leadership in Energy and Environmental Design (LEED) standards, as set forth by the U.S. Green Building Council (USGBC, [www.usgbc.org](http://www.usgbc.org)), should be reviewed and integrated into design and construction processes for all infill development and renovation projects, where this does not negatively impact historic character.
4. Awnings and overhangs should be placed and mature trees should be planted to shade south-facing glazing. When possible, fenestration should be aligned on opposite sides of buildings to facilitate cross-ventilation.
5. The use of green materials should be used in both the exterior and interior finishes of buildings. These include recycled or salvaged materials, rapidly renewable materials, and local materials.
6. Drainage swales, cisterns, rain gardens, and other devices should be used to capture slow water runoff.
7. Site grading, paving and planting should be done in a manner that minimizes off-site stormwater runoff.
8. Drought resistant or slow-growing hardy grasses, native plants, shrubs, ground covers, and trees should be used throughout the M-U-TC area.
9. Captured rainwater or building greywater should be used as an alternative to lawn irrigation systems. The use of potable water should be avoided in permanent irrigation systems.



Site Design | Orientation, Build-to Line and Frontage

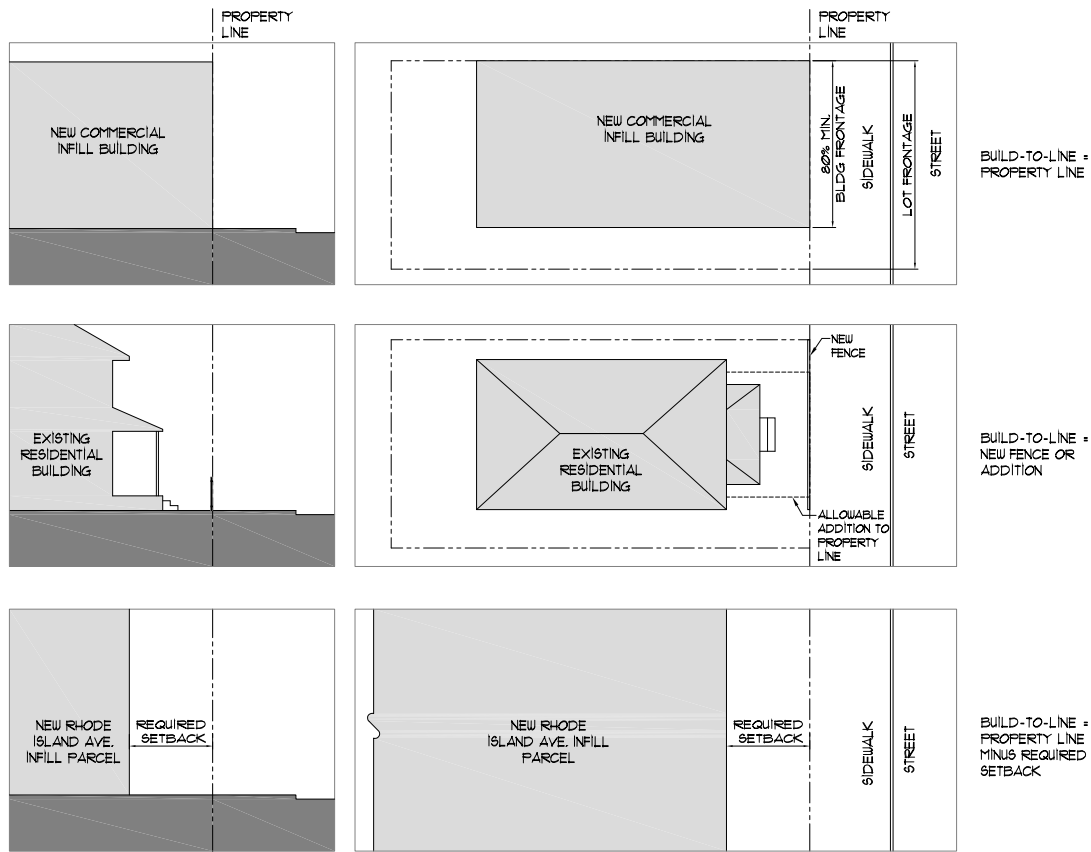


FIGURE 39: ORIENTATION OF NEW BUILDINGS.

Standards and Guidelines

1. **New buildings shall face 34th Street, Perry Street, and Rhode Island Avenue. Buildings in downtown Mount Rainier shall create a consistent street wall that provides a sense of enclosure.**
2. **Buildings with frontage on both 34th Street and Rhode Island Avenue must have two primary façades.**
3. **For a new building without a required setback, the build-to line shall be at the property line directly abutting street right-of-way.**
4. **All new buildings shall front on the build-to line. The building's front façade shall cover 80 percent minimum of the total build-to line.**
5. The maximum percentage of lot coverage for new infill development with the M-U-TC area should be 90 percent of the net lot area. The distribution of lot coverage on a specific property should consider the relationship of the proposed building's bulk and massing to adjoining existing properties, particularly residential uses. Applicants should demonstrate through appropriate site studies that the new infill development will not negatively impact adjoining properties and land uses.
6. Properties with no alley access may have a side setback to allow vehicle access.
7. Existing residential buildings should have fences at the build-to line. Front building additions may extend to the build-to line, as long as they are sensitive to the existing structure and do not conceal the architectural integrity of the building.

## Site Design | Height

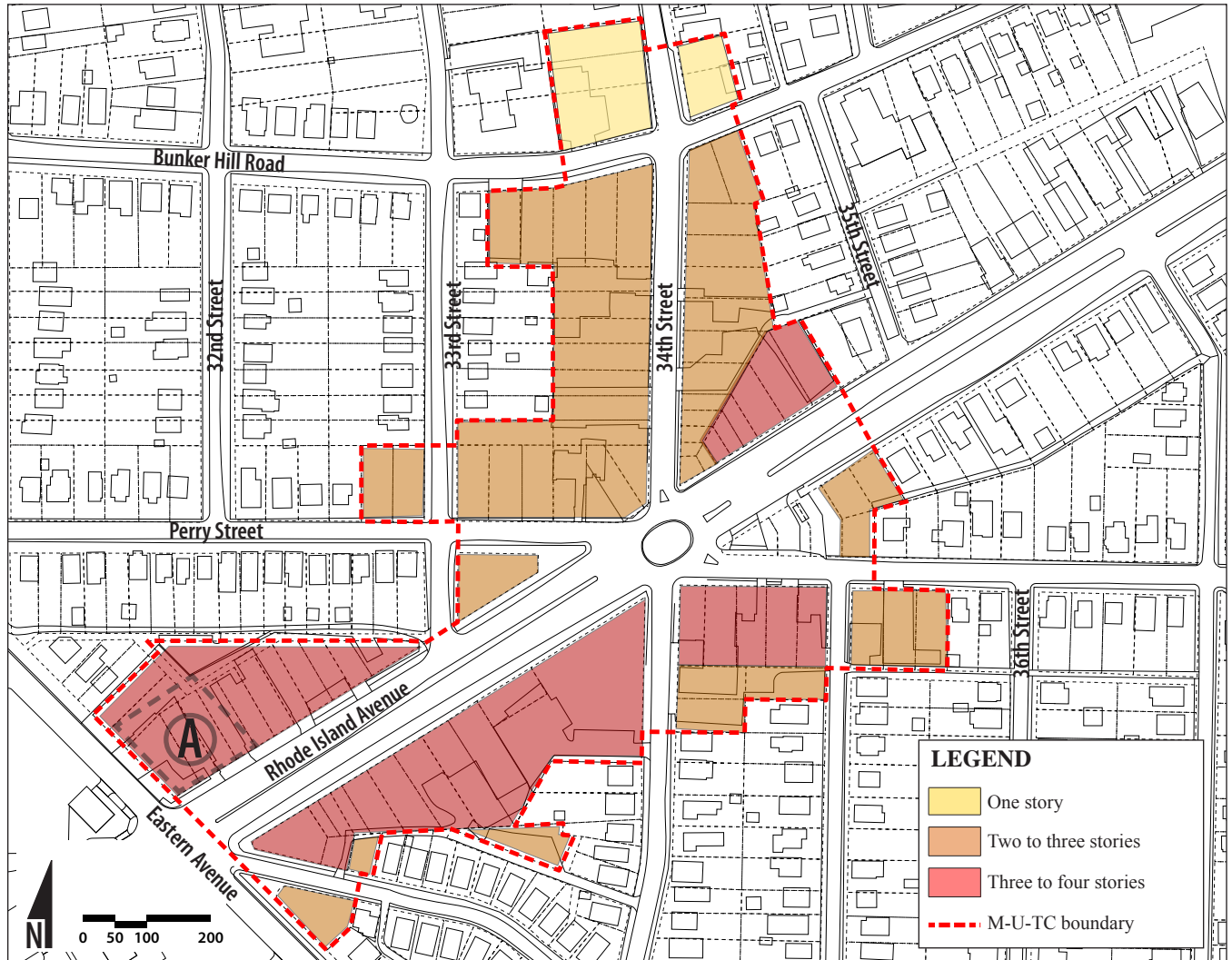


FIGURE 40: MAXIMUM BUILDING HEIGHTS WITHIN THE M-U-TC BOUNDARY AREA.

### Standards and Guidelines

1. **First stories shall be a maximum 15 feet floor to floor (including properties with ground floor retail).**
2. **Additional stories shall be a minimum 10 feet to maximum 12 feet floor to floor.**
3. The building at the corner of Site A may have five stories as shown on the diagram above.



## Site Design | *Setback*

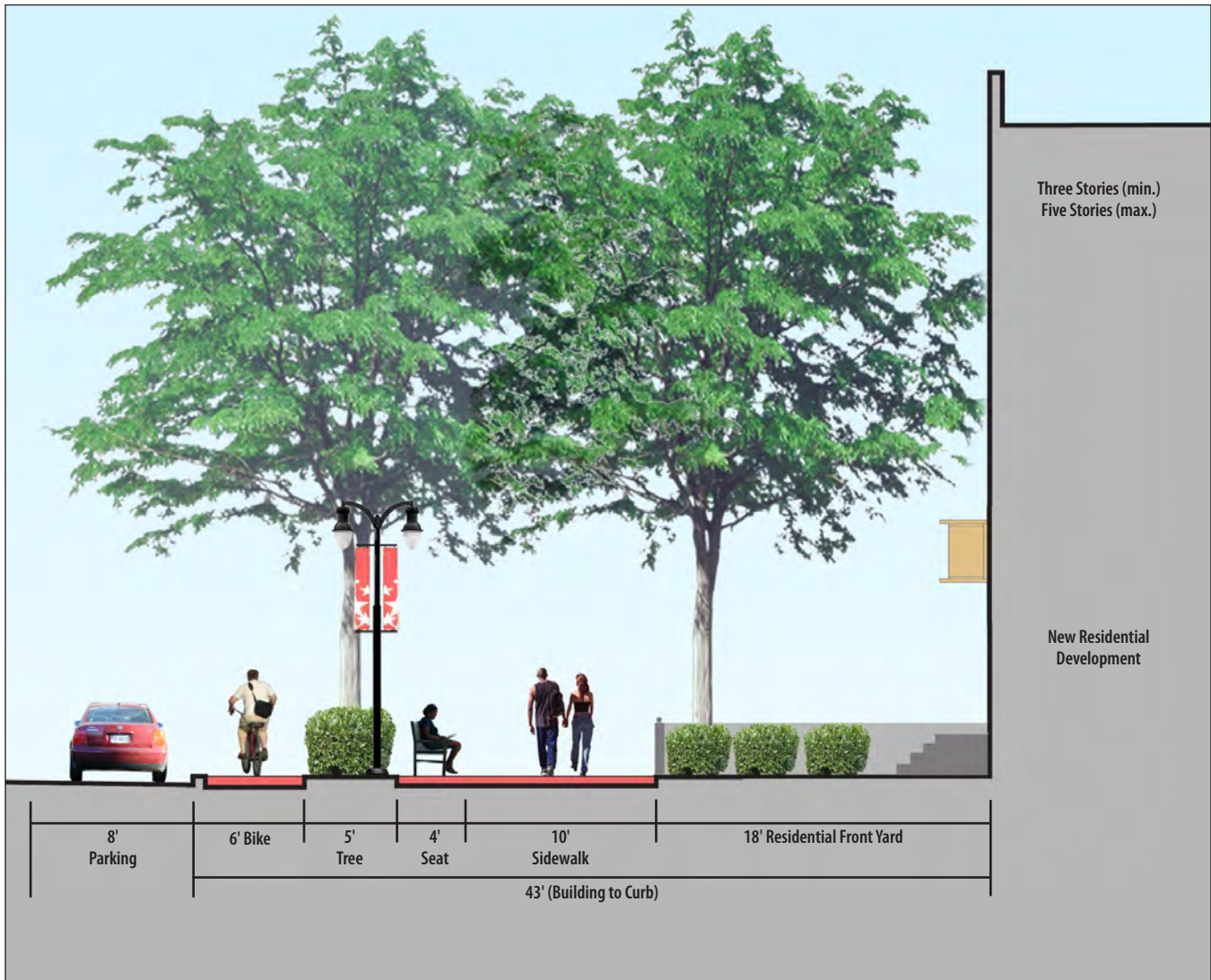


FIGURE 41: PROPOSED STREET SECTION AT RHODE ISLAND AVENUE ADJACENT TO NEW INFILL RESIDENTIAL, LOOKING WEST.

### Standards and Guidelines

1. **Main entrances shall be oriented towards the street.**
2. **A continuous building wall has the effect of defining the built environment and the public realm of the street. Consequently, infill buildings shall maintain the established setback of the area and shall be built flush with the front façades of adjacent existing buildings.**
3. Exceptions to the setback may be made for pedestrian-oriented spaces, such as sidewalk cafés and/or former residential units with existing front yards, which contribute to the quality and character of the street.

Site Design | *Rhode Island Avenue Infill Site*



FIGURE 42: PROPOSED SITE PLAN AT THE RHODE ISLAND AVENUE NEW INFILL RESIDENTIAL SITE.

Standards and Guidelines

1. Potential infill buildings on the north side of Rhode Island Avenue, between Eastern Avenue and 34th Street, shall maintain the setback of the existing buildings found between 3300 and 3310 Rhode Island Avenue.
2. New development on this site shall utilize fence and landscape buffers to the north side, along the rear lot line.
3. New development between 3200 and 3220 Rhode Island Avenue may have a residential garden-type wall constructed on the property line along Rhode Island Avenue.



## Site Design | Walls, Fences, and Screening



EXAMPLE OF FENCING THAT DELINEATES THE EDGE OF THE PUBLIC REALM.

### Standards

1. Walls and fences shall be a minimum of three feet to a maximum four feet in height.
2. Walls and fences shall be made of durable and attractive natural materials, such as brick, wood, and wrought iron. Walls and/or fences must be maintained by property owners, so that they do not fall into disrepair (i.e., chipped paint, crumbling walls, etc.).
3. Unclad cinder block, chain-link fencing, barbed wire, corrugated metal, fiberglass, sheet metal, and wire mesh shall be prohibited.

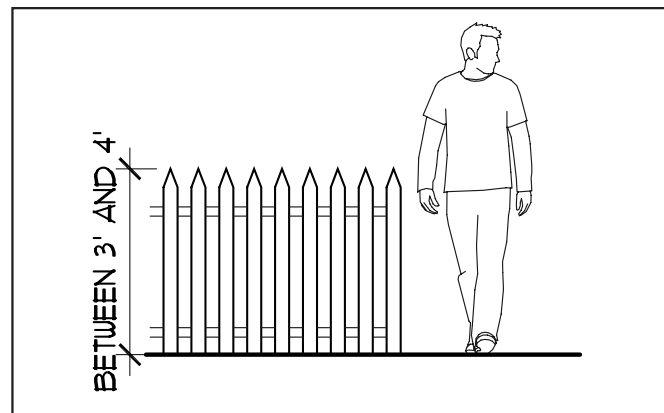


FIGURE 43: DIAGRAM SHOWING FENCE HEIGHT.

## Site Design | *Outdoor Seating*



EXAMPLE OF AN OUTDOOR SEATING AREA ON THE SIDE OF A RESTAURANT IN THE DEL RAY NEIGHBORHOOD OF ALEXANDRIA, VIRGINIA.

### Guidelines

1. Businesses, especially restaurants, are encouraged to provide outdoor seating.
2. Outdoor seating areas should be placed in informal open spaces in front of, beside, and between buildings. These areas may be enclosed by walls, fences, and/or plantings.
3. Outdoor furniture should be durable, high-quality, easy to maintain, and designed to withstand outdoor use. Furniture color and materials should be compatible with building color and materials.

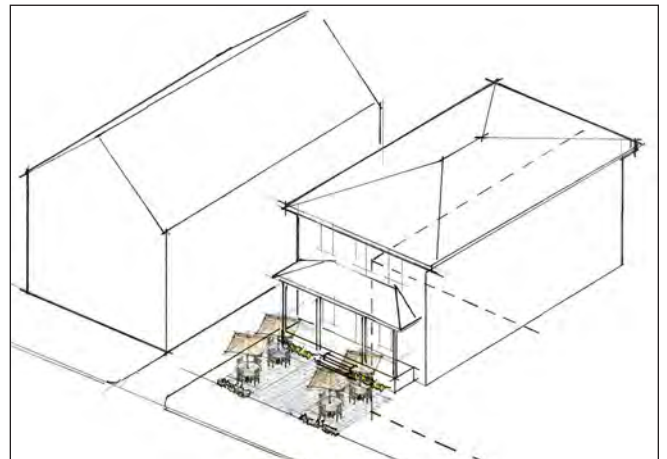


FIGURE 44: PROPOSED OUTDOOR SEATING IN FRONT OF BUILDINGS WITH A SETBACK.



## Site Design | *Accent Lighting*



AN EXAMPLE OF DECORATIVE UPLIGHTING TO EMPHASIZE A WALKWAY IN FLORIDA.



TREE LIGHTING OF A STREETScape IN LEWES, DELAWARE.

### Standards

Accent lighting may add special character to the nighttime appearance of the M-U-TC area. It may be used to illuminate a building entrance, pedestrian walkway, or advertising. It may also highlight special landmark buildings.

1. **A coordinated lighting plan shall be submitted for review with building plans. Lighting plans shall address security and safety issues.**
2. **High intensity security lighting fixtures shall be directed away from adjoining properties and public right-of-ways.**
3. **Lighting fixtures shall be energy efficient and in scale with the height and use of the structures.**

4. **Lighting color, intensity, and fixture design shall be compatible with building features.**



## Site Design | *Utilities and Mechanical Equipment*



EXAMPLES OF WALLS AND LANDSCAPING USED TO CONCEAL MECHANICAL EQUIPMENT.

### Standards and Guidelines

Unsightly mechanical equipment, dumpsters, satellite dishes, and other visually obtrusive mechanical equipment can detract from the appearance of the M-U-TC area.

1. **Dumpsters, exterior trash and storage areas, service yards and loading areas, transformers, HVAC/air conditioning units, and other utility/mechanical equipment shall be concealed from view of nearby streets and adjacent structures with landscaping and/or screen walls and in a manner that is compatible with the building and site design.**
2. Air conditioning units protruding from windows obscure storefronts. Units should be placed on the side or back façades whenever possible. If a storefront location cannot be avoided, the unit should be mounted flush with the exterior wall surface.
3. Satellite dishes or antennas shall be located on the least visible areas of buildings, and if located on roofs, the elements shall be moved back from the roof edges and out of view from the street.
4. Mechanical equipment should be screened with rooflines, walls, fences, awnings, canopies, and parapets whose design and material is compatible with the style of the building.





# Architectural Elements Standards and Guidelines

## Architectural Elements | *Façades and Fenestration*



50% GLASS  
MINIMUM



EXAMPLE OF A STOREFRONT WITH LARGE FIRST-FLOOR DISPLAY WINDOWS.

### Standards and Guidelines

Building façades provide a human scale and “Main Street” atmosphere to commercial areas. Multiple buildings may share common façade elements. Some of these elements are a first floor with large display windows and recessed entrances; an upper façade with more wall and smaller regularly spaced windows, or in the case of one-story buildings, a wide wall band; and a detailed cornice, pediment, or roof that crowns the building. Building alterations shall respect and incorporate these elements.

1. **Building alterations shall preserve and restore original historic details, such as openings, cornices, lintels, and arches.**
2. **Storefronts shall be emphasized by large windows and decorative wall band at their top.**
3. Blank walls should be discouraged because they disrupt the street pattern and make it uninviting.
4. Upper story windows should have a repeated pattern that ties together the façade. Windows and doors should be aligned and uniform window sizes used to bring order to the façade.
5. The size and proportion of windows and door openings should reflect the historic character of the M-U-TC area and be compatible with adjacent façades.
6. If a window has deteriorated or is missing, the replacement should match the original. For example, a double-hung sash window should not be replaced by a single pane of glass. Existing openings should never be blocked or filled in with a stock window. The replacement materials should match original windows.
7. New or enlarged windows or doors should be chosen after careful consideration of façade composition, scale, and materials of the original building.

## Architectural Elements | Storefronts



EXAMPLE OF A STOREFRONT IN GEORGETOWN.

### Standards and Guidelines

Storefronts built from 1900 to 1940 featured large storefront windows, wood panels below the windows and transom windows above. Doors were generally wood and glass. Surviving wood-framed store windows are rare and should be valued and restored. Recessed entries were also typical and should be preserved since they provide a place for the door to swing out without danger of hitting a passerby.

1. **Façades with storefronts shall have a minimum 50 percent windows with clear glass at the ground floor.**
2. **Storefront windows shall extend to at least eight feet above the sidewalk. The window base shall not exceed 30 inches in height.**
3. **Mirrored, dark, or heavily tinted glass shall not be permitted.**
4. **Storefront windows shall not be bricked or paneled.**
5. **Existing bars and grills on windows shall be phased out over time.**
6. Whenever possible, storefront restoration should return the façade to its original character and reopen windows and transoms.
7. Replacement display windows should be large, single- or multi-paned openings allowing views into the window display area.
8. Creative and colorful display of merchandise should be encouraged.



## Architectural Elements | Security



PERFORATED SECURITY SCREEN WITH CLEAR VIEWS INTO INTERIOR SPACES AT NIGHT.



EXAMPLE OF PROHIBITED SOLID ROLL GATES.

### Standards and Guidelines

1. **Metal bars and solid metal rolling gates shall not be permitted on windows or doors. Existing solid gates shall be phased out over time.**
2. **Storefronts shall provide clear views of interior spaces lit from within at night.**
3. For interim security needs, perforated rolling gates may be used as a security device.
4. Chain-link fences and barbed wire shall not be allowed as a security measure.

## Architectural Elements | *Building, Color, Lighting, and Materials*



EXAMPLE OF COLOR, LIGHTING, AND MATERIALS WORKING TOGETHER TO CREATE A UNIFIED FAÇADE.

### Building Color Standards

1. **Colors shall enhance the original character of the buildings. Buildings shall use a limited palette of paint colors (no more than three: one base color and one or two colors for trim or accents).**
2. **Walls with artwork and murals shall be exempt from color standards.**

### Building Lighting Standards and Guidelines

1. **Lighting fixture placement and type shall be integrated into the overall design of the project and unify the building composition.**
2. **Colored lighting shall be used to complement and highlight buildings, signs or public space while achieving harmony with street lighting in the downtown area.**

3. **Lighting fixtures should carefully blend in with building architecture and highlighting architectural features, such as cornices and window and door openings.**

### Commonly Used Materials Standards and Guidelines

1. **Natural wood or composite equivalent (e.g., hardiplank) shall be permitted. Synthetic siding (e.g., vinyl and aluminum) shall not be permitted.**
2. **Surfaces finished in stucco should be smooth and hand-troweled. Spray-on stucco finishes and exterior insulation and finish systems (EIFS) should not be used.**
3. **Walls should be made of brick or natural stone. Masonry should retain its natural color and not be painted over.**



## Architectural Elements | *Porches and Front Yards*



EXAMPLE OF A RETAIL USE WITH A FRONT PORCH.

### Standards and Guidelines

1. **Dumpsters, HVAC units, and other utility equipment shall be completely screened and not visible from public sidewalks.**
2. Porch additions should enhance and blend with building architecture.
3. Open space between the lot line and building face should be enclosed by walls, fences or vegetation.
4. For commercial/retail uses, freestanding, pedestrian scaled signage may be placed in front yards.

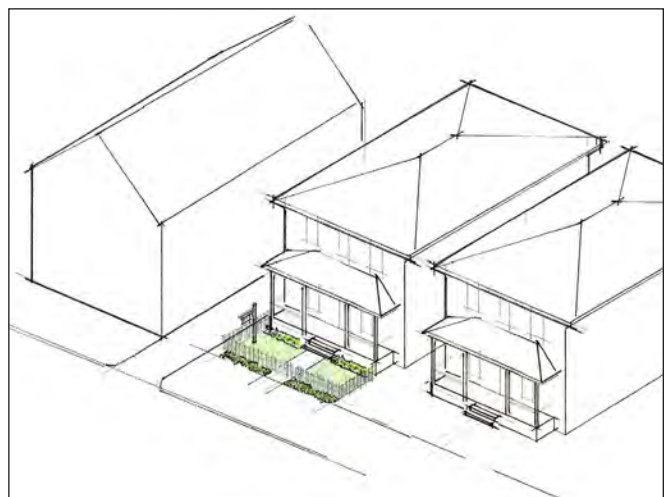


FIGURE 45: PROPOSED FRONT YARD IMPROVEMENTS.

## Architectural Elements | *Additions and Subtractions*



VISION FOR AN ADDITION TO THE EASTERN STAR BUILDING.



EXAMPLE OF A MODERN ROOFTOP ADDITION TO A BUILDING.

### Guidelines

It is important to consider the effect that an addition's location, size, and exterior appearance will have on an existing building. When deciding where to locate an addition, its visibility from a public right-of-way, the importance of the elevation to which it is attached, and the effect it will have on the overall form and character of a historic building should be carefully considered. The height, width, proportions, rhythm of windows and doors, roof shape, ornamentation, projections, and materials of the addition all contribute to its appearance.

In terms of subtraction, the removal of an addition should only be done after carefully considering its architectural and historic significance in consultation with M-NCPPC staff, the Historic Preservation Commission, and the M-U-TC Design Review Committee. Some additions do not clearly contribute to the original structure, and their removal will assist in revealing the architectural character of that original structure. In other cases, additions may have some historical significance, but may actually hinder the original buildings from contributing to the district in a more significant manner.

1. Additions should not be larger than the buildings on which the additions are placed.
2. Additions should not be from an architectural period prior to the original building.
3. If an addition to a building is removed, any damage done to the original building should be repaired or replaced using physical, photographic, or other documentary evidence.
4. If a front addition is compatible with the original building, it should be retained. If not, a front addition may be altered or removed with the proper approvals.
5. The most common type of existing additions found in Mount Rainier are those located on the rear of commercial buildings or houses. A contrasting rear addition may be acceptable if it is not visible from a public street or alley and when it does not destroy existing character-defining details, ornamentation, and materials of a rear elevation.



## Architectural Elements | *Additions and Subtractions*



**LEFT: KLEIN'S BAKERY BUILDING, CIRCA 1905.  
BELOW: THE BUILDING TODAY.**

6. A new rear addition that can be seen from a public street or alley should be compatible with the design of the rear elevation of the existing building. If the new addition is not visible from the street or alley, a less compatibly designed addition may be acceptable.
7. While not as common as rear or front additions, side additions may also be found on buildings. These include open side porches as well as enclosed rooms. Since side additions can usually be seen from the public street, side additions should be distinctive, yet compatible with, the design of the original buildings, respecting their characters without duplicating them exactly.
8. An existing side addition that detracts from the appearance of the original building due to its incompatibility in scale, proportions, rhythm, materials, or other design elements, may be altered or removed with the proper approvals.
9. Rooftop additions should be designed so that they are inconspicuous from the public rights-of-way.
10. Rooftop additions should be set back from the primary elevations of buildings and should not

damage character-defining features of historic buildings such as parapets or cornice work.

11. Rooftop additions are more compatible on buildings adjacent to taller buildings and generally should not be more than one-story minimum to two-stories maximum in height.
12. Although they should be differentiated as new elements, rooftop additions should still be compatible in scale, size of windows, orientation, detailing, roof slope, and materials to the original structure.



## Architectural Elements | *Former Residential with Non-Residential Use*



EXAMPLE OF A RESIDENTIAL BUILDING TYPE CONTAINING A NON-RESIDENTIAL USE.

### Standards and Guidelines

1. **Commercial buildings originally constructed as residences shall maintain their residential character.**
2. **Closing all or any portion of existing window and door openings shall be prohibited.**
3. **Façade upgrades shall not include aluminum or vinyl sidings that poorly imitate original materials.**
4. **Window openings above the first floor shall not be enlarged or altered.**
5. Window openings on front façades at the first floor level may be widened to create picture windows, as long as the new openings respect the existing proportions of the structures.
6. Front yards should remain as green space and or outdoor seating areas with appropriate landscaping.
7. New porches and stoops should be encouraged, and the enclosing of front porches and stoops should be discouraged, except when the design of these elements retains the open appearance of the porches/stoop.



## Architectural Elements | *Historic Buildings*

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### Standards and Guidelines

Historic designation is an important planning tool that may serve as a way to protect neighborhoods from unmanaged change. Maintenance of existing materials, elements, and forms are the best method of preserving the character of historic buildings. However, regardless of how well they are maintained, most historic buildings will eventually require repair.

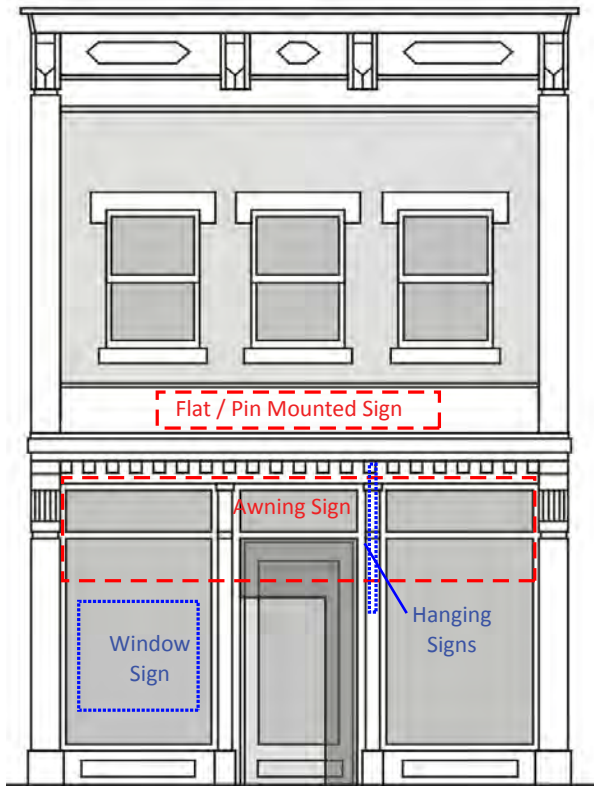
1. **Architectural features that define historic buildings shall be preserved. Any alterations that affect the appearances of historic buildings or their landscapes shall be done in a manner that does not detract from their character-defining features, their sites, or neighborhoods within the M-U-TC area.**
2. **To the extent possible, inconsistent layers of siding, signage, and incompatible additions to storefronts and building façades shall be removed in order to emphasize the design and materials of the buildings.**
3. **If feasible, repairs to a historic building shall be done so that original materials and elements remain intact. If, for technical reasons, replacement with compatible material proves not to be feasible, the building owner may then consider replacing the deteriorated material or element with a reasonable substitute material. If historical evidence is lacking, compatible scaled materials should be used to reconstruct architectural elements.**
4. **Substitute materials shall have the same appearance, size, shape, texture, color, and other defining characteristics as the original.**
5. **Substitute materials shall be physically and chemically compatible with adjacent materials, so that they do not cause future maintenance problems.**
6. Any additions to historic buildings should respect the proportions, as well as the detailing, of the original design and should use materials which are consistent with those used in the original buildings.
7. The gentlest procedures should be used for cleaning, refinishing, and replacing historic materials.
8. When the disassembly of historic elements is necessary for their rehabilitation, methods that minimize damage to original materials should be used.
9. Older alterations that have achieved historical significance should be preserved. More recent alterations that are not historically significant may be removed.
10. New construction within the historic district should have a mass and scale similar to historic structures in the area. The façades of these new buildings should align with the established setbacks of the area.
11. New construction should emphasize horizontal features that can align with other buildings.
12. New construction should use building materials that are similar to those employed historically for all major surfaces.

## Architectural Elements | Signage

### Standards and Guidelines

Signs should complement the architecture of buildings in terms of size, shape, color, texture, and lighting. Signage considerations include the name of a business and its address to temporary sales and product information.

1. All proposed flush mount, projecting, and window signage, as well as banners and flags, shall be included in the building sign plans submitted for review by the county and M-U-TC Design Review Committee. Signage plans shall specify color, size, type, and location of all signs on or adjacent to buildings.
2. All signs shall be attached to building façades. Signs shall be flat against façades or mounted projecting from façades. Buildings with existing front yards may also have free-standing bracket signs (as part of allowable gross signage area).
3. The maximum gross area of signage shall be one gross square foot of signage for every linear foot of building frontage. Total signage should not exceed 10 percent of the building's façade area (also see sign dimension restrictions).
4. Signs shall relate in color, size, and placement to building architecture. Signs shall not block, alter, or remove architectural details, such as windows, cornices, or decorative details, or extend above the roofline.
5. Signage submitted for multi-tenant buildings shall coordinate and present unified signage strategies.
6. Illuminated signs shall be designed to enhance the overall composition of façades. Sign illumination shall be limited to illuminate the signs only.



TYPE AND LOCATION OF SIGNAGE.

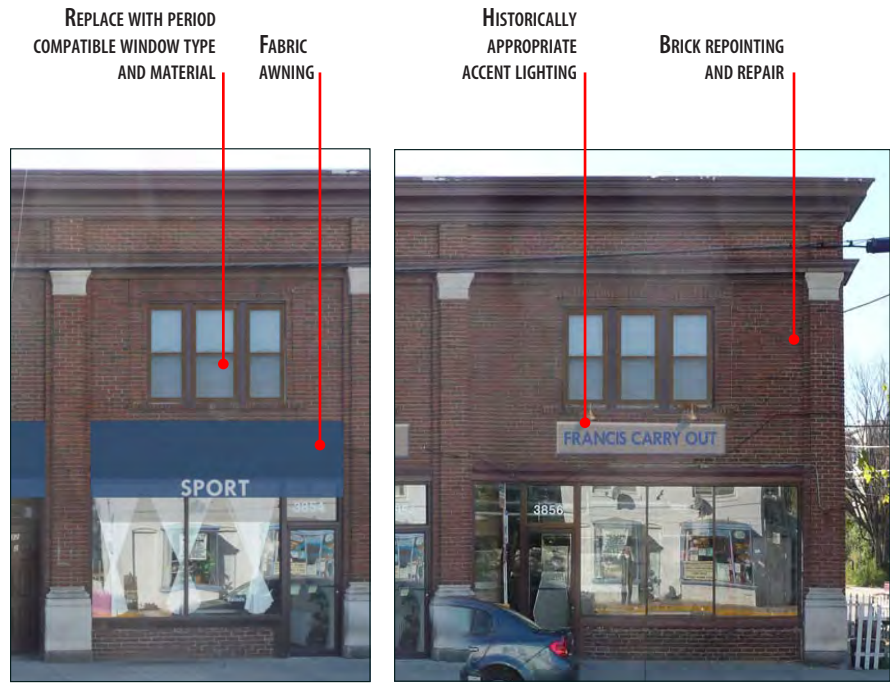


AN EXAMPLE OF HISTORICALLY-COMPATIBLE SIGNAGE.



## Architectural Elements | Signage

7. Backlit signs shall not be allowed, with the exception of channel letter signs with individually-lit letters and numbers.
8. Signs shall be made of attractive and durable materials, such as metal, wood, and glass. Signs shall be of professional quality and finish. Signs shall be properly repaired and maintained throughout the year.
9. Street numbers should be legible and located in one of the above-mentioned areas.
10. Street numbers located in transoms should be sized proportional to the openings.
11. Signs form an important visual element of urban streetscapes. Signs should play a secondary role and not detract from building architecture.
12. Signage is an integral component of a building's overall façade and should enhance a retail storefront and its architectural character.



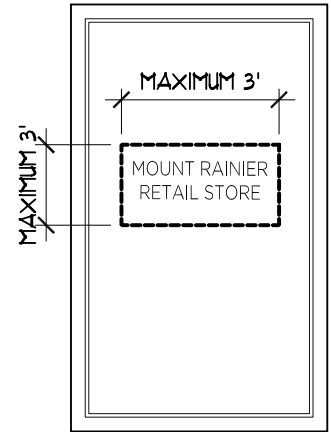
EXAMPLES OF SIGNAGE AND PRESERVATION FOR HISTORIC BUILDINGS.

## Architectural Elements | Signage



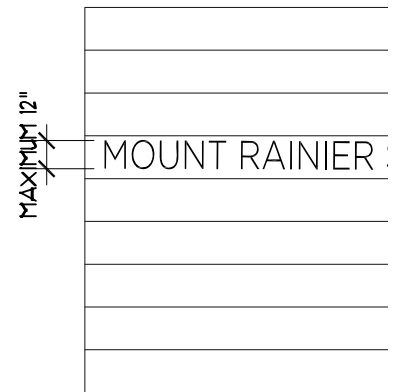
### Adhesive and/or Window Painted Signs

1. Adhesive signs shall not obscure views into the interior of retail establishments.
2. Window signs, including those advertising sales in any one window, shall not exceed 30 percent of the window area.
3. Text displaying web site addresses and phone numbers shall not exceed three inches in height.



### Pin-mounted Signs

1. The size and position of signs shall fit within architectural features.
2. Individual letter signs should be mounted on the front of the building. Fastening hardware shall not be visible from the street.
3. Pin-mounted letters should use clear, bold fonts.



### Hanging Signs

1. Hanging signs shall be suspended from brackets and mounted perpendicular to façades.
2. Hanging signs shall be hung at least 10 feet above the sidewalk, shall project no more than 42 inches from the face of buildings, and shall not exceed 9 square feet.
3. The size of a hanging sign shall be proportional to the building façade.



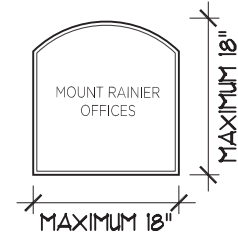


## Architectural Elements | Signage



### Plaque Signs

1. Signs shall be located adjacent to the main entrances to businesses.
2. Metal or cast metal should be used as materials for plaques.
3. Signs should be for professional offices and home-work studios.



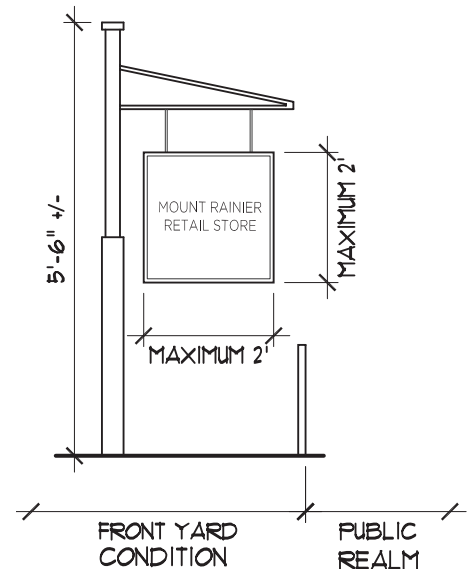
### Flat signs

1. Flat signs shall be mounted flush against the walls of buildings.
2. Flat signs shall respect building proportions and architectural features, such as bays and cornices.
3. Lettering height shall not exceed 12 inches.



### Free-standing Bracket Signs

1. Free-standing bracket signs shall only be utilized for buildings with existing setbacks and defined front-yard conditions.
2. Bracket signs shall be independent from buildings and mounted on wood or metal posts. Signs shall be pedestrian-scaled with a maximum post height of five foot- six inches.
3. Only bracket-type signs shall be used on freestanding posts.



## Architectural Elements | Signage



BANNER SIGNS.

### Temporary Signs

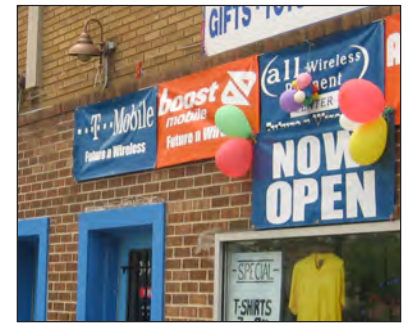
1. Temporary banner-style signs can be used as an interim low-cost option for new businesses in Mount Rainier. Temporary signs shall be reviewed by the M-U-TC Design Review Committee and will require permits. Temporary sign permits shall be valid for six months. By the end of the six-month period temporary signage will have to be replaced with properly approved permanent signage.



INTERNALLY-LIT PLASTIC SIGNS.

### Prohibited Signs

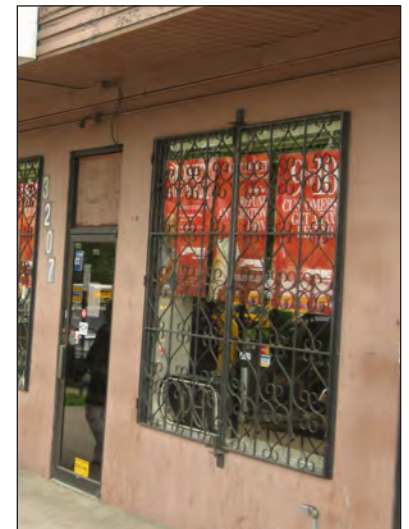
1. The following sign types are prohibited in the M-U-TC downtown area:
  - Animated, flashing or electronic scrolling signs.
  - Tall, free-standing, or oversized signs that visually overpower.
  - Pole-mounted signs.
  - Internally lit plastic signs or box signs.
  - Vinyl signs, except for temporary signage for which permits have been issued (see temporary signage above).
  - Window signs that block interior views.



MULTIPLE PERMANENT VINYL SIGNS.



POLE-MOUNTED SIGNS.



WINDOW SIGNS.



## Architectural Elements | Awnings



EXAMPLES OF AWNINGS.

### Standards and Guidelines

Awnings and canopies traditionally protect pedestrians and merchandise from the weather. They provide color and a three-dimensional quality to storefronts. Awnings and canopies can enliven utilitarian exteriors and tie together different buildings along a block. They can be used for signs if printed with the name, symbol, and street number of the store.

1. **Awnings shall be made of durable fabric and may be fixed or retractable. The design and color should complement building architecture and the historic character of Mount Rainier.**
2. **Metal, plasticized, synthetic, and back-lit awnings shall not be allowed.**
3. **A standard street awning shall be mounted with its valance between eight and twelve feet above the sidewalk, so it will not obstruct pedestrian flow. Awnings shall project out a maximum of four to six feet and are exempt from setback requirements.**
4. Logos, text and other signage should not cover more than 30 percent of the awning surface. This

signage surface area counts towards the allowable gross signage area of a façade.

5. The awning should be attached above the display windows and below the cornice or sign panel. Individual awnings should be installed over each separate opening.
6. The awning should emphasize the frame of the storefront and should not cover the piers or be too close to the upper floor window sills or the building's cornice.

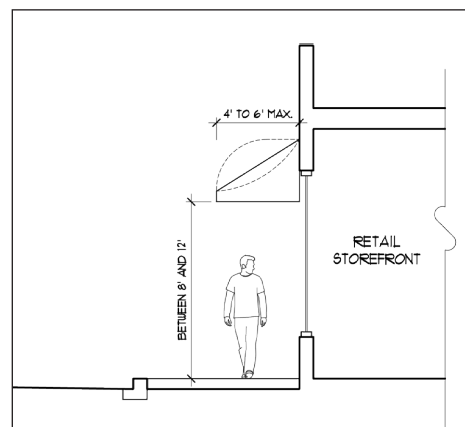


FIGURE 46: DIAGRAM SHOWING AWNING HEIGHT DIMENSIONS.



Implementation **5**



## Implementation

### Implementation Philosophy

No simple formula exists to revitalize the M-U-TC area. For the effective implementation of the development plan vision, a number of key stakeholders—which include local government, potential developers, existing businesses, and others in the private sector—will need to form new working relationships and coordinate initiatives.

Implementation should build on recent public- and private-funded activities. Past public initiatives have included the traffic circle, the tree-lined median, and the building at 3311 Rhode Island Avenue. More recent public sector investment has focused on the properties on the north side of Rhode Island Avenue between Eastern Avenue and 33rd Street as well as the Eastern Star Building. Recent private investment has focused along 34th Street and the 3300 block of Rhode Island Avenue. In the near-term, both public and private resources are limited. Therefore, it is important to focus projects in specific areas where stakeholders can create the biggest “bang for their bucks.”

However, even with limited budgets, small-scale, low-cost actions can be taken. Collectively, these actions can build upon existing successful revitalization efforts. Smaller projects may give way to larger ones and over time dramatic changes may be achieved.

### Implementation Precedents

Barracks Row and Takoma Park are two local examples of commercial areas that experienced revitalization over the past decade. In many respects, the issues faced by those two areas are similar to the issues the M-U-TC is facing today. Consequently, these areas can serve as case studies on how implementation may work in the M-U-TC area.

#### **Barracks Row, 8th Street S.E., Washington, D.C.**

In 1999 the Barracks Row Business Alliance teamed up with key citizens to form the Barracks Row Main Street organization. This group worked with the government and was awarded a District Department of Transportation (DDOT) grant in 2003 to create a comprehensive urban streetscape, which dealt with

urban design issues such as parking, public transit, water management, urban forestry and lighting. Since the program’s inception, Barracks Row has emerged as a highly successful and vibrant commercial corridor, and was awarded the 2005 Great American Main Street Award by the National Trust for Historic Preservation.

The Barracks Row Main Street organization has the following committees:

- *Organization Committee*: creates a five-year strategic plan, and organizes fund raising events.
- *Design Committee*: reviews façades, signage, streetscapes, and historic preservation applications.
- *Promotion Committee*: markets Barracks Row to new customers through image campaigns, retail promotions, and special events.
- *Economic Development Committee*: provides market analysis and marketing brochures and promotes communication between property owners and businesses.
- *Arts and Crafts Committee*: promotes the arts along 8th Street in order to create an environment conducive to more art galleries, antique dealers, design stores, and house wares.

Some of the accomplishments of the Barracks Row Main Street organization to date are as follows:

- In 2003 a DDOT streetscape project was launched.
- Public and private funds reinvested in the community have totaled \$19 million.
- Fifty-one façades have been restored.
- Forty signs have been replaced through private funding.
- Forty-three new businesses and 12 new outdoor cafés have opened, and 198 additional jobs have been created.
- Three new buildings have been constructed.
- A self-guided history trail has been created to facilitate tours of the neighborhood.

#### **“Main Street Takoma,” Takoma Park, Washington, D.C., and Maryland**

Main Street Takoma is a “Main Street” program that promotes and markets downtown Takoma Park businesses. Incorporated in 2008, the program has already recruited two new business tenants and is actively pursuing county, state, and federal grants.

The “Main Street Takoma” has the following committees:

- *Design Committee*: organizes clean-up days, window display competitions, plantings, signage, tree adoption, and façade improvements with matching grants from the State of Maryland Business Works program.
- *Economic Restructuring Committee*: does market analysis and manages a revolving loan fund, parking, and outreach.
- *Promotional Committee*: manages monthly events, marketing, and the organization’s web site.
- *Organization Committee*: controls membership, volunteering, and web site review.

Some of the accomplishments of the Main Street Takoma organization to date are as follows:

- A “Main Street Takoma” executive director has been hired.
- The 501(c)(3) organization has been set up, a board of directors established, and a web site launched ([www.mainstreettakoma.org](http://www.mainstreettakoma.org)).
- Two new tenants, Mayorga Coffee (open) and Ace Hardware (under construction) have been recruited.
- A retail market analysis, which was funded via a Community Legacy Grant, has been completed.
- A \$250,000 DC Arts and Humanities Grant (a three-year process) has been awarded for Art in Transit for an installation at the Takoma Park Metro Station.

Like other commercial centers in older urban residential areas, such as Barracks Row and Takoma Park, successful redevelopment in the M-U-TC will be built on investment in programs that promote streetscape and façade improvements, new development, and institutional support for ongoing management and branding of an area.

The Mixed-Use Town Center Zone Development Plan is a critical tool that should guide a unified approach to decision making in the M-U-TC area. All public-related initiatives should be measured and tested against the best interests of the entire M-U-TC. This will help to prioritize strategies and the allocation of limited public resources to achieve the most impact in the area. Some of the implementation priorities for the area are as follows:

1. Public investment should be concentrated on specific projects within the 34th Street, Rhode Island Avenue, and Civic Core districts. Reinforcing specific locations can result in enhancing existing buildings and creating a higher economic impact. A downtown action plan should include:
  - b. Prioritizing 34th Street-related improvements as outlined in the implementation charts.
  - c. Prioritizing the Singer Building triangle and supporting redevelopment with Rhode Island Avenue ground-floor retail, including investment in streetscape and corner park improvements.
  - d. Committing to a civic green and plaza (and Washington Metropolitan Area Transit Authority relocation) concept.
  - e. Renovating and modifying the Eastern Star Building alongside the existing city hall/library so that they match a civic green and plaza concept.
3. Coordinate redevelopment of the properties on the north side of Rhode Island Avenue between Eastern Avenue and 33rd Street.
4. Prioritize façade improvements and regulation.
5. Streamline the building- and business-permitting process.
6. Commit public investment in shared off-street parking.
7. Invest in an environmental showcase feature for a lower 34th Street or “green street” prototype.
8. Bring art into the public realm.
9. Channel resources into a dedicated Mount Rainier management and marketing entity (similar to the Maryland “Main Street” program but on a smaller scale).



## Implementation Area Recommendations and Charts

### Institutional/Regulatory/Economic Development: “Main-Street” Type Program

#### Program Manager

Barracks Row and Takoma Park are local examples of successfully revitalized main streets. At both these locations, setting up and committing to a full-time program manager and a marketing/promotion institution were extremely critical revitalization elements. The City of Mount Rainier should explore partnership opportunities with local organizations in order to establish a “Main Street” type program and to promote Mount Rainier through a program manager who is primarily responsible for plan implementation activities, including assisting organizations with grant proposals, advertising, promotion/special events, and marketing campaigns. The program manager should use the plan vision as a tool to encourage appropriate activity in the M-U-TC area. The program manager should have the following roles and responsibilities:

1. *Program Organization:*
  - a. Create a strategic plan to implement the plan vision.
  - b. Develop a comprehensive fundraising plan and conduct annual fundraising activities.
2. *Design Standards and Guideline Liaison:*
  - a. Manage implementation of M-U-TC Design Standards and Guidelines by the M-U-TC Design Review Committee.
  - b. Seek funding for streetscape and façade improvements.
  - c. Support historic preservation initiatives.
3. *Marketing and Promotion:*
  - a. Market the M-U-TC area to customers, investors, local brokers, new businesses, residents, and visitors by developing and carrying out image campaigns, retail promotions, and special events.
  - b. Assist with coordination of special events, including Mount Rainier Day.
4. *Economic Development:*
  - a. Develop and implement an economic strategy to identify Mount Rainier as a successful commercial corridor; provide sustainable business opportunities for merchants; and help to create job opportunities, especially for local residents.
  - b. Create partnership opportunities with local institutions and non-profits.
  - c. Create a database of all properties and contact information.
  - d. Communicate regularly with businesses and owners about the program manager’s work.
  - e. Facilitate access to financing programs for façade improvements and business expansion.
  - f. Establish relations with local brokers and real estate organizations.
  - g. Pursue grant funding for projects identified in the implementation chapter, and manage the grant funding process.
  - h. Help streamline the business license process, and serve as liaison between businesses, the city, and the county.
4. *The Arts:*
  - a. Coordinate arts initiatives with the Gateway Community Development Corporation.
  - b. Advance the visibility of art in the community by applying for public art-installation grants.
  - c. Manage and oversee public art once it is installed.
5. *Property Oversight:*
  - a. Assist with the management of real property owned by the City of Mount Rainier and follow up on recommendations concerning implementation of the development plan.
  - b. Engage absentee landlords in order to facilitate the implementation of the development plan.
  - c. Facilitate implementation of green building initiatives by promoting relevant programs.
  - d. Obtain historic property designations on appropriate properties.

## M-U-TC Façade Improvement Program

A city-sponsored façade improvement program (FIP) should be created that coordinates activities with approved development plan priorities. A committee set up to review all FIP applications should:

1. Prioritize the 40–45 properties within the M-U-TC that require some type of rehabilitation, upgrade, and/or restoration action according to the development plan (proposed façade improvement recommendations are listed in the appendix of this plan).
2. Use the plan vision and design standards and guidelines criteria specified in the development plan review and prioritize applicants for inclusion in the program.
3. Plan city investment around a five-year façade improvement program (a recommended public commitment to this program ranges from \$30,000–\$80,000 per year).
4. Hold a public invitation-and-training session for building and property owners within the M-U-TC area, encourage them to apply for FIP grants for a maximum of \$10,000, allowing a shared maximum of \$20,000 per façade. Owners should participate in a hands-on training session that discusses a variety of renovation topics including:
  - a. How to fill out the FIP applications.
  - b. How to determine eligible repairs and upgrades.
  - c. How to delineate the scope of repairs for individual properties.
  - d. How to select an architect or a contractor and manage the construction process.

The city should both accept applications and rank, score and award fund amounts based on pre-determined criteria for that fiscal year. All actions must comply with the new Approved City of Mount Rainier Mixed-Use Town Center Zone Development Plan guidelines.

5. Target specific areas, such as the 34th Street corridor, for the FIP program and combine it with the proposed public street improvement activities.
6. Help with an updated approval process that allows the M-U-TC Design Review Committee to streamline the permit review process for M-U-TC compliant applications.
7. Guide applicants through project permit applications and assist them with the historic area work permit (HAWP) process.



# City Actions/Regulatory/Implementation Chart

IMPLEMENTING ACTION	RESPONSIBLE AGENCIES/STAKEHOLDERS	POSSIBLE IMPLEMENTING TOOLS AND FUNDING SOURCES	PHASING	PRIORITY ACTION
<p>1</p> <p><b>Initiate a “Main-Street” type program</b> and create the position of a program manager responsible for coordinating the following programs on behalf of the city:</p> <ul style="list-style-type: none"> <li>• Façade improvement program (geared toward historic preservation).</li> <li>• Gateway Arts District:                             <ul style="list-style-type: none"> <li>i. Arts events program.</li> <li>ii. Public art program.</li> <li>iii. Pop-up art galleries program.</li> </ul> </li> <li>• Marketing/promotion program.</li> <li>• Design/signage/wayfinding program.</li> <li>• Economic development program.</li> </ul>	<p>City, county, Gateway CDC, property owners, State of Maryland, and Neighborhood Design Center.</p>	<ul style="list-style-type: none"> <li>• Community Development Block Grant Program (page 123).</li> <li>• The Community Legacy Program (page 123).</li> <li>• Prince George’s County Economic Development Corporation (page 123).</li> <li>• Capital Improvement Project (page 124).</li> <li>• Historic Property Grant Program (page 125).</li> <li>• Art in Communities (page 128).</li> </ul>	<p>Near term</p>	<p>Yes</p>
<p>2</p> <p>Engage landlords in dialogue to implement the plan vision.</p>	<p>City and property owners.</p>	<ul style="list-style-type: none"> <li>• Not applicable.</li> </ul>	<p>Near term</p>	
<p>3</p> <p>Streamline the permit process and make it clearer.</p>	<p>City, county, and property owners.</p>	<ul style="list-style-type: none"> <li>• Not applicable.</li> </ul>	<p>Near term</p>	
<p>4</p> <p>Create a fund within the M-U-TC area with the objective of undergrounding utility lines.</p>	<p>City, county, property owners, utility companies, and the SHA.</p>	<ul style="list-style-type: none"> <li>• Capital Improvement Project (page 124).</li> <li>• Contribution from Local Developer (page 124).</li> <li>• Capital Improvement Program (page 125).</li> </ul>	<p>Medium term and long term</p>	

## Upper 34th Street



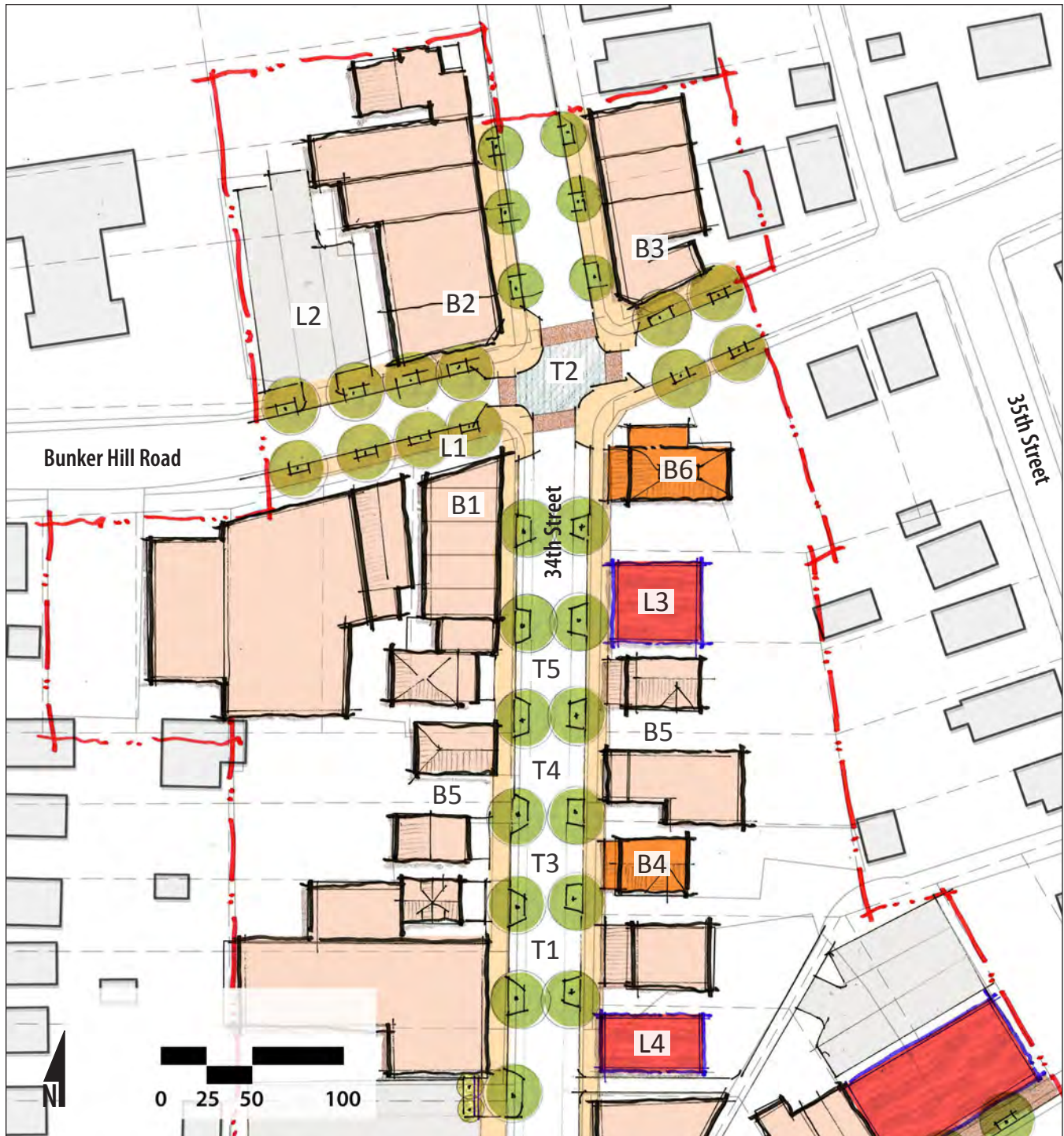
### Implementation Action Timeline\*

PHASE	LOCATION	RECOMMENDATION	REFERENCE NUMBER
NEAR TERM (0–5 years)	4000–4008 34th Street	New shared parking lot (public/private)	<b>L2</b> on page 110
	3840 34th Street	Renovation of building and new retail/restaurant at north end with outdoor seating	<b>B1</b> on page 110
	Intersection at Bunker Hill Road and 34th Street	New pedestrian improvements (curb extensions and crosswalks)	<b>T2</b> on page 110
	34th Street	Bike sharrow lane and new pay-on-foot parking meters	<b>T1</b> and <b>T3</b> on page 110
MEDIUM TERM (5–10 years)	34th Street	New street trees, expanded and widened sidewalks, and new street lights (east side)	<b>T4</b> on page 110
	Middle 34th Street cluster	Renovations, façade improvements, and outdoor seating	<b>B5</b> on page 110
	3401 Bunker Hill Road	Removal of 1930s front addition, restoration of the original building façade, and new retail/restaurant with outdoor seating	<b>B6</b> on page 110
LONG TERM (10+ years)	34th Street	Underground utility lines	<b>T5</b> on page 110
	3841, 3815, and 3817 34th Street	Infill buildings	<b>L3</b> and <b>L4</b> on page 110

\* The implementation action timeline on this page lists potential phasing of some projects in the Upper 34th Street District from 0 to 10 years. For a complete listing of proposed implementation actions, please refer to the Upper 34th Street Implementation charts found on pages 111 to 113.



### Implementation Map—Upper 34th Street



- T—TRANSPORTATION RECOMMENDATIONS
- L—LAND USE RECOMMENDATIONS
- B—BUILDING RECOMMENDATIONS

## Upper 34th Street—Transportation and Other Infrastructure Implementation Chart

	IMPLEMENTING ACTION	RESPONSIBLE AGENCIES/STAKEHOLDERS	POSSIBLE IMPLEMENTING TOOLS AND FUNDING SOURCES	PHASING	PRIORITY ACTION
T1	Replace parking meters on both sides of 34th Street from the circle to the retail properties just north of Bunker Hill Road with multi-space pay-on-foot parking meters.	City.	<ul style="list-style-type: none"> <li>• Parking Equipment Maintenance Budget (page 124).</li> <li>• City Speed Camera Legislation (page 124).</li> </ul>	Near term	
T2	Install new crosswalks, curb extensions, and textured pavement in the center of the intersection of 34th Street and Bunker Hill Road.	City and county.	<ul style="list-style-type: none"> <li>• Community Development Block Grant Program (page 123).</li> <li>• Sidewalk and Street Maintenance Budget (page 124).</li> <li>• City Speed Camera Legislation (page 124).</li> <li>• Capital Improvement Program (page 125).</li> </ul>	Near term	Yes
T3	Install shared bicycle/vehicular pavement markings (“sharrows”) on 34th Street just south of Bunker Hill Road and just north of the circle in both directions.	City.	<ul style="list-style-type: none"> <li>• Sidewalk and Street Maintenance Budget (page 124).</li> </ul>	Near term	
T4	Upgrade 34th Street pedestrian and parking areas: <ul style="list-style-type: none"> <li>• Expand sidewalks on both sides of 34th Street from the circle to Bunker Hill Road.</li> <li>• Reconfigure on-street parking on both sides of 34th Street from the circle to just north of Bunker Hill Road to install street trees in curb extensions.</li> </ul>	City and county.	<ul style="list-style-type: none"> <li>• Sidewalk and Street Maintenance Budget (page 124).</li> <li>• Capital Improvement Project (page 124).</li> <li>• City Speed Camera Legislation (page 124).</li> <li>• Capital Improvement Program (page 125).</li> </ul>	Medium term	
T5	Undergrounding of utility lines (34th Street and Bunker Hill Road).	City, county, and utility companies.	<ul style="list-style-type: none"> <li>• Capital Improvement Project (page 124).</li> <li>• Contribution from Local Developer (page 124).</li> <li>• Capital Improvement Program (page 125).</li> </ul>	Long term	



## Upper 34th Street—Land Use Implementation Chart

IMPLEMENTING ACTION	RESPONSIBLE AGENCIES/STAKEHOLDERS	POSSIBLE IMPLEMENTING TOOLS AND FUNDING SOURCES	PHASING	PRIORITY ACTION
L1 34th Street: <ul style="list-style-type: none"> <li>New vibrant retail (e.g., full-service restaurant).</li> </ul>	City, property owners, and new business owners.	<ul style="list-style-type: none"> <li>Community Development Block Grant Program (page 123).</li> <li>The Community Legacy Program (page 123).</li> <li>Public-Private Shared Parking Arrangement (page 124).</li> <li>Main Street Maryland (page 126).</li> <li>State of Maryland Small Business Development Center (page 128).</li> </ul>	Near term	Yes
L2 4000–4008 34th Street (parking lot at rear of building): <ul style="list-style-type: none"> <li>Install permeable pavers on the existing surface lot to provide shared parking opportunities for multiple retail uses on 34th Street.</li> </ul>	City and property owner.	<ul style="list-style-type: none"> <li>The Community Legacy Program (page 123).</li> <li>Community Development Block Grant Program (page 123).</li> <li>Public-Private Shared Parking Arrangement (page 124).</li> </ul>	Near term	
L3 3841 34th Street (empty lot): <ul style="list-style-type: none"> <li>New mixed-use infill building (interim shared parking lot/short to medium term).</li> </ul>	City and property owner.	<ul style="list-style-type: none"> <li>The Community Legacy Program (page 123).</li> <li>Public-Private Shared Parking Arrangement (page 124).</li> </ul>	Long term	
L4 3815/3817 34th Street (empty lot): <ul style="list-style-type: none"> <li>New mixed-use infill building.</li> </ul>	City and property owner.	<ul style="list-style-type: none"> <li>The Community Legacy Program (page 123).</li> <li>Public-Private Shared Parking Arrangement (page 124).</li> <li>Main Street Maryland (page 126).</li> </ul>	Long term	

## Upper 34th Street—Building Implementation Chart

IMPLEMENTING ACTION	RESPONSIBLE AGENCIES/STAKEHOLDERS	POSSIBLE IMPLEMENTING TOOLS AND FUNDING SOURCES	PHASING	PRIORITY ACTION
B1 3840 34th Street: <ul style="list-style-type: none"> <li>• Outdoor seating and upgraded storefront for restaurant use.</li> <li>• Façade improvements.</li> </ul>	City, county, property owner, and business owner.	<ul style="list-style-type: none"> <li>• Community Development Block Grant Program (page 123).</li> <li>• The Community Legacy Program (page 123).</li> <li>• Public-Private Shared Parking Arrangement (page 124).</li> <li>• Contribution from Local Developer (page 124).</li> <li>• Prince George’s County’s Revitalization Area Tax Credit Program (page 124).</li> <li>• Prince George’s Financial Service Corporation (page 125).</li> <li>• Historic Property Grant Program (page 125).</li> <li>• Gateway Community Development Corporation (page 125).</li> </ul>	Near term	Yes
B2 4000–4008 34th Street: <ul style="list-style-type: none"> <li>• Façade improvements (bring back original building).</li> <li>• Signage.</li> </ul>	City, county, property owner, and business owner.	<ul style="list-style-type: none"> <li>• The Community Legacy Program (page 123).</li> <li>• Contribution from Local Developer (page 124).</li> <li>• Public-Private Shared Parking Arrangement (page 124).</li> <li>• Prince George’s County’s Revitalization Area Tax Credit Program (page 124).</li> <li>• Main Street Maryland (page 126).</li> </ul>	Near term	
B3 4001 34th Street: <ul style="list-style-type: none"> <li>• Outdoor market stand.</li> <li>• Façade improvements (bring back original building).</li> </ul>	City, property owner, and business owner.	<ul style="list-style-type: none"> <li>• The Community Legacy Program (page 123).</li> <li>• Contribution from Local Developer (page 124).</li> <li>• Public-Private Shared Parking Arrangement (page 124).</li> <li>• Main Street Maryland (page 126).</li> </ul>	Near term	
B4 3829 34th Street: <ul style="list-style-type: none"> <li>• Outdoor/indoor seating.</li> <li>• Façade improvements.</li> </ul>	City, property owner, and business owner.	<ul style="list-style-type: none"> <li>• The Community Legacy Program (page 123).</li> <li>• Contribution from Local Developer (page 124).</li> <li>• Public-Private Shared Parking Arrangement (page 124).</li> <li>• Main Street Maryland (page 126).</li> </ul>	Near term	
B5 Middle 34th Street cluster: <ul style="list-style-type: none"> <li>• Selective outdoor seating.</li> <li>• Façade improvements.</li> </ul>	City, property owner, and business owner.	<ul style="list-style-type: none"> <li>• The Community Legacy Program (page 123).</li> <li>• Contribution from Local Developer (page 124).</li> <li>• Public-Private Shared Parking Arrangement (page 124).</li> <li>• Main Street Maryland (page 126).</li> </ul>	Medium term	
B6 3401 Bunker Hill Road: <ul style="list-style-type: none"> <li>• Remove 1930s front addition.</li> <li>• Restore original building façade and add outdoor seating and new retail/restaurant.</li> </ul>	City, property owner, and business owner.	<ul style="list-style-type: none"> <li>• The Community Legacy Program (page 123).</li> <li>• Contribution from Local Developer (page 124).</li> <li>• Public-Private Shared Parking Arrangement (page 124).</li> <li>• Main Street Maryland (page 126).</li> </ul>	Medium term	



## Rhode Island Avenue



### Implementation Action Timeline\*

PHASE	LOCATION	RECOMMENDATION	REFERENCE NUMBER
NEAR TERM (0–5 years)	3201 Rhode Island Avenue	Façade improvements and signage	<b>B1</b> on the facing page
	3200–3212 Rhode Island Avenue	New residential and flex community space	<b>L1</b> and <b>L2</b> on the facing page
	3220 Rhode Island Avenue	Renovation and new retail/outdoor seating	<b>B2</b> on the facing page
	33rd Street and Rhode Island Avenue	New pedestrian-activated signal and pedestrian crosswalk	<b>T3</b> on the facing page
	Rhode Island Avenue	Pay-on-foot parking meters	<b>T1</b> on the facing page
	3231–3239 Rhode Island Avenue	Façade improvements and additions, restripe existing wide parking lanes	<b>B3</b> and <b>B4</b> on the facing page
MEDIUM TERM (5–10 years)	Eastern Avenue and Rhode Island Avenue intersection	New pedestrian crosswalk	<b>T2</b> on the facing page
	Rhode Island Avenue	Expand the tree-lined median to Eastern Avenue	<b>T4</b> on the facing page
	Rhode Island Avenue (north side)	Initial utility undergrounding (potential interim reconfiguration of lines in alleys)	<b>T6</b> on the facing page
LONG TERM (10+ years)	Rhode Island Avenue	Boulevard streetscape and roadway improvements, pedestrian pavement at the circle area, and cycle tracks	<b>T5</b> on the facing page
	Rhode Island Avenue (south side)	Undergrounding of “heavy” utility lines	<b>T6</b> on the facing page
	Rhode Island Avenue	DDOT light rail	<b>T7</b> on the facing page

### Implementation Map—Rhode Island Avenue



\* THE IMPLEMENTATION ACTION TIMELINE ON THE FACING PAGE LISTS POTENTIAL PHASING OF SOME PROJECTS IN THE RHODE ISLAND AVENUE DISTRICT FROM 0 TO 10 YEARS. FOR A COMPLETE LISTING OF PROPOSED IMPLEMENTATION ACTIONS, PLEASE REFER TO THE RHODE ISLAND AVENUE IMPLEMENTATION CHARTS FOUND ON PAGES 116 AND 117.

- T—TRANSPORTATION RECOMMENDATIONS
- L—LAND USE RECOMMENDATIONS
- B—BUILDING RECOMMENDATIONS



## Rhode Island Avenue—Transportation and Other Infrastructure Implementation Chart

	IMPLEMENTING ACTION	RESPONSIBLE AGENCIES/STAKEHOLDERS	POSSIBLE IMPLEMENTING TOOLS AND FUNDING SOURCES	PHASING	PRIORITY ACTION
T1	Replace parking meters on Rhode Island Avenue from Eastern Avenue to the circle with multi-space pay-on-foot parking meters.	City.	<ul style="list-style-type: none"> <li>Parking Equipment Maintenance Budget (page 124).</li> </ul>	Near term	
T2	Install new pedestrian crosswalks across Rhode Island Avenue on the east side of Eastern Avenue and the east side of 33rd Street.	City and SHA.	<ul style="list-style-type: none"> <li>Sidewalk and Street Maintenance Budget (page 124).</li> </ul>	Near term	
T3	Install a pedestrian-activated crosswalk signal and curb extensions at Rhode Island Avenue and 33rd Street.	City and SHA.	<ul style="list-style-type: none"> <li>City Speed Camera Legislation (page 124).</li> <li>Federal Government Stimulus Funding (page 124).</li> <li>Contribution from Local Developer (page 124).</li> </ul>	Near term	
T4	Expand the Rhode Island Avenue median between 33rd Street and Eastern Avenue to plant more ornamental-size street trees near Eastern Avenue.	City, county, and the State of Maryland.	<ul style="list-style-type: none"> <li>Prince George’s County Tree Releaf Program (page 123).</li> <li>Capital Improvement Project (page 124).</li> <li>City Speed Camera Legislation (page 124).</li> <li>Capital Improvement Program (page 125).</li> </ul>	Medium term	
T5	Upgrade both sides of Rhode Island Avenue (Streetscape): <ul style="list-style-type: none"> <li>Install a cycle track on Rhode Island Avenue.</li> <li>Install two rows of street trees.</li> <li>Replace/expand existing sidewalks.</li> <li>Install special paving.</li> <li>Provide outdoor seating.</li> </ul>	City, county, property owners, and SHA.	<ul style="list-style-type: none"> <li>Department of Natural Resources State Forest Conservation Program (page 123).</li> <li>Prince George’s County Tree ReLeaf Program (page 123).</li> <li>Sidewalk and Street Maintenance Budget (page 124).</li> <li>City Speed Camera Legislation (page 124).</li> <li>Contribution from Local Developer (page 124).</li> <li>Capital Improvement Program (page 125).</li> </ul>	Medium term	Yes
T6	Undergrounding utility lines.	City, county, property owners, and utility companies.	<ul style="list-style-type: none"> <li>The Community Legacy Program (page 123).</li> <li>Prince George’s County Economic Stimulus Program (page 123).</li> <li>Contribution from Local Developer (page 124).</li> <li>Capital Improvement Program (page 125).</li> </ul>	Long term	
T7	Extend the District DOT light-rail tracks on Rhode Island Avenue into Prince George’s County.	City, county, SHA, and District DOT.	<ul style="list-style-type: none"> <li>Capital Improvement Program (page 125).</li> </ul>	Long term	

### Rhode Island Avenue—Land Use Implementation Chart

IMPLEMENTING ACTION	RESPONSIBLE AGENCIES/STAKEHOLDERS	POSSIBLE IMPLEMENTING TOOLS AND FUNDING SOURCES	PHASING	PRIORITY ACTION
L1 3212 Rhode Island Avenue: <ul style="list-style-type: none"> <li>New community flex space.</li> <li>New infill residential building.</li> </ul>	City and property owner.	<ul style="list-style-type: none"> <li>Community Development Block Grant Program (page 123).</li> <li>HOME Investment Partnership Program (page 123).</li> <li>Property Tax Abatements (page 124).</li> </ul>	Near term	Yes
L2 3200-3208 Rhode Island Avenue: <ul style="list-style-type: none"> <li>New infill residential building.</li> </ul>	City and developer.	<ul style="list-style-type: none"> <li>Community Development Block Grant Program (page 123).</li> <li>HOME Investment Partnership Program (page 123).</li> <li>Property Tax Abatements (page 124).</li> </ul>	Near term	Yes

### Rhode Island Avenue—Building Implementation Chart

IMPLEMENTING ACTION	RESPONSIBLE AGENCIES/STAKEHOLDERS	POSSIBLE IMPLEMENTING TOOLS AND FUNDING SOURCES	PHASING	PRIORITY ACTION
B1 3201 Rhode Island Avenue: <ul style="list-style-type: none"> <li>Façade improvements.</li> <li>Signage.</li> </ul>	City, property owner, and business owner.	<ul style="list-style-type: none"> <li>The Community Legacy Program (page 123).</li> <li>Contribution from Local Developer (page 124).</li> <li>Main Street Maryland (page 126).</li> </ul>	Near term	
B2 3220 Rhode Island Avenue: <ul style="list-style-type: none"> <li>Renovate historic structure.</li> <li>Reuse as small retail store.</li> </ul>	City and property owner.	<ul style="list-style-type: none"> <li>The Community Legacy Program (page 123).</li> <li>Contribution from Local Developer (page 124).</li> <li>Historic Property Grant Program (page 125).</li> <li>Main Street Maryland (page 126).</li> </ul>	Near term	Yes
B3 3231/3239 Rhode Island Avenue: <ul style="list-style-type: none"> <li>Add additional floor.</li> <li>Façade improvements.</li> </ul>	City, property owner, and business owner.	<ul style="list-style-type: none"> <li>The Community Legacy Program (page 123).</li> <li>Contribution from Local Developer (page 124).</li> <li>Main Street Maryland (page 126).</li> </ul>	Long term	
B4 South Rhode Island Ave. cluster: <ul style="list-style-type: none"> <li>Façade improvements.</li> <li>Signage.</li> </ul>	City, property owner, and business owner.	<ul style="list-style-type: none"> <li>The Community Legacy Program (page 123).</li> <li>Contribution from Local Developer (page 124).</li> <li>Main Street Maryland (page 126).</li> </ul>	Medium term	



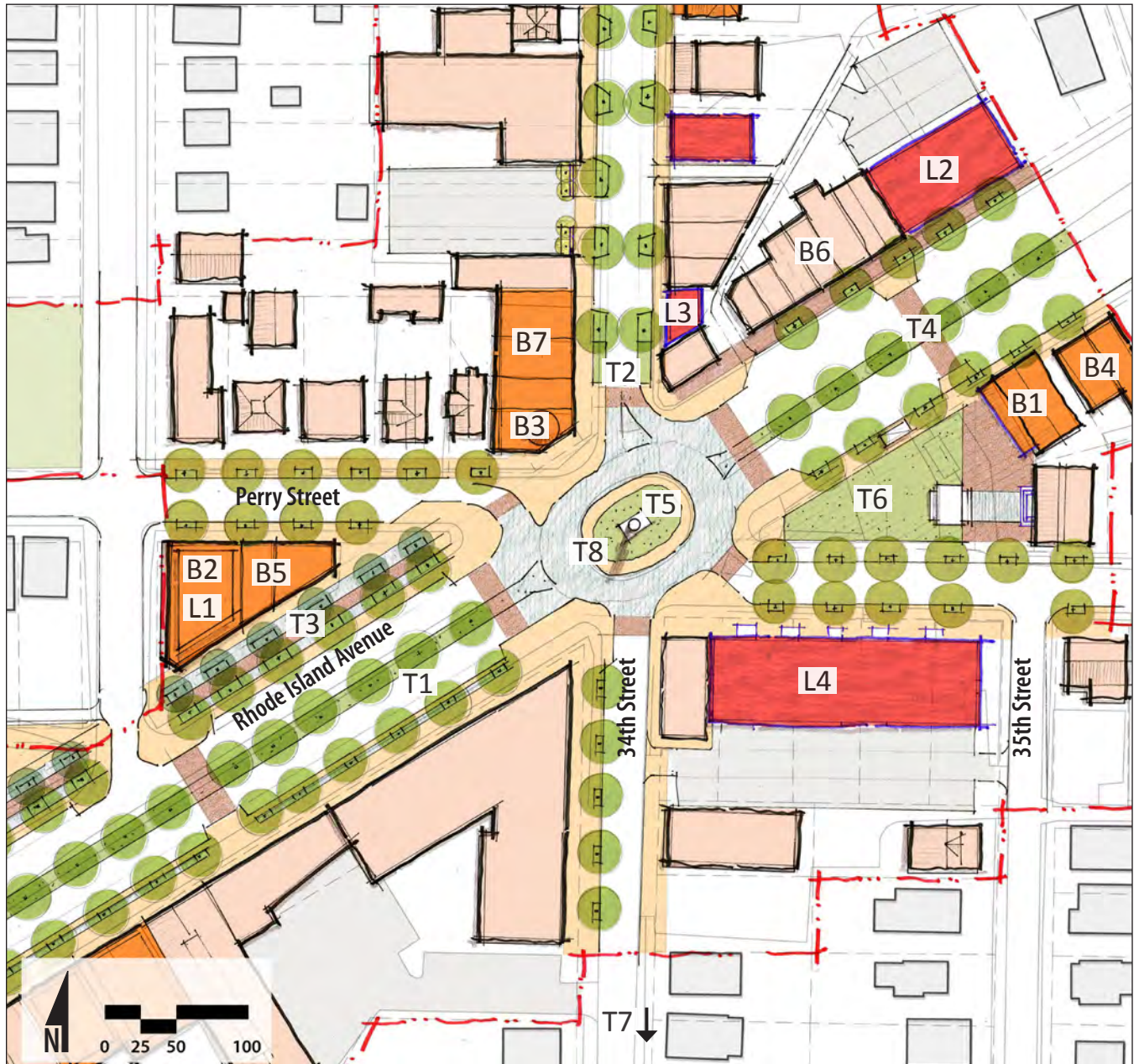
## Civic Core/City Center



### Implementation Action Timeline\*

PHASE	LOCATION	RECOMMENDATION	REFERENCE NUMBER
NEAR TERM (0–5 years)	3300–3310 Rhode Island Avenue	Façade restoration, potential additional floor, and potential residential renovation	<b>B2</b> and <b>B5</b> on the facing page
	3405 Rhode Island Avenue	Façade restoration, building renovation, and façade addition	<b>B1</b> on the facing page
	Intersection of 34th Street and Rhode Island Avenue	Stormwater rain garden and “Green Street” prototype	<b>T2</b> on the facing page
MEDIUM TERM (5–10 years)	3409 Rhode Island Avenue (library)	Renovation and new building	<b>B4</b> on the facing page
	3804 34th Street	Mixed-use infill building	<b>L3</b> on the facing page
	North Rhode Island Avenue cluster	Renovations and new infill buildings	<b>B6</b> and <b>L2</b> on the facing page
LONG TERM (10+ years)	Bus turnaround removal	Civic green, plaza with transit stop, landscaping, and gathering spaces	<b>T6</b> on the facing page
	Traffic circle	Monument, landscaping, and textured pavement	<b>T5</b> and <b>T8</b> on the facing page
	Immediately south of M-U-TC area	New MARC station	<b>T7</b> on the facing page
	3403–3415 Perry Street	Infill building	<b>L4</b> on the facing page

Implementation Map—Civic Core/City Center



- T—TRANSPORTATION RECOMMENDATIONS
- L—LAND USE RECOMMENDATIONS
- B—BUILDING RECOMMENDATIONS

\* The implementation action timeline on the facing page lists a potential phasing of some projects in the Civic Core District from 0 to 10 years. For a complete listing of proposed implementation actions, please refer to the Civic Core Implementation charts found on pages 120 to 122.



## Civic Core/City Center—Transportation and Other Infrastructure Implementation Chart

IMPLEMENTING ACTION	RESPONSIBLE AGENCIES/STAKEHOLDERS	POSSIBLE IMPLEMENTING TOOLS AND FUNDING SOURCES	PHASING	PRIORITY ACTION
T1 Replace parking meters on both sides of Rhode Island Avenue from the circle to the eastern end of the M-U-TC with multispace pay-on-foot parking meters.	City.	<ul style="list-style-type: none"> <li>• Parking Equipment Maintenance Budget (page 124).</li> </ul>	Near term	
T2 Rain garden, stormwater, “Green Street” prototype.	City and utility companies.	<ul style="list-style-type: none"> <li>• Community Development Block Grant Program (page 123).</li> <li>• Capital Improvement Project (page 124).</li> </ul>	Near term	Yes
T3 Upgrade the north side of Rhode Island Avenue east of the circle to the eastern M-U-TC: <ul style="list-style-type: none"> <li>• Install two rows of street trees.</li> <li>• Replace/expand existing sidewalks.</li> <li>• Install special paving treatments to emphasize the walking path along Rhode Island Avenue.</li> <li>• Reconfigure the on-street parking to fit within the proposed curb extensions.</li> <li>• Provide outdoor seating for adjacent land uses.</li> </ul>	City, property owners, and SHA.	<ul style="list-style-type: none"> <li>• Sidewalk and Street Maintenance Budget (page 124).</li> <li>• Capital Improvement Project (page 124).</li> <li>• City Speed Camera Legislation (page 124).</li> <li>• Contribution from Local Developer (page 124).</li> <li>• Capital Improvement Program (page 125).</li> </ul>	Medium term	Yes
T4 Add pedestrian curb extensions at the northernmost Rhode Island Avenue crosswalk near the library.	City and SHA.	<ul style="list-style-type: none"> <li>• Sidewalk and Street Maintenance Budget (page 124).</li> </ul>	Medium term	
T5 Traffic Circle: <ul style="list-style-type: none"> <li>• Install new landmark monument.</li> </ul>	City.	<ul style="list-style-type: none"> <li>• Sidewalk and Street Maintenance Budget (page 124).</li> </ul>	Medium Term	
T6 Redesign bus turnaround as a new civic green and plaza: <ul style="list-style-type: none"> <li>• Open green space.</li> <li>• Landscape platform.</li> </ul>	City, SHA, and WMATA.	<ul style="list-style-type: none"> <li>• Capital Improvement Program (page 125).</li> </ul>	Long Term	Yes
T7 Advocate new MARC train station.	City, county, and State of Maryland.	<ul style="list-style-type: none"> <li>• Community Development Block Grant Program (page 123).</li> </ul>	Long term	
T8 Traffic Circle: <ul style="list-style-type: none"> <li>• Install special paving treatment.</li> </ul>	SHA.	<ul style="list-style-type: none"> <li>• Capital Improvement Program (page 125).</li> </ul>	Long term	

### Civic Core/City Center—Land Use Implementation Chart

IMPLEMENTING ACTION	RESPONSIBLE AGENCIES/STAKEHOLDERS	POSSIBLE IMPLEMENTING TOOLS AND FUNDING SOURCES	PHASING	PRIORITY ACTION
L1 3300 Rhode Island Ave. <ul style="list-style-type: none"> <li>• New retail/restaurant use.</li> </ul>	City and property owner.	<ul style="list-style-type: none"> <li>• Community Development Block Grant Program (page 123).</li> <li>• HOME Investment Partnership Program (page 123).</li> </ul>	Near term	Yes
L2 3424–3428 Rhode Island Ave. (empty lot): <ul style="list-style-type: none"> <li>• New mixed-use infill building.</li> </ul>	City and property owner.	<ul style="list-style-type: none"> <li>• Public-private shared parking arrangement (page 124).</li> <li>• Prince George’s County’s Revitalization Area Tax Credit Program (page 124).</li> <li>• Property Tax Abatements (page 124).</li> </ul>	Medium term	
L3 3805 34th St. (empty lot): <ul style="list-style-type: none"> <li>• New mixed-use infill building.</li> </ul>	City, county, and property owner.	<ul style="list-style-type: none"> <li>• Prince George’s County’s Revitalization Area Tax Credit Program (page 124).</li> </ul>	Medium term	
L4 3403–3415 Perry St. cluster (South Perry Street): <ul style="list-style-type: none"> <li>• New infill residential building, new commercial office building, or mixed-use building.</li> </ul>	City and property owner.	<ul style="list-style-type: none"> <li>• Community Development Block Grant Program (page 123).</li> <li>• Property Tax Abatements (page 124).</li> <li>• Gateway Community Development Corporation (page 125).</li> </ul>	Long term	



## Civic Core/City Center—Building Implementation Chart

IMPLEMENTING ACTION	RESPONSIBLE AGENCIES/STAKEHOLDERS	POSSIBLE IMPLEMENTING TOOLS AND FUNDING SOURCES	PHASING	PRIORITY ACTION
B1 3405 Rhode Island Avenue: <ul style="list-style-type: none"> <li>• Renovation for new municipal use.</li> <li>• Façade addition/renovation on western side, towards civic green.</li> </ul>	City.	<ul style="list-style-type: none"> <li>• The Community Legacy Program (page 123).</li> <li>• Capital Improvement Project (page 124).</li> <li>• Historic Property Grant Program (page 125).</li> </ul>	Near term	Yes
B2 3300 Rhode Island Avenue: <ul style="list-style-type: none"> <li>• Explore additional floor.</li> <li>• Façade improvements.</li> <li>• Renovate building for mixed-use (retail/residential).</li> </ul>	City and property owner.	<ul style="list-style-type: none"> <li>• Community Development Block Grant Program (page 123).</li> <li>• HOME Investment Partnership Program (page 123).</li> <li>• Property Tax Abatements (page 124).</li> <li>• Historic Property Grant Program (page 125).</li> </ul>	Near term	Yes
B3 3800 34th Street: <ul style="list-style-type: none"> <li>• Façade improvements/renovations.</li> <li>• Signage.</li> </ul>	City and property owner.	<ul style="list-style-type: none"> <li>• The Community Legacy Program (page 123).</li> <li>• Capital Improvement Project (page 124).</li> <li>• Main Street Maryland (page 126).</li> </ul>	Near term	
B4 3409 Rhode Island Avenue: <ul style="list-style-type: none"> <li>• New/renovated library building.</li> </ul>	City and county.	<ul style="list-style-type: none"> <li>• Capital Improvement Project (page 124).</li> </ul>	Medium term	
B5 3308–3310 Rhode Island Avenue: <ul style="list-style-type: none"> <li>• Building renovation.</li> <li>• Façade improvements.</li> <li>• New retail/restaurant use.</li> </ul>	City, property owner, and business owner.	<ul style="list-style-type: none"> <li>• The Community Legacy Program (page 123).</li> <li>• Contribution from Local Developer (page 124).</li> <li>• Main Street Maryland (page 126).</li> </ul>	Medium term	
B6 North Rhode Island Avenue cluster: <ul style="list-style-type: none"> <li>• Façade improvements.</li> <li>• Signage.</li> </ul>	City, property owner, and business owner.	<ul style="list-style-type: none"> <li>• The Community Legacy Program (page 123).</li> <li>• Contribution from Local Developer (page 124).</li> <li>• Main Street Maryland (page 126).</li> </ul>	Medium term	
B7 3802 34th Street: <ul style="list-style-type: none"> <li>• Add additional floor.</li> <li>• Façade improvements.</li> <li>• Building renovations.</li> </ul>	City and property owner.	<ul style="list-style-type: none"> <li>• The Community Legacy Program (page 123).</li> <li>• Contribution from Local Developer (page 124).</li> <li>• Main Street Maryland (page 126).</li> </ul>	Medium term	

## Implementation Tools and Programs

The implementation charts describe actions that should be taken to revitalize the M-U-TC area. These actions can utilize numerous funding programs that will achieve the plan vision for the area. Residential development is recommended as the primary growth opportunity with focused ancillary new commercial/retail space and designated arts space.

Numerous incentives and techniques may be employed to facilitate development, minimize existing barriers and constraints, and build on specific opportunities created by the plan. They may range from fairly conceptual incentives and programs to direct subsidies and assistance by the public sector. These programs and incentives should be considered individually and collectively for their applicability to desired development and redevelopment projects.

### Current Programs

The City of Mount Rainier currently utilizes several programs that serve as vital funding resources for community development. Throughout implementation of the development plan, the city should continue working with the following programs:

1. **The Community Development Block Grant Program (CDBG).** The CDBG is a Department of Housing and Urban Development (HUD) program that provides communities with resources to address a wide range of development needs. It is administered and sponsored by the State of Maryland's Department of Housing and Community Development. Project grants can range from community infrastructure repairs to affordable housing and public facilities.
2. **HOME Investment Partnership Program.** Used in partnership with local nonprofit groups, this HUD initiative provides formula grants to states and localities. The grants are used to buy, build and/or rehabilitate affordable housing for rent or home ownership.
3. **The Community Legacy Program.** This program provides local governments and community development organizations with funding for essential projects aimed at strengthening communities through activities such as business retention and attraction, encouraging home ownership and commercial revitalization. Projects should capitalize on the strengths of a community and be part of a larger revitalization strategy to revitalize a declining area.
4. **Prince George's County Economic Development Corporation (EDC).** EDC provides investment incentives to attract new businesses; access to support services and capital to help businesses grow; and information on procurement opportunities to enable businesses to sustain their growth.
5. **Maryland Technical Assistance Program.** This program provides funding for market and feasibility analyses that are administered through Maryland's Department of Housing and Community Development Neighborhood Revitalization program.
6. **State Bond Bills.** Municipalities may apply for capital grants via a Senate Bond Bill. The City of Mount Rainier used this funding mechanism to obtain funds to renovate the community center/public library located at 3409 Rhode Island Avenue.
7. **Department of Natural Resources State Forest Conservation Program.** This program supports the city's matching fund requirement for its tree replacement stock.
8. **Prince George's County Tree ReLeaf Program.** The ReLeaf Grant Program provides landscape funding to community organizations and municipalities for planting projects in public spaces. Small community-based projects are eligible to receive up to \$5,000 per project and municipalities are eligible for up to \$10,000 per project.
9. **Congressional Appropriations.** Municipalities may request direct funding for specific projects (i.e., acquisition of vacant and blighted buildings).
10. **Prince George's County Economic Stimulus Program.** Municipalities may apply for state funding through the County Executive's office.



## Transportation Implementation Tools

1. **Parking Equipment Maintenance Budget.** The City of Mount Rainier has an annual budget for parking meter maintenance that can be used to gradually phase in multispace pay-on-foot meters.
2. **Sidewalk and Street Maintenance Budget.** The City of Mount Rainier has an annual budget for sidewalk and street maintenance that may be used to gradually phase in plan vision recommendations.
3. **Public-Private Shared Parking Arrangement.** The City of Mount Rainier should make agreements with private property owners enabling them to retain access to parking spaces serving their developments and to lease or allow area patrons to use their excess parking. In terms of these agreements the city could install meters for excess parking on private properties.
4. **Capital Improvement Project.** The City of Mount Rainier should allocate funds toward construction projects with a fixed budget and timeline.
5. **City Speed Camera Legislation.** The City Council of Mount Rainier has passed speed camera legislation that allows speed cameras to be placed in the city within half a mile of an elementary school. Most of the city is covered by this legislation. Revenue received from this program could possibly be used for plan vision initiatives.
6. **Federal Government Stimulus Funding.** The city received \$98,000 from a federal grant for city sidewalks near Kaywood Gardens. Maryland Governor Martin O'Malley has made a portion of the \$206.7 million federal grant available to local jurisdictions that have highway projects "shovel-ready." Thirty percent of the remaining funds, or \$62 million, will be made available to counties throughout the state.
7. **Contribution from Local Developer.** Any major redevelopment project that may benefit from streetscape improvements should warrant a contribution from the project developer.

## Prince George's County Economic Revitalization Programs

1. **Prince George's County's Revitalization Area Tax Credit Program.** Revitalization/Redevelopment tax credits encourage redevelopment and investment in inner-Beltway communities of Prince George's County. The credits are available in all census tracts inside the Beltway where the median household income does not exceed the county's median. Eligible improvements to real property located within these districts shall be allowed a tax credit on county real property taxes. Qualifying commercial projects receive a graduated tax credit over five years, beginning with a 100 percent credit the first year, 80 percent in the second year, 60 percent in the third year, 40 percent in the fourth year, and 20 percent in the fifth year. Residential property taxes are abated 100 percent in the first year, 66 percent in the second year, and 33 percent in the third year. The maximum amount of eligible residential improvements is \$100,000 per dwelling unit.
2. **Tax Increment Financing (TIF).** Property tax revenues are frozen when a TIF is implemented. This base revenue will continue to flow to the taxing entities throughout the life of the district. However, as development and redevelopment occur, property tax revenues increase. This increase in property tax revenue from the base year (or the increment) is retained in a special allocation fund (TIF fund). These funds can be reinvested in the district or used to purchase land and/or fund capital investment through TIF revenue bonds underwritten by the dedicated increment of related municipal taxes and guaranteed by the issuing municipality.
3. **Property Tax Abatements.** This comprises the abatement of defined municipal taxes for a specified period, usually not exceeding 10 to 15 years, and perhaps on a declining scale. The abated city and, if applicable, county taxes are retained by the development entity to help cover a shortfall in financing for municipally prioritized private development.

4. **Business Improvements District (BID).** BIDs target areas managed by local business-oriented organizations. These organizations are supported by supplemental fees paid by affected property owners and businesses.
5. **Revolving Loan Program.** This program is directed toward for-profit organizations with 50 or fewer employees, whose primary sales are generated from engineering, life sciences, computer sciences, electronics, and other technology. To qualify for this program, companies must have an operating physical facility or headquarters located in a priority funding area. Loan amounts range from \$25,000 to \$100,000.
6. **Prince George's Financial Service Corporation.** Small Business Growth Fund targets businesses with 25 or fewer employees. Funding from this source is often directed towards expansion of existing businesses, but start-up funding is considered on a case-by-case basis. Funding can be used for building renovations or leasehold improvements.

SBA 504 is a federal program administered through the Prince George's Financial Services Corporation (FSC). The target of this program is for the healthy, expanding of small businesses. The FSC offers below-market rate financing, which can be used to acquire land or buildings, construct buildings, make leasehold improvements, or to purchase machinery.

ACE-NET is a program offering equity financing from \$250,000 to \$3 million for smaller-scale entrepreneurs looking for equity financing.

7. **Historic Property Grant Program.** This program is designed to preserve, protect, and enhance historic properties and promote interest in the study of historic properties. Grants are awarded for the rehabilitation, restoration, preservation, or acquisition of historic property.
8. **Land Readjustment Programs.** This program involves the private sector pooling land for the purpose of creating a larger unified development site. It allows property owners to retain the incremental value gained from the development of their land to more intensive use rather than having the benefit accrue to the developer after the land is sold.

Properties are consolidated through a private corporation, landowners association, public corporation, or public agency. Owners are accorded shares relating their assessed property values as percentages of the total value of all properties combined. The land is then planned without regard to property lines and is resubdivided and returned to individual property owners with all development requirements having been satisfied. The project can then be built out separately by several developers or by a single developer. Some lots may be sold to offset the cost of infrastructure improvements. The result is that the original property owners realize greater value for their properties by creating a larger developable site.

9. **Capital Improvement Program.** Direct funding is allocated from either city or county budgets for specific public infrastructure or community revitalization initiatives.
10. **Gateway Community Development Corporation.** This corporation targets arts-related revitalization and economic development in the Gateway Arts District, which spans from Mount Rainier to Hyattsville.
11. **Public Parking.** Public parking is appropriate when a range of land uses, rather than a single user, benefit from the parking. The Prince George's County Revenue Authority's mission is to create revenue streams for the county and to encourage economic development. As initial costs may be high, partnerships with municipalities, the Redevelopment Authority, a business association, or other entities may be required. The Redevelopment Authority, with the approval of a municipality and the direction of the County Council, may create a parking district within any municipal commercial area. A parking district collects parking fees for all public parking spaces in the district from individual users, commercial center businesses, or an alternative entity such as a business association. Initial financing would come from non-city sources, but in the long run projected parking revenue must be sufficient to pay off construction, financing, and maintenance.

**12. Foreclosure Prevention.** Prince George’s County has the highest rate of foreclosures in the state. A variety of programs and services exist to prevent foreclosures. These include the Maryland Department of Housing and Community Development’s Bridge to HOPE Loan Program, Lifeline Refinance Mortgage Program, Homesaver Mortgage Refinance Program, and the Prince George’s County Department of Social Services’ Homelessness Prevention Program.

## Maryland State Programs

- 1. Maryland Economic Development Assistance Authority and Fund (MEDAAF).** Local Economic Development Opportunity targets expanding businesses that have already had dramatic economic development impacts on their communities. The local jurisdiction must sponsor the businesses and must participate in the form of either guarantees, direct loans or grants in amounts equal to at least 10 percent of the state’s financial assistance. Loans may be made up to \$5 million, while conditional loans and grants may be made up to \$2 million.
  - 2. Main Street Maryland.** This comprehensive downtown revitalization program was created in 1998 by the Maryland Department of Housing and Community Development. The program strives to strengthen the economic potential of Maryland’s traditional main streets and neighborhoods. The City of Takoma Park is a successful local example of the Main Street Maryland program.
  - 3. Maryland’s Job Creation Tax Credit.** This program provides income tax credits to businesses that create new jobs by encouraging them to expand or relocate in Maryland. In most cases, the credit is 2.5 percent of annual wages for all newly created, full-time jobs, subject to a limit of \$1,000 per new job. In a state enterprise zone, a federal empowerment zone, or a Maryland Department of Housing and Community Development designated neighborhood, the credit is increased to five percent of annual wages for all newly created full-time jobs. Tax credits are subject to a limit of \$1,500 per new job. The total credit earned by a qualified business entity may not exceed \$1 million per credit year.
- If the credit is more than the tax liability, the unused credit may be carried forward for five years following the credit year. The credit may be recaptured if the business experiences job losses.
- 4. One Maryland Economic Development Tax Credit.** Businesses can qualify for up to \$5.5 million in income tax credits under the income tax credit program. Businesses that invest in an economic development project in a “qualified distressed county” may qualify for project tax credits of up to \$5 million and start-up tax credits of up to \$500,000.
- Project tax credits of up to \$5 million are based on qualifying costs and expenses incurred by the business entity in connection with the acquisition, construction, rehabilitation, installation, and equipping of an eligible economic development project. Eligible costs may include land acquisition, performance and contract bonds, insurance, architectural and engineering services, environmental damage mitigation, and utility installation. Eligible project costs must be at least \$500,000; project costs in excess of \$5 million are not eligible for the project tax credit.
- 5. Start-up Tax Credits.** These are provided for the expense of moving a business into Maryland and for the costs of furnishing and equipping a new location for typical business functions. Examples of eligible start-up costs include the cost of fixed telecommunications equipment, office equipment, or office furnishings. The start-up credit earned may not exceed the lesser of \$500,000 of eligible start-up costs or \$10,000 times the number of new, qualified positions created.
  - 6. Regional or Local Revolving Loan Fund.** This program provides grants to local jurisdictions to capitalize local revolving loan funds. Eligible applicants include a county or regional economic development agency. A jurisdiction may transfer all or a portion of its allocation to a regional revolving loan fund. The Department of Business and Economic Development (DBED) may not make grants totaling more than \$2 million per fiscal year. To qualify for a grant, the local government must provide a matching grant of funds to the local revolving loan fund.



- 7. Maryland Industrial Development Financing Authority (MIDFA).** This program encourages private sector financing for economic development projects located in priority funding areas. MIDFA facilitates capital access by issuing private activity revenue bonds and providing credit insurance in the form of a deficiency guaranty to reduce lender's risk. While transaction size is not generally limited, the credit enhancement is subject to the applicable program limits. MIDFA based Private Activity Revenue Bonds comes in two forms, a Taxable Bond or Tax Exempt Bond. The first type of bond provides access to long-term capital markets for primarily fixed asset financing. The second type of bond provides access to long-term capital markets for fixed asset financing at tax-exempt rates. Eligibility is limited by federal tax law to 501(c)(3) non-profit organizations, manufacturing facilities, and certain energy projects. Additional limitations apply to each specific transaction type.

MIDFA also provides credit insurance through a conventional program that insures up to 80 percent, not to exceed \$2.5 million, of transactions made by a financial institution. Export transactions may be insured up to 90 percent. The second type of credit insurance is provided through a bond program that insures bonds up to 100 percent, not to exceed \$7.5 million, of taxable or tax-exempt bonds. A third type of insurance is associated with a linked deposit.

- 8. Maryland Economic Development Corporation.** This state enabled corporation issues bonds to finance public benefit developments.
- 9. Community Investment Tax Credits (CITC).** This program supports nonprofit projects by awarding allocations of state tax credits to the sponsoring organizations to use as incentives for business contributions. Any business may reduce its Maryland tax liability by contributing cash, goods, or real property to CITC projects. Contributions of real property are limited to designated nonprofits that sponsor community activities. The Maryland Department of Housing and Community Development (DHCD) must authorize the real property donation before making the contribution. Contributions of services are not eligible for tax credits. The business earns credits

equal to 50 percent of the contribution, in addition to deductions on both state and federal taxes as a result of the charitable contribution.

- 10. Offices and Commercial Space Conversion Initiative.** This initiative assists in the revitalization of Maryland's downtown areas by converting older office and commercial space into new, market rate, rental housing. The program is designed to supplement conventional financing. There are no income limits and processing requirements are limited to those that are necessary to ensure prudent lending practices and compliance with the program's statutory requirements. A recommendation from local government is required as a condition for the submission of an application.

- 11. Maryland Capital Access Program (MCAP).** This program is a revitalization resource that supports the growth and success of small businesses in priority funding areas throughout Maryland. MCAP is a small business credit enhancement program that enables private lenders to establish a loan loss reserve fund from fees paid by lenders, borrowers, and the State of Maryland.

Communities that have small businesses receiving financing through loans enrolled in MCAP will benefit from new or expanded services provided by the small businesses. Most Maryland small businesses, including nonprofit organizations, are eligible. Businesses must be located in Maryland's priority funding areas approved by the Maryland Department of Planning for state funding in accordance with the Smart Growth Act of 1997. Nearly all populated areas in the state are recognized PFAs. Participating lenders are federally insured financial institutions, institutions regulated by the Commissioner of Financial Regulation, and others who have a participation agreement with DHCD. An enrolled loan, or portion of a loan, may range from \$10,000 to \$1,000,000.

- 12. Neighborhood Business Works Program.** This program provides flexible gap financing in the form of below-market interest rate loans to small businesses and nonprofit organizations locating or expanding in locally designated neighborhood revitalization areas. Financing ranges from \$25,000 to \$500,000 for up to 50 percent of a project's

total cost. Eligibility requirements include the pre-lease rate of 51 percent before loan closing. The Maryland Department of Housing and Community Development administers this program. The program also requires that the applicant have a five percent minimum cash equity, based on total development cost, in the project. Personal guarantees and collateral are required. There are no application fees and no prepayment penalties.

### 13. State of Maryland Small Business Development

**Center.** The center provides a wide variety of services, including classes and one-on-one counseling. Many services are free. Free services include a Neighborhood Business Development Program that provides gap financing for new or expanding businesses for a wide range of needs, such as real estate acquisition and working capital. Loans of \$25,000-\$500,000, covering up to 50 percent of project costs, can be provided.

### 14. Small Business Development Financing

**Authority.** The authority provides a range of loans, bond guarantees, and equity investments targeted toward socially and economically disadvantaged business owners.

### 15. Community Legacy: Community Development Finance Institution (CDFI) Neighborhood

**Intervention.** CDFI provides financial assistance to individuals or businesses that are owner-occupants, community development organizations, or local governments for the purpose of buying properties in need of rehabilitation and located in stable neighborhoods. Neighborhood Intervention demolition provides funding to local governments to demolish properties that are dangerous to use or occupy, are so deteriorated that rehabilitation is not feasible, and are located in stable neighborhoods. Additionally, Neighborhood Intervention provides funding for projects sponsored by a local government for the purpose of demolishing improvements on property to prepare the property for revitalization, as part of a redevelopment plan.

### 16. Maryland Technical Assistance Grant Program.

This program provides funding for market and feasibility analyses administered through Maryland's

Department of Housing and Community Development Neighborhood Revitalization program.

**17. Art in Communities.** This is a quarterly grant program for Maryland nonprofit organizations that produce or present the arts.

**18. Community Arts Development.** This program provides funds and technical assistance to the local arts councils of the 23 counties of Maryland.

## Federal Programs

**1. New Market Tax Credits (NMTC).** Federal tax credits may be used to facilitate a project by providing an additional equity or financing source. Although these tax credits are federally provided, the Prince George's County Community Capital Corporation is the local intermediary that applies to the U.S. Treasury Department to receive NMTCs.

**2. Historically Underutilized Business Zones (HUB).** This program is located in census tracts that have been designated as "statutory mandated designation of qualified census tracts and difficult development areas" for Section 42 of the Internal Revenue Code of 1996. A non-metropolitan county area is also considered a HUB Zone if the median household income of the county is less than 80 percent of the non-metropolitan state median household income, or has an unemployment rate that is more than 1.4 times the statewide average.

**3. Small Business Administration (SBA).** The Federal Microloan Program is for loans of \$500-\$25,000. SBA authorizes over 100 micro lenders nationwide to administer the SBA Microloan program.

## Specific Property or Building Recommendations



PROPERTY AND BUILDING CLUSTER PROFILES LOCATION MAP.

The tables on the following pages provide details pertaining to the existing conditions and development potential for specific buildings and major development/redevelopment opportunity sites highlighted in the plan vision. The numbers on the above map indicate the location referred to in each of the tables.

The tables' Existing Condition sections include information such as: lot area, land use, building details, historic integrity, and whether a property is owned or leased.

The Development Plan Vision section provides information on how the plan envisions the particular site in the future.

The Implementation section provides some steps that should be taken to realize the vision and indicates whether the phasing for the vision is near, medium, or long term.



**ADDRESS:** 4000–4008 34TH STREET  
**DISTRICT:** UPPER 34TH STREET  
**ACTION:** BUILDING RENOVATION  
**KEY MAP:** 1



EXISTING CONDITION	
<b>Lot Area</b>	20,691 square feet
<b>Existing Improvements</b>	8,526 square feet
<b>Building Height</b>	10-15 feet (est.)
<b>Land Use</b>	Commercial/Retail
<b>Year Built</b>	1910
<b>Description/ Physical Condition</b>	Several one-story brick buildings in good condition.
<b>Leased or Owned</b>	Leased and Owned

DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial
<b>Product Type</b>	Retail
<b>Square Footage</b>	8,526 square feet
<b>Building Height</b>	10-15 feet (est.)
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	On and off-site
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Restore clerestory windows above storefront.</li> <li>• Repair existing and/or provide new historically compatible storefront, doors and windows.</li> <li>• Provide historically compatible lighting, signage and/or sign band.</li> <li>• Repair/replace cornice, coping.</li> <li>• Repair, point brick as required.</li> <li>• Coordinate signage/awnings.</li> </ul>
<b>Phasing</b>	Medium term
<b>Other Comments</b>	N/A

**ADDRESS:** 3840–3856 34TH STREET  
**DISTRICT:** UPPER 34TH STREET  
**ACTION:** BUILDING RENOVATION  
**KEY MAP:** 2



EXISTING CONDITION	
<b>Lot Area</b>	5,489 square feet
<b>Existing Improvements</b>	9,760 square feet
<b>Building Height</b>	30 feet (est.)
<b>Land Use</b>	Commercial/Mixed-Use
<b>Year Built</b>	1927
<b>Description/ Physical Condition</b>	Two-story brick building in average condition with some vacancy.
<b>Leased or Owned</b>	Leased

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Mixed-Use
<b>Product Type</b>	Retail and Residential/ Restaurant
<b>Square Footage</b>	9,760 square feet
<b>Building Height</b>	30 feet (est.)
<b>Historic Preservation</b>	Protected historic site
<b>Parking</b>	Off-site
<b>Other</b>	Outdoor Seating at Corner

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Provide new historically compatible storefront doors and windows.</li> <li>• Provide historically compatible lighting.</li> <li>• Repair signage and/or sign band.</li> <li>• Repair/replace cornice and coping.</li> <li>• Replace second floor windows with historically compatible windows.</li> <li>• Repair and point brick as necessary.</li> </ul>
<b>Phasing</b>	Near to Medium term
<b>Other Comments</b>	Requires historic design review process. See page 61.

**ADDRESS:** 3820–3822 34TH STREET  
**DISTRICT:** UPPER 34TH STREET  
**ACTION:** BUILDING RENOVATION  
**KEY MAP:** 3



EXISTING CONDITION	
<b>Lot Area</b>	12,023 square feet
<b>Existing Improvements</b>	8,164 square feet
<b>Building Height</b>	20 feet (est.)
<b>Land Use</b>	Institutional
<b>Year Built</b>	1920, 1940
<b>Description/ Physical Condition</b>	One-story brick building in good condition.
<b>Leased or Owned</b>	Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Institutional
<b>Product Type</b>	Church
<b>Square Footage</b>	8,164 square feet
<b>Building Height</b>	20 feet (est.)
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	On-site
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Replace windows and doors with historically compatible elements.</li> <li>• Provide consistent signage as necessary.</li> <li>• Repair and point brick as necessary.</li> </ul>
<b>Phasing</b>	Medium term
<b>Other Comments</b>	N/A



**ADDRESS: BETWEEN 3817 AND 3827  
34TH STREET**  
**DISTRICT: UPPER 34TH STREET**  
**ACTION: INFILL  
DEVELOPMENT**  
**KEY MAP: 4**



EXISTING CONDITION	
<b>Lot Area</b>	4,748 square feet
<b>Existing Improvements</b>	N/A
<b>Building Height</b>	N/A
<b>Land Use</b>	Vacant
<b>Year Built</b>	N/A
<b>Description/ Physical Condition</b>	Small vacant Lot.
<b>Leased or Owned</b>	Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial
<b>Product Type</b>	Commercial
<b>Square Footage</b>	TBD
<b>Building Height</b>	TBD
<b>Historic Preservation</b>	N/A
<b>Parking</b>	TBD
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>Potential location for one- to three-story infill building.</li> </ul>
<b>Phasing</b>	Medium term
<b>Other Comments</b>	Possible access to 3424–3428 Rhode Island Avenue development site and parking.

**ADDRESS:** 3829 34TH STREET  
**DISTRICT:** UPPER 34TH STREET  
**ACTION:** BUILDING RENOVATION  
**KEY MAP:** 5



EXISTING CONDITION	
<b>Lot Area</b>	6,839 square feet
<b>Existing Improvements</b>	2,530 square feet
<b>Building Height</b>	35 feet (est.)
<b>Land Use</b>	Commercial
<b>Year Built</b>	1920
<b>Description/ Physical Condition</b>	Three-story building in average to good condition.
<b>Leased or Owned</b>	Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial
<b>Product Type</b>	Retail
<b>Square Footage</b>	2,530 square feet
<b>Building Height</b>	35 feet (est.)
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	Off-site
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Add new historically-compatible front addition to expand interior retail space.</li> <li>• Remove existing ramp and install more functional new one.</li> <li>• Remove vinyl siding.</li> <li>• Provide historically compatible signage.</li> <li>• Repair wood elements.</li> </ul>
<b>Phasing</b>	Near term
<b>Other Comments</b>	N/A

**ADDRESS: 3841 34TH STREET**  
**DISTRICT: UPPER 34TH STREET**  
**ACTION: INFILL DEVELOPMENT**  
**KEY MAP: 6**



EXISTING CONDITION	
<b>Lot Area</b>	5,619 square feet
<b>Existing Improvements</b>	N/A
<b>Building Height</b>	N/A
<b>Land Use</b>	Vacant (former house)
<b>Year Built</b>	N/A
<b>Description/ Physical Condition</b>	Vacant Lot
<b>Leased or Owned</b>	Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial or Mixed-Use
<b>Product Type</b>	TBD
<b>Square Footage</b>	TBD
<b>Building Height</b>	TBD
<b>Historic Preservation</b>	N/A
<b>Parking</b>	TBD
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>Potential location for one- to three-story infill building.</li> </ul>
<b>Phasing</b>	Long term
<b>Other Comments</b>	Possible interim use as a surface shared parking lot.



**ADDRESS:** 3401 BUNKER HILL ROAD  
**DISTRICT:** UPPER 34TH STREET  
**ACTION:** BUILDING RENOVATION  
**KEY MAP:** 7



EXISTING CONDITION	
<b>Lot Area</b>	8,712 square feet
<b>Existing Improvements</b>	4,720 square feet
<b>Building Height</b>	30 feet (est.)
<b>Land Use</b>	Commercial
<b>Year Built</b>	1912 (original building)
<b>Description/ Physical Condition</b>	Two and one-half story brick building with storefront addition in good condition.
<b>Leased or Owned</b>	Leased

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial
<b>Product Type</b>	Commercial
<b>Square Footage</b>	TBD
<b>Building Height</b>	30 feet (est.)
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	Off-site
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Provide a new historically compatible storefront, doors, and windows.</li> <li>• Provide historically compatible lighting, signage, and/or sign band.</li> <li>• Repair and point brick as necessary.</li> <li>• Remove the incompatible front addition and repair existing wood frame/ masonry building.</li> <li>• Re-install historic porch, roof, and window elements.</li> </ul>
<b>Phasing</b>	Medium term
<b>Other Comments</b>	Explore the possibility of removing the storefront addition and restoring the historic facade and porch to its original integrity.

**ADDRESS:** 4001–4009 34TH STREET  
**DISTRICT:** UPPER 34TH STREET  
**ACTION:** BUILDING RENOVATION  
**KEY MAP:** 8



EXISTING CONDITION	
<b>Lot Area</b>	6,883 square feet
<b>Existing Improvements</b>	5,382 square feet
<b>Building Height</b>	20 feet (est.)
<b>Land Use</b>	Commercial/Retail
<b>Year Built</b>	1910, 1915, 1937
<b>Description/ Physical Condition</b>	Several one-story brick buildings in average condition.
<b>Leased or Owned</b>	Leased and Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial
<b>Product Type</b>	Retail
<b>Square Footage</b>	5,382 square feet
<b>Building Height</b>	20 feet (est.)
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	Off-site
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Paint and provide consistent signage.</li> <li>• Repair storefront windows.</li> <li>• Provide consistent awning design.</li> <li>• Provide outdoor display areas, if possible.</li> <li>• Repair and point brick as necessary.</li> </ul>
<b>Phasing</b>	Medium term
<b>Other Comments</b>	N/A

**ADDRESS:** 3200–3208 RHODE ISLAND AVENUE & 3701-3705 EASTERN AVENUE  
**DISTRICT:** RHODE ISLAND AVENUE  
**ACTION:** INFILL DEVELOPMENT  
**KEY MAP:** 9



EXISTING CONDITION	
<b>Lot Area</b>	32,845 square feet
<b>Existing Improvements</b>	7,424 square feet
<b>Building Height</b>	20 feet (est.)
<b>Land Use</b>	Commercial
<b>Year Built</b>	1920
<b>Description/ Physical Condition</b>	Six lots combined and improved with a single-family residences.
<b>Leased or Owned</b>	City Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Residential
<b>Product Type</b>	Multifamily/Stacked Townhouse
<b>Square Footage</b>	+65,000 square feet
<b>Building Height</b>	50 feet
<b>Historic Preservation</b>	N/A
<b>Parking</b>	On-site
<b>Other</b>	Possible Limited Retail.

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>Two- to five-story development to serve as a “signature gateway building” into Mount Rainier (see Infill and Height Standards and Guidelines in the Design Standards and Guidelines chapter).</li> </ul>
<b>Phasing</b>	Near to medium term
<b>Other Comments</b>	Coordinate redevelopment with adjacent properties in the 3200 block of Rhode Island Avenue.



**ADDRESS:** 3210–3212 RHODE ISLAND AVENUE  
**DISTRICT:** RHODE ISLAND AVENUE  
**ACTION:** INFILL DEVELOPMENT  
**KEY MAP:** 10



EXISTING CONDITION	
<b>Lot Area</b>	17,404 square feet
<b>Existing Improvements</b>	8,910 square feet
<b>Building Height</b>	30 feet (est.)
<b>Land Use</b>	Commercial
<b>Year Built</b>	1940
<b>Description/ Physical Condition</b>	Two-story brick building with adjacent surface parking lot.
<b>Leased or Owned</b>	Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Mixed-Use
<b>Product Type</b>	Residential with Some Retail
<b>Square Footage</b>	+25,000 square feet
<b>Building Height</b>	TBD
<b>Historic Preservation</b>	N/A
<b>Parking</b>	On-site
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>Two- to five-story development to serve as a “signature gateway building” into Mount Rainier (see Infill and Height Standards and Guidelines in the Design Standards and Guidelines chapter).</li> </ul>
<b>Phasing</b>	Near to medium term (Priority)
<b>Other Comments</b>	Coordinate redevelopment with adjacent properties in the 3200 block of Rhode Island Avenue.

**ADDRESS:** VACANT LOT 3212 & 3220  
**RHODE ISLAND AVENUE**  
**DISTRICT:** RHODE ISLAND AVENUE  
**ACTION:** INFILL  
**DEVELOPMENT**  
**KEY MAP:** 11



EXISTING CONDITION	
<b>Lot Area</b>	2,483 square feet.
<b>Existing Improvements</b>	N/A
<b>Building Height</b>	N/A
<b>Land Use</b>	Commercial
<b>Year Built</b>	N/A
<b>Description/ Physical Condition</b>	Surface Parking Lot
<b>Leased or Owned</b>	Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Civic
<b>Product Type</b>	Community Flex Space
<b>Square Footage</b>	TBD
<b>Building Height</b>	TBD
<b>Historic Preservation</b>	N/A
<b>Parking</b>	Integrate with Adjacent Parcels.
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>Two- to five-story development to serve as a “signature gateway building” into Mount Rainier (see Infill and Height Standards and Guidelines in the Design Standards and Guidelines Chapter).</li> </ul>
<b>Phasing</b>	Near to medium term
<b>Other Comments</b>	Coordinate redevelopment with adjacent properties in the 3200 block of Rhode Island Avenue.

**ADDRESS:** 3220 RHODE ISLAND AVENUE  
**DISTRICT:** RHODE ISLAND AVENUE  
**ACTION:** BUILDING RENOVATION  
**KEY MAP:** 12



EXISTING CONDITION	
<b>Lot Area</b>	5,401 square feet.
<b>Existing Improvements</b>	520 square feet
<b>Building Height</b>	20 feet (est.)
<b>Land Use</b>	Commercial
<b>Year Built</b>	1934
<b>Description/ Physical Condition</b>	One-story former gas station in poor condition (vacant).
<b>Leased or Owned</b>	Owned

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Stabilize structure immediately.</li> <li>• Research and document original condition of building.</li> <li>• Integrate the roof and steel beam at porte cochere.</li> <li>• Repair clay tile roof.</li> <li>• Restore brick as necessary.</li> <li>• Replace/restore windows and doors.</li> <li>• Repair frieze</li> <li>• Sensitively integrate with adjacent development.</li> </ul>
<b>Phasing</b>	Near term
<b>Other Comments</b>	Coordinate redevelopment with adjacent properties in the 3200 block of Rhode Island Avenue.

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial
<b>Product Type</b>	Retail
<b>Square Footage</b>	TBD
<b>Building Height</b>	One-story
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	Integrate with Adjacent Parcels.
<b>Other</b>	Active Adjacent Sidewalk.



**ADDRESS:** 3201 RHODE ISLAND AVENUE  
**DISTRICT:** RHODE ISLAND AVENUE  
**ACTION:** BUILDING RENOVATION  
**KEY MAP:** 13



EXISTING CONDITION	
<b>Lot Area</b>	13,634 square feet
<b>Existing Improvements</b>	34,133 square feet
<b>Building Height</b>	+40 feet (est.)
<b>Land Use</b>	Commercial/Apartments
<b>Year Built</b>	1938
<b>Description/ Physical Condition</b>	Four-story brick building in average to good condition.
<b>Leased or Owned</b>	Leased and Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Mixed-Use
<b>Product Type</b>	Commercial/Apartments
<b>Square Footage</b>	34,133 square feet
<b>Building Height</b>	+40 feet (est.)
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	Off-site
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Replace vinyl windows with uniform wood/metal.</li> <li>• Repair and point brick as necessary.</li> <li>• Replace coping, gutters and down spouts.</li> <li>• Locate coordinated sign band.</li> <li>• Repair/restore brushed aluminum awning and/ or install consistent new awning.</li> <li>• Remove grilles.</li> <li>• Provide exterior lighting</li> <li>• Install new storefronts of brushed/natural aluminum.</li> <li>• Provide proper drainage at rear of building.</li> </ul>
<b>Phasing</b>	Medium term
<b>Other Comments</b>	N/A

**ADDRESS:** 3231–3239 RHODE ISLAND AVENUE  
**DISTRICT:** RHODE ISLAND AVENUE  
**ACTION:** BUILDING RENOVATION/  
 ADDITION  
**KEY MAP:** 14

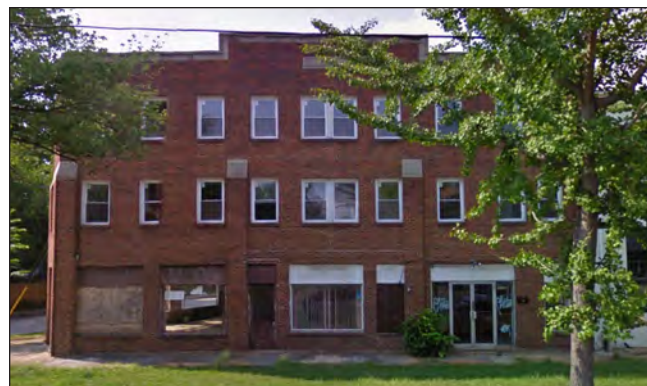


EXISTING CONDITION	
<b>Lot Area</b>	23,348 square feet
<b>Existing Improvements</b>	17,010 square feet
<b>Building Height</b>	10–20 feet (est.)
<b>Land Use</b>	Commercial
<b>Year Built</b>	1920, 1940, and 1965
<b>Description/ Physical Condition</b>	One one-story and two two-story commercial buildings.
<b>Leased or Owned</b>	Leased and Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial or Mixed-Use
<b>Product Type</b>	Commercial
<b>Square Footage</b>	TBD
<b>Building Height</b>	TBD
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	Off-site
<b>Other:</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>Potential one- to two story addition to the existing buildings or an up to four-story replacement building on the sites.</li> </ul>
<b>Phasing</b>	Long term
<b>Other Comments</b>	Potential to rebuild and/or add floors to building in order to establish a continuous multi-story street front along Rhode Island Avenue.

**ADDRESS:** 3300 RHODE ISLAND AVENUE  
**DISTRICT:** CIVIC CORE  
**ACTION:** BUILDING REDEVELOPMENT  
**KEY MAP:** 15



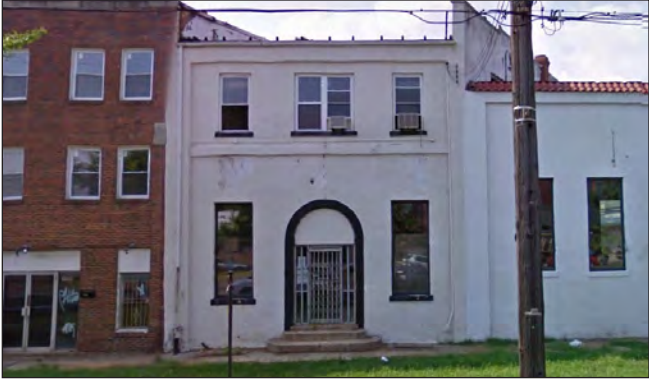
EXISTING CONDITION	
<b>Lot Area</b>	5,881 square feet
<b>Existing Improvements</b>	11,849 square feet
<b>Building Height</b>	40 feet (est.)
<b>Land Use</b>	Mixed-Use Commercial and Residential
<b>Year Built</b>	1936
<b>Description/ Physical Condition</b>	Three-story brick building in poor condition (vacant for 20 years).
<b>Leased or Owned</b>	For Sale

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Mixed-Use
<b>Product Type</b>	Retail and Residential
<b>Square Footage</b>	11,849 square feet
<b>Building Height</b>	40 feet/3-stories
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	Off-site
<b>Other:</b>	Active Adjacent Sidewalk.

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Repair or replace windows and doors with consistent brushed aluminum (or historically accurate) metal windows.</li> <li>• Clean, point, and repair brick.</li> <li>• Repair the stone cap.</li> <li>• Resolve front drainage issues.</li> <li>• Locate sign band.</li> <li>• Provide lighting.</li> </ul>
<b>Phasing</b>	Near term (priority)
<b>Other Comments</b>	Coordinate redevelopment with 3308 and 3310 Rhode Island Avenue.



**ADDRESS:** 3308 RHODE ISLAND AVENUE  
**DISTRICT:** CIVIC CORE  
**ACTION:** BUILDING REDEVELOPMENT  
**KEY MAP:** 16



EXISTING CONDITION	
<b>Lot Area</b>	1,481 square feet
<b>Existing Improvements</b>	2,200 square feet
<b>Building Height</b>	35 feet (est.)
<b>Land Use</b>	Commercial/Retail
<b>Year Built</b>	1925
<b>Description/ Physical Condition</b>	Two-story building in fair condition (vacant).
<b>Leased or Owned</b>	For Lease

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial
<b>Product Type</b>	Retail/Office
<b>Square Footage</b>	2,200 square feet (est.)
<b>Building Height</b>	35 feet/2 stories
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	Off-site
<b>Other:</b>	Active Adjacent Sidewalk.

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Repair or restore stucco.</li> <li>• Replace gutters and downspouts.</li> <li>• Repair clay tile roofing.</li> <li>• Locate coordinated sign band or awning element.</li> <li>• Replace windows and doors with historically accurate elements.</li> </ul>
<b>Phasing</b>	Near term
<b>Other Comments</b>	Coordinate redevelopment with 3300 and 3310 Rhode Island Avenue.

**ADDRESS:** 3310 RHODE ISLAND AVENUE  
**DISTRICT:** CIVIC CORE  
**ACTION:** BUILDING REDEVELOPMENT  
**KEY MAP:** 17



EXISTING CONDITION	
<b>Lot Area</b>	1,525 square feet
<b>Existing Improvements</b>	1,650 square feet
<b>Building Height</b>	35 feet (est.)
<b>Land Use</b>	Commercial/Retail
<b>Year Built</b>	1966
<b>Description/ Physical Condition</b>	One-story building in fair condition (vacant).
<b>Leased or Owned</b>	For Lease

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial
<b>Product Type</b>	Retail/Office
<b>Square Footage</b>	1,650 square feet (est.)
<b>Building Height</b>	35 feet/2 stories
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	Off-site
<b>Other</b>	Active Adjacent Sidewalk.

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Repair or restore stucco.</li> <li>• Replace gutters and downspouts.</li> <li>• Repair clay tile roofing.</li> <li>• Locate coordinated sign band or awning element.</li> <li>• Replace windows and doors with historically accurate elements.</li> </ul>
<b>Phasing</b>	Near term
<b>Other Comments</b>	Coordinate redevelopment with 3300 and 3310 Rhode Island Avenue.

**ADDRESS:** 3208–3210 PERRY STREET  
**DISTRICT:** CIVIC CORE  
**ACTION:** OPEN SPACE OR INFILL DEVELOPMENT  
**KEY MAP:** 18



EXISTING CONDITION	
<b>Lot Area</b>	11,979 square feet
<b>Existing Improvements</b>	N/A
<b>Building Height</b>	N/A
<b>Land Use</b>	Temporary Garden
<b>Year Built</b>	N/A
<b>Description/ Physical Condition</b>	Two vacant lots surrounded by single-family and multifamily dwellings.
<b>Leased or Owned</b>	Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Park Space or Low Density Residential
<b>Product Type</b>	TBD
<b>Square Footage</b>	TBD
<b>Building Height</b>	TBD
<b>Historic Preservation</b>	N/A
<b>Parking</b>	On-site (if Residential)
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Park space or low-scale residential.</li> </ul>
<b>Phasing</b>	Near term
<b>Other Comments</b>	Coordinate redevelopment with City of Mount Rainier as either a city park or low-scale residential.



**ADDRESS:** 3800 34TH STREET  
**DISTRICT:** CIVIC CORE  
**ACTION:** BUILDING RENOVATION  
**KEY MAP:** 19



EXISTING CONDITION	
<b>Lot Area</b>	1,699 square feet
<b>Existing Improvements</b>	3,024 square feet
<b>Building Height</b>	30 feet (est.)
<b>Land Use</b>	Commercial
<b>Year Built</b>	1922
<b>Description/ Physical Condition</b>	Two-story brick building in good condition.
<b>Leased or Owned</b>	Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial
<b>Product Type</b>	Commercial
<b>Square Footage</b>	3,024 square feet
<b>Building Height</b>	30 feet (est.)
<b>Historic Preservation</b>	Protected historic site
<b>Parking</b>	Off-site
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Consider public use for Prince George’s Bank Building.</li> <li>• Repair and/or replace windows and doors with historically compatible elements.</li> <li>• Repair brick/stone; repair trim bands.</li> <li>• Replace/restore historic cornice at entry.</li> <li>• Preserve and restore historic copper roof of dome.</li> </ul>
<b>Phasing</b>	Medium term
<b>Other Comments</b>	The building is situated on a high profile corner location. Requires historic design review process. See page 61.

**ADDRESS:** 3802–3812 34TH STREET  
**DISTRICT:** CIVIC CORE  
**ACTION:** BUILDING RENOVATION  
**KEY MAP:** 20



EXISTING CONDITION	
<b>Lot Area</b>	17,860 square feet
<b>Existing Improvements</b>	6,481 square feet
<b>Building Height</b>	15 feet (est.)
<b>Land Use</b>	Commercial
<b>Year Built</b>	1910
<b>Description/ Physical Condition</b>	One-story brick buildings.
<b>Leased or Owned</b>	Leased

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial/Mixed-Use
<b>Product Type</b>	Retail/Commercial-Office
<b>Square Footage</b>	6,481 square feet
<b>Building Height</b>	15 feet (est.)
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	Off-site
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Provide consistent signage, lighting and awnings.</li> <li>• Repair storefronts as necessary.</li> </ul>
<b>Phasing</b>	Medium term
<b>Other Comments</b>	N/A

**ADDRESS:** 3805 34TH STREET  
**DISTRICT:** CIVIC CORE  
**ACTION:** INFILL DEVELOPMENT  
**KEY MAP:** 21



EXISTING CONDITION	
<b>Lot Area</b>	871 square feet
<b>Existing Improvements</b>	N/A
<b>Building Height</b>	N/A
<b>Land Use</b>	Vacant
<b>Year Built</b>	N/A
<b>Description/ Physical Condition</b>	Small Vacant Lot (former building).
<b>Leased or Owned</b>	Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial
<b>Product Type</b>	TBD
<b>Square Footage</b>	TBD
<b>Building Height</b>	TBD
<b>Historic Preservation</b>	N/A
<b>Parking</b>	TBD
<b>Other:</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Potential location for one- to three-story infill building.</li> </ul>
<b>Phasing</b>	Medium term
<b>Other Comments</b>	Potential for redevelopment in conjunction with 3424–3428 Rhode Island Avenue.



**ADDRESS:** 3402–3416 RHODE ISLAND AVENUE  
**DISTRICT:** CIVIC CORE  
**ACTION:** BUILDING RENOVATION  
**KEY MAP:** 22



EXISTING CONDITION	
<b>Lot Area</b>	9,475 square feet
<b>Existing Improvements</b>	10,879 square feet
<b>Building Height</b>	30 feet (est.)
<b>Land Use</b>	Commercial
<b>Year Built</b>	1927, 1935
<b>Description/ Physical Condition</b>	Several two-story brick buildings in average to good condition.
<b>Leased or Owned</b>	Leased and Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Commercial
<b>Product Type</b>	Retail
<b>Square Footage</b>	10,696 square feet
<b>Building Height</b>	30 feet (est.)
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	Off-site
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Provide new storefronts, doors, and windows as necessary.</li> <li>• Repair trim work and frieze.</li> <li>• Provide appropriate lighting, signage, and sign band.</li> <li>• Remove siding.</li> </ul>
<b>Phasing</b>	Medium to long term
<b>Other Comments</b>	N/A

**ADDRESS:** 3424–3428 RHODE ISLAND AVENUE  
**DISTRICT:** CIVIC CORE  
**ACTION:** INFILL DEVELOPMENT  
**KEY MAP:** 23



EXISTING CONDITION	
<b>Lot Area</b>	12,676 square feet
<b>Existing Improvements</b>	N/A
<b>Building Height</b>	N/A
<b>Land Use</b>	Surface Parking Lot
<b>Year Built</b>	N/A
<b>Description/ Physical Condition</b>	Two vacant lots.
<b>Leased or Owned</b>	Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Mixed-Use
<b>Product Type</b>	Residential/Commercial
<b>Square Footage</b>	TBD
<b>Building Height</b>	TBD
<b>Historic Preservation</b>	N/A
<b>Parking</b>	On-site
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Potential location for three- to four-story infill building with a shared parking lot at the rear of the site.</li> </ul>
<b>Phasing</b>	Medium term
<b>Other Comments</b>	Redevelopment may include existing adjacent buildings.

**ADDRESS:** 3405 RHODE ISLAND AVENUE  
**DISTRICT:** CIVIC CORE  
**ACTION:** BUILDING RENOVATION ADDITION  
**KEY MAP:** 24



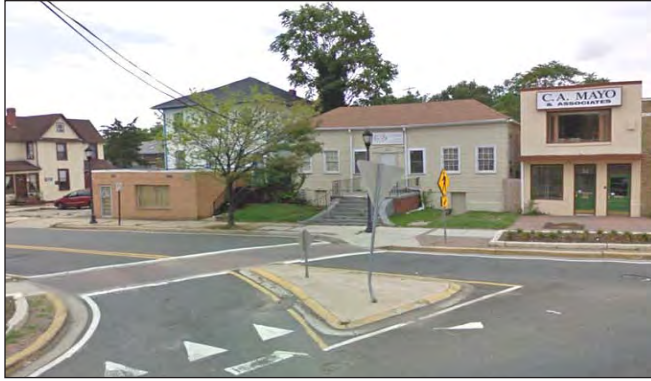
EXISTING CONDITION	
<b>Lot Area</b>	2,614 square feet
<b>Existing Improvements</b>	4,088 square feet
<b>Building Height</b>	+50 feet (est.)
<b>Land Use</b>	Commercial
<b>Year Built</b>	1902
<b>Description/ Physical Condition</b>	Two and one-half story brick building in good condition.
<b>Leased or Owned</b>	City Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Civic
<b>Product Type</b>	City Hall Expansion/Civic
<b>Square Footage</b>	4,088 square feet
<b>Building Height</b>	+50 feet (est.)
<b>Historic Preservation</b>	Protected historic resource
<b>Parking</b>	Off-site
<b>Other</b>	Potential Façade Addition.

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Build new addition on façade facing civic green (use appropriate fenestration).</li> <li>• Research and restore original Rhode Island Avenue-facing façade.</li> <li>• Renovate interior to better fit the city’s program needs.</li> <li>• Build new additions to connect building with city hall and city library.</li> </ul>
<b>Phasing</b>	Near term (priority)
<b>Other Comments</b>	Requires historic design review process. Refer to page 61.



**ADDRESS:** 3403–3415 PERRY STREET  
**DISTRICT:** CIVIC CORE  
**ACTION:** INFILL DEVELOPMENT  
**KEY MAP:** 25



EXISTING CONDITION	
<b>Lot Area</b>	25,309 square feet
<b>Existing Improvements</b>	7,830 square feet
<b>Building Height</b>	30 feet (est.)
<b>Land Use</b>	Commercial
<b>Year Built</b>	1900-1930
<b>Description/ Physical Condition</b>	Multiple buildings and a surface parking lot in good condition.
<b>Leased or Owned</b>	Leased and Owned

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Mixed-Use
<b>Product Type</b>	TBD
<b>Square Footage</b>	TBD
<b>Building Height</b>	TBD
<b>Historic Preservation</b>	Design Standards and Guidelines
<b>Parking</b>	TBD
<b>Other</b>	N/A

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Potential three- to four story infill development on the sites.</li> </ul>
<b>Phasing</b>	Long term
<b>Other Comments</b>	Possible long-term redevelopment opportunity with lot consolidation. Possible interim use as a surface shared parking lot on the vacant lot.

**ADDRESS: 3401 PERRY STREET**  
**DISTRICT: CIVIC CORE**  
**ACTION: BUILDING**  
**RENOVATION**  
**KEY MAP: 26**



EXISTING CONDITION	
<b>Lot Area</b>	3,746 square feet
<b>Existing Improvements</b>	1,875 square feet
<b>Building Height</b>	15 feet (est.)
<b>Land Use</b>	Institutional
<b>Year Built</b>	1930
<b>Description/ Physical Condition</b>	One-story brick building in good condition.
<b>Leased or Owned</b>	Owned

IMPLEMENTATION	
<b>Specific Property or Building Recommendations</b>	<ul style="list-style-type: none"> <li>• Repair storefront elements.</li> <li>• Provide coordinated signage as necessary.</li> <li>• Repair and point brick as necessary.</li> <li>• Provide lighting.</li> <li>• Restore awning.</li> </ul>
<b>Phasing</b>	Medium term
<b>Other Comments</b>	Requires historic design review process. See page 61.

PROPOSED DEVELOPMENT PLAN VISION	
<b>Land Use</b>	Institutional
<b>Product Type</b>	Church
<b>Square Footage</b>	1,875 square feet
<b>Building Height</b>	15 feet (est.)
<b>Historic Preservation</b>	Protected historic site.
<b>Parking</b>	On- and off-site
<b>Other</b>	N/A







Appendices

## Gateway Arts District Uses Permitted

The letter P indicates that the use is permitted in the character area. The letters SP indicate that the use is permitted with a special permit and will require a detailed site plan (DSP) process. (Applicants will be assigned a DSP and an SP for all applications within the Gateway Arts District.) The letter X indicates that the use is prohibited. The source column references the location in the Zoning Ordinance that is the basis for the use in the table. In some cases the use as it appears in the table is verbatim, in other cases the description of a use that appears in the Zoning Ordinance has been modified but is based on the Zoning Ordinance description. Where there is no reference to the Zoning Ordinance, the uses are being added via the Development District Overlay Zone and did not previously exist in the Zoning Ordinance. If the use is not listed in the table, it is prohibited.

It should be noted that this table controls the underlying uses listed in the Zoning Ordinance. Thus, some of the uses permitted in a character area might be prohibited in the underlying zone's use table as listed in the Zoning Ordinance. For example, many residential uses are prohibited in industrial zones but are now permitted in the I-2 Zone within the arts production and entertainment character area because it is consistent with the goals of the Arts District. In addition, many uses that have an adverse impact upon the character area due to scale, traffic, noise or other elements may be permitted in the underlying zones but, as listed in the table, are prohibited by the Gateway Arts Development District Overlay Zone. However, the affected properties are very limited and would not create adverse impact to their surrounding development. As a result, there are very few zoning changes to the underlying zones in the sectional map amendment section.

In general, most commercial and industrial zones now support a mix of uses of varying scale and intensity. Uses that have been added include arts production, art-related services, artistic endeavors, computers and digital arts, lifestyle uses (such as café and bakery), and dwelling styles that support artists.

Certain uses have been limited as disruptive and out of scale with the character area. For example, institutional uses will no longer be allowed within single-family neighborhoods except as an adaptive reuse of an existing institutional building. Uses incompatible with either pedestrian-orientation or family aspects of character areas have been similarly limited.

### Legend:

TC	Town Center
APE	Arts Production and Entertainment
NAP	Neighborhood Arts and Production
MRC	Multifamily Residential Community
TRN	Traditional Residential Neighborhood
NC	Neighborhood Commercial
SVP	Stream Valley Park

USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
<b>(1) ARTS-RELATED USES:</b>							
Private or public arts institutions for the promotion, practice, and dissemination of information related to the fine arts and handcrafts	P	P	SP	X	X	X	X
Private or public multipurpose arts center (for multiple uses including administration, community space, studio space, information dissemination, arts production, publication, classes, gallery space, and events)	P	P	SP	X	X	X	X
Art gallery	P	P	SP	X	X	X	X
Art school, including dance, photography, filmmaking, music, writing, painting, sculpting, printmaking, or glassmaking	P	P	SP	X	X	X	SP
Artist studio as a working space for use by a fine arts artist or artist collective, hand crafter, designer, or digital media producer in day to day production, which may be used for auxiliary instruction and auxiliary sales of items produced on-site:							
(i) As an accessory to an allowed use	SP	SP	SP	SP	SP	SP	X
(ii) All others	P	P	P	X	X	P	X
Craftsman or artisan primarily design and production shop that may include auxiliary sales, for small-scale production of goods including but not limited to musical instruments, papermaking, jewelry, and puppet making	P	P	P	X	SP	P	X
Rehearsal space	P	P	P	X	X	X	X
Sound stage for TV/film recording studio	X	P	SP	X	X	X	X
Arts services, including set design or construction and restoration of artworks	P	P	P	X	X	P	X
Concert hall or other performing arts space	P	P	SP	X	X	X	X
Outdoor performance sites	P	P	SP	SP	SP	SP	P
Dinner theater	P	P	SP	X	X	X	X
Performing arts ticket office or booking agency	P	X	X	X	X	X	X
Performing arts ticket office or booking agency as auxiliary use	P	P	SP	X	X	X	X
Artists' supply store	P	P	SP	X	X	P	X
Bead, jewelry supply	P	P	SP	X	X	P	X
Blueprint/GIS Mapping/architectural supply store	P	P	SP	X	X	X	X
Cafés, for table and/or bar service of tea, coffee, other beverages, and food	P	P	SP	X	X	P	X
Card shop	P	P	SP	X	X	P	X
Ceramic supply	P	P	SP	X	X	X	X
Costume rental	P	P	SP	X	X	X	X
Digital services, e.g., web, sound, or graphic design and editing	P	P	SP	X	X	P	X
Ice cream shop	P	P	SP	X	X	P	X
Fabric, knitting or rug making supply store	P	P	SP	X	X	P	X
Fashion design studio	P	P	SP	X	X	P	X
Mail box service	P	P	SP	X	X	P	X
Musical recording or musical instruments store	P	P	SP	X	X	P	X
Paper and supplies store	P	P	SP	X	X	P	X
Picture framing shop	P	P	SP	X	X	P	X
Live/work unit	P	P	P	X	SP	P	X



USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
<b>(2) COMMERCIAL:</b>							
Distillery for the production of fuel alcohol	X	X	X	X	X	X	X
Limited professional uses in multifamily projects	P	P	SP	P	X	P	X
<b>(A) Eating or Drinking Establishments:</b>							
Drive-in restaurant	X	X	X	X	X	X	X
Fast-food restaurant	SP	SP	X	X	X	SP	X
Other than a drive-in or fast-food restaurant (which may include incidental carry-out service, except where specifically prohibited):							
(i) Permitting no entertainment other than music, and no patron dancing with hours that do not extend beyond 11 P.M.:							
(a) On properties of five acres or greater.	X	X	X	X	X	X	P
(b) All others	P	P	SP	X	X	P	X
(ii) Permitting live entertainment <sup>1</sup> or patron dancing with hours of operation that extend beyond 11:00 p.m. and not within 60 feet of single-family residentially zoned property.	P	P	SP	X	X	X	X
<b>(B) Vehicle, Mobile Home, Camping Trailer, and Boat Sales and Service:</b>							
Bus maintenance accessory to a private school, educational institution, church, or other place of worship	X	X	X	X	X	X	X
Boat fuel sales at the waterfront	X	X	X	X	X	X	X
Boat sales, rental, service, and repair, including outdoor storage of boats and boat trailers	X	X	X	X	X	X	SP
Boat storage yard	X	X	X	X	X	X	SP
Car wash	X	X	X	X	X	X	X
Gas station (in the C-M Zone, subject to detailed site plan review in accordance with Section 27-358(a)(1),(2),(4),(5),(6),(7),(8),(9), and (10))	X	X	X	X	X	X	X
Incidental automobile service in a parking garage	X	X	X	X	X	X	X
Vehicle lubrication or tune-up facility, provided all sales and installation operations are conducted in a wholly enclosed building with no outdoor storage	X	X	X	X	X	X	X
Vehicle, mobile home, or camping trailer repair and service station	X	X	X	X	X	X	X
Vehicle, mobile home, or camping trailer sales or rental lot, including outdoor display of vehicles	X	X	X	X	X	X	X
Vehicle or camping trailer rental (in the C-M Zone, subject to Section 27-417(a),(b)(2), and (c))	X	X	X	X	X	X	X
Vehicle or camping trailer storage yard	X	X	X	X	X	X	X
Vehicle parts or tire store including installation facilities, provided all sales and installation operations are conducted in a wholly enclosed building with no outdoor storage	X	X	X	X	X	X	X
Vehicle towing station	X	X	X	X	X	X	X
Mobile or modular home sales lot, which may include the storage of mobile homes or modular home components for sale	X	X	X	X	X	X	X

<sup>1</sup>No adult, x-rated, nude, or semi-nude venues of any type, including but not limited to film, digital, hologram and similar technology, live performance, or exhibition.

USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
(C) Offices:							
Bank, savings and loan association, or other savings or lending institution:							
(i) With drive through	SP	SP	X	X	X	SP	X
(ii) All others	P	P	SP	SP	X	P	SP
Office accessory to an allowed use	P	P	P	P	P	P	P
Office (except as otherwise provided)	P	SP	SP	X	X	P	X
Office of a certified massage therapist	P	P	P	P	P	P	X
Office of a medical practitioner or medical clinic (which may include an accessory private spa)	P	SP	X	SP	SP	P	X
Real estate subdivision sales office:							
(i) As a temporary use, in accordance with Sections 27-260 and 27-261	P	P	P	P	P	P	P
(ii) As an accessory use in a dwelling	P	P	P	P	P	P	SP
(iii) All others	X	X	X	X	X	X	X
Accountants, architects, clergymen, engineers, lawyers, medical practitioners, and similar recognized and learned professions, as an accessory use in a dwelling	P	P	P	P	P	P	X
Business office and model apartments in a multifamily dwelling or multifamily project and used only in connection with the sale, rental, operation, service, and maintenance of the dwelling or project	P	P	P	P	X	P	X
Insurance sales office as an accessory use in a dwelling	P	P	P	P	P	P	X
Multifamily dwelling management company (must manage the project within which it is located)	P	P	P	P	X	P	X
Temporary trailer for office space accessory to an existing group residential facility, which services more than eight persons, in accordance with Sections 27-260 and 27-261	X	X	X	X	X	X	X
(D) Services:							
Acupuncturist	P	P	SP	SP	SP	P	X
Alternative health practitioners, licensed	P	P	SP	SP	SP	P	X
Ambulance service, private	X	P	SP	X	X	X	X
Animal hospital, veterinary office, animal training, kennel:							
(i) On properties of five acres or greater	X	X	X	X	X	X	P
(ii) All others	P	P	SP	X	X	P	X
Barber or beauty shop	P	P	SP	X	X	P	X
Bicycle repair shop:							
(i) Nonmotorized only	P	P	SP	X	X	P	P
(ii) All others	X	SP	X	X	X	X	X
Blacksmith shop	X	P	P	X	X	X	X
Blueprinting, photostating, or other photocopying establishment	P	P	SP	X	X	P	X
Carpet or rug shampooing establishment	X	P	SP	X	X	X	X
Catering establishment	P	P	SP	X	X	P	X
Computer system hardware assembly and maintenance shop	X	P	SP	X	X	X	X
Data processing	P	P	SP	X	X	P	X
Dry cleaning or laundry pickup station	P	P	SP	X	X	P	X

USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
Dry cleaning store or plant:							
(i) Retail, gross floor area under 3,000 square feet	P	P	SP	X	X	P	X
(ii) Retail, unrestricted	X	X	X	X	X	X	X
(iii) Wholesale (may include retail service)	X	X	X	X	X	X	X
Electrical or electronic equipment, appliances, radio or television, computer repair shop	P	P	SP	X	X	P	X
Employment agency	P	X	X	X	X	P	X
Fortune telling	P	X	X	X	X	X	X
Funeral parlor, undertaking establishment	X	SP	X	X	X	X	X
Household appliance or furniture repair shop	P	P	P	X	X	P	X
Key or locksmith shop	P	P	P	X	X	P	X
Laboratory	P	P	P	X	X	X	X
Laundromat:							
(i) Accessory to an allowed use	P	SP	X	P	X	P	X
(ii) All others	P	X	X	X	X	P	X
Laundry store or plant:							
(i) Retail, gross floor area under 3,000 square feet	P	P	SP	X	X	P	X
(ii) Retail, unrestricted	X	X	X	X	X	X	X
(iii) Wholesale (may include retail service)	X	X	X	X	X	X	X
Lawn mower repair shop:							
(i) Nonmotorized only	X	P	SP	X	X	P	X
(ii) All others, provided all repairs are performed within a wholly enclosed building	X	P	SP	X	X	X	X
Limousine service	X	X	X	X	X	X	X
Machine shop accessory to an allowed use	X	P	P	X	X	X	X
Massage establishment	SP	SP	SP	SP	SP	SP	X
Methadone treatment center	X	X	X	X	X	X	X
Model studio	P	P	P	X	X	X	X
Monument and headstone sales establishment	P	P	SP	X	X	X	X
Newspaper publishing establishment	X	P	SP	X	X	X	X
Pet grooming shop, provided all animals are confined to the interior of the building and adequate measures are taken to control noise and odor	P	X	X	X	X	P	X
Photographic processing plant	P	P	SP	X	X	X	X
Photography studio or darkroom:							
(i) As an accessory use solely by the resident of a one-family detached dwelling and located within such dwelling	P	P	P	P	P	P	P
(ii) All others	P	P	SP	X	X	P	X
Pizza delivery service, limited to off-premises delivery with no eat-in or drive-in service:							
(i) With carry-out service in a building with less than 2,500 square feet of gross floor area	P	P	SP	X	X	P	X
(ii) Unrestricted in size with no carryout service	X	X	X	X	X	X	X
Printing shop:							
(i) Not exceeding 2,000 square feet of gross floor area	P	P	P	X	X	P	X
(ii) All others	X	P	SP	X	X	X	X



USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
Research and Development	P	P	P	X	X	X	X
Sauna or steam bath	P	X	X	X	X	SP	X
Septic tank service	X	X	X	X	X	X	X
Sewage dump station for camping trailers or boats	X	X	X	X	X	X	X
Shoe repair shop	P	P	P	X	X	P	X
Tailor or dressmaking shop	P	P	P	X	X	P	X
Taxidermy	X	P	SP	X	X	X	X
Travel bureau	P	X	X	X	X	P	X
Upholstery shop	P	P	SP	X	X	SP	X
Watch or jewelry repair shop	P	P	P	X	X	P	X
Welding shop:							
(i) Accessory to an allowed use	X	P	P	X	X	X	X
(ii) All others	X	P	SP	X	X	X	X
(E) Trade (Generally Retail):							
Adult book store	X	X	X	X	X	X	X
Antique shop	P	P	SP	X	X	P	X
Arts, crafts, and hobby supply store	P	P	SP	X	X	P	X
Bait shop	X	X	X	X	X	X	P
Bakery, primarily retail sales with or without production on-site	P	P	SP	X	X	P	X
Bakery products, wholesale (may include retail sales)	X	P	SP	X	X	X	X
Bicycle (sales) shop:							
(i) Nonmotorized only	P	P	SP	X	X	P	P
(ii) All others	X	SP	X	X	X	X	X
Book (except adult bookstore) or camera store	P	P	SP	X	X	P	X
Bottled gas sales:							
(i) Accessory to an allowed use	X	P	SP	X	X	X	X
(ii) All others	X	X	X	X	X	X	X
Building supply store:							
(i) Wholly enclosed, except for nursery stock	X	P	SP	X	X	SP	X
(ii) With outdoor storage on not more than 50 percent of the lot, provided it is enclosed by a slightly opaque wall or fence at least eight feet high	X	X	X	X	X	X	X
Bulk retailing	X	X	X	X	X	X	X
Buying of items within guest rooms and vehicles, pursuant to Section 27-115(a)(2)	X	X	X	X	X	X	X
Carpet or floor covering store	P	P	SP	X	X	P	X
Clothing, dry goods, millinery, or shoe store	P	P	SP	X	X	P	X
Computer store	P	X	X	X	X	X	X
Confectioner (not exceeding 40,000 square feet of gross floor area):							
(i) Retail	P	X	X	X	X	P	X
(ii) Wholesale (may include accessory retail sales)	X	P	SP	X	X	X	X
Department or variety store, excluding pawnshops	P	X	X	X	X	X	X
Drug paraphernalia display or sales, pursuant to Section 27-115(a)(1)	X	X	X	X	X	X	X

USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
Drug store	P	X	X	X	X	P	X
Electrical supply store	P	P	SP	X	X	X	X
Farm implement, supplies, or feed sales; farm implement repair	X	X	X	X	X	X	X
Firewood sales as a temporary use in accordance with Sections 27-260 and 27-261	X	X	X	X	X	P	P
Farmers' market or flea market as a temporary use, in accordance with Sections 27-260 and 27-261	P	SP	SP	SP	SP	P	P
Florist shop	P	P	SP	X	X	P	X
Food or beverage goods preparation on the premises of a food or beverage store, provided the goods are only sold on the premises and at retail:							
(i) Not exceeding 3,000 square feet of gross floor area or occupying more than ten percent of the site, whichever is larger	P	P	SP	X	X	P	P
(ii) All others	P	P	SP	X	X	P	X
Food or beverage goods preparation for wholesale sales	X	P	SP	X	X	X	X
Food or beverage store:							
(i) Not exceeding 3,000 square feet of gross floor area	P	P	SP	X	X	P	P
(ii) Not exceeding 25,000 square feet of gross floor area	P	SP	X	X	X	P	X
(iii) All others	SP	X	X	X	X	SP	X
Garden supplies store, floricultural or horticultural nursery, which may include the outdoor display of nursery stock, such as plants, shrubbery, and trees	X	P	SP	X	X	X	P
Garden center, nursery, or greenhouse for primarily retail sales with under 15,000 square feet total gross sales area	P	P	SP	X	X	P	X
Gift, jewelry, music, souvenir, or other specialty store not specifically listed	P	P	SP	X	X	P	X
Hardware store (may include electrical or plumbing supplies)	P	P	SP	X	X	P	X
Home furnishing store (such as interior decorations, electronics, housewares, household accessories, or textiles)	P	SP	X	X	X	P	X
Household appliance or furniture store	P	SP	X	X	X	SP	X
Ice vending machine (not exceeding eight-ton capacity)	X	P	SP	X	X	X	X
Landscaping contractor's business							
(i) On properties of five acres or greater	X	X	X	X	X	X	P
(ii) All others	X	P	SP	X	X	X	X
Lawn mower (sales) store	X	SP	X	X	X	P	X
Newspaper, magazine, or tobacco shop	P	P	SP	X	X	P	X
Nursery and garden center, which may include the outdoor display of nursery stock, such as plants, shrubbery, and trees:							
(i) On properties of five acres or greater	X	SP	SP	X	X	X	P
(ii) All others	X	P	SP	X	X	P	X
Outdoor display of merchandise for sale (except as otherwise specified) and excluding merchandise displayed on gasoline pump islands associated with gas stations which is allowed):							
(i) Not more than six feet from main building (subject to Section 27-388)	P	P	SP	X	X	P	X

USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
(ii) More than six feet from main buildings (subject to Section 27-388)	X	X	X	X	X	X	X
Outdoor display of merchandise for sale or outdoor customer seating that does not obstruct the seven-foot wide path of sidewalk across the frontage of the property	P	P	SP	X	X	P	X
Paint or wall covering store	P	P	SP	X	X	P	X
Pawnshop	X	X	X	X	X	X	X
Pet (sales) shop, provided all animals are confined to the interior of the building and adequate measures are taken to control noise and odor; may include the sale of pet feed and supplies	P	X	X	X	X	P	X
Pet feed and supply store	P	SP	SP	X	X	P	X
Retail shop or store (not listed) similar to one permitted (P)	P	P	SP	X	X	P	X
Retail sales as accessory to production	P	P	P	X	X	P	X
Sales events sponsored by bona fide nonprofit groups or organizations, in accordance with Sections 27-260 and 27-261	P	P	P	SP	SP	P	P
Sales from guest rooms and vehicles, in accordance with Section 27-115(a)(2)	X	X	X	X	X	X	X
Seafood market:							
(i) Containing less than 3,000 square feet of gross retail space	P	P	SP	X	X	P	X
(ii) Containing less than 7,000 square feet of gross retail space	P	P	SP	X	X	P	X
(iii) Unrestricted in size	X	X	X	X	X	X	X
Seasonal decorations display and sales as a temporary use, in accordance with Sections 27-260 and 27-261	P	P	SP	X	X	P	P
Septic tank sales	X	X	X	X	X	X	X
Sporting goods shop for the sales and rental of nonmotorized athletic equipment, which may include marine equipment and supplies	P	P	X	X	X	P	P
(i) Under 8,000 square feet in area	P	P	SP	X	X	P	P
(ii) All others	P	SP	SP	X	X	X	X
Stationery or office supply store which may include the sale of furniture or business machines	P	P	SP	X	X	P	X
Swimming pool or spa sales and service:							
(i) Excluding outdoor display	X	P	SP	X	X	X	X
(ii) Including outdoor display, provided it is enclosed by a six-foot high fence (subject to Section 27-388)	X	X	X	X	X	X	X
Toy store	P	P	SP	X	X	P	X
Video electronics, games, tapes, or digital discs store	P	P	SP	X	X	P	X
Wayside stand:							
(i) As a temporary use, subject to Sections 27-260 and 27-261	P	P	SP	X	X	P	P
(ii) All others	X	X	X	X	X	X	X
<b>(3) INDUSTRIAL:</b>							
<b>(A) Chemical and Allied Manufacturing:</b>							
Abrasive and asbestos products	X	X	X	X	X	X	X
Acids and caustics	X	X	X	X	X	X	X
Carbon black	X	X	X	X	X	X	X
Celluloid or pyroxylin	X	X	X	X	X	X	X



USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
Cosmetics, medicinal, pharmaceutical, and toilet article products, fabrication	X	X	X	X	X	X	X
Distillery for the production of fuel alcohol, which may include bulk storage	X	X	X	X	X	X	X
Drugs, compounding	X	X	X	X	X	X	X
Dyestuffs	X	X	X	X	X	X	X
Fertilizers	X	X	X	X	X	X	X
Gum and wood chemicals, which may include distilling	X	X	X	X	X	X	X
Ink:							
(i) Paste inks	X	X	X	X	X	X	X
(ii) All other inks	X	X	X	X	X	X	X
Organic and inorganic chemicals (except as specified):							
(i) Blending and mixing	X	X	X	X	X	X	X
(ii) Breaking bulk	X	X	X	X	X	X	X
Paints, varnishes, lacquers, enamels, and shellacs	X	X	X	X	X	X	X
Pesticides and insecticides	X	X	X	X	X	X	X
Plastics and synthetic fibers	X	X	X	X	X	X	X
Soaps, cleaners, polishes, sanitation preparations, bleaches, and detergents	X	X	X	X	X	X	X
Soaps and toiletries, handcrafted	X	P	P	X	X	X	X
(B) Fabrication of Wood, Metal, Paper, Plastic, and Glass Products from Materials Produced Elsewhere:							
Armament control and sighting systems	X	X	X	X	X	X	X
Communications equipment, computers, telephones, electronic equipment, radios, sound systems, or televisions (which may include parts for these items)	X	P	P	X	X	X	X
Containers:							
(i) Paper or plastic	X	P	P	X	X	X	X
(ii) Glass, metal, or wood or other materials	X	P	P	X	X	X	X
Electrical lighting fixtures	X	P	P	X	X	X	X
Engines, generators, turbines, or miscellaneous machinery	X	X	X	X	X	X	X
Furniture, cabinets, fixtures, or interior decorating components	X	P	P	X	X	X	X
Hardware (except as otherwise specified)	X	P	SP	X	X	X	X
Heavy armament	X	X	X	X	X	X	X
Household appliances:							
(i) Small electrical household appliances, including televisions but excluding refrigerators and similar large appliances	X	P	SP	X	X	X	X
(ii) All others	X	X	X	X	X	X	X
Motorized vehicles, aircraft, construction, farm, or railroad equipment, ships or boats (which may include the parts for these items)	X	X	X	X	X	X	X
Nonmotorized vehicles, aircraft, construction, farm, or railroad equipment, ships or boats (which may include the parts for these items):							
(i) Handcrafted vehicles or water-going craft	X	P	P	X	X	X	X
(ii) All others	X	X	X	X	X	X	X

USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
Office supplies and equipment	X	P	SP	X	X	X	X
Plumbing, air conditioning, carpentry, electrical heating, and lighting supplies and equipment (except where otherwise specified):							
(i) Plastic	X	P	SP	X	X	X	X
(ii) Other materials	X	P	SP	X	X	X	X
Spas and swimming pools	X	X	X	X	X	X	X
Structural components of buildings (except as otherwise specified)	X	P	SP	X	X	X	X
Vending machines	X	P	SP	X	X	X	X
(C) Manufacturing and Processing of Lumber, Wood, and Related Products:							
Logging camps or contractors	X	X	X	X	X	X	X
Mobile or manufactured homes	X	X	X	X	X	X	X
Modular buildings	X	X	X	X	X	X	X
Sawmills or planing mills	X	X	X	X	X	X	X
Veneer and plywood	X	P	SP	X	X	X	X
Wood preserving	X	P	SP	X	X	X	X
(D) Manufacturing of Scientific, Specialized, and Technical Instruments and Equipment:							
Engineering, computer, drafting, laboratory, office, research, and scientific instruments, devices, equipment and supplies	X	P	P	X	X	X	X
Optical, dental, medical, ophthalmic, orthopedic, and prosthetic devices and supplies	X	P	P	X	X	X	X
Photographic equipment and supplies	X	P	P	X	X	X	X
Timing, light, mixing, safety, temperature, or weight control devices	X	P	P	X	X	X	X
Watches, clocks, measuring, or time-keeping devices	X	P	P	X	X	X	X
(E) Metal Production:							
Blast furnace	X	X	X	X	X	X	X
Foundry:							
(i) Nonferrous metals	X	SP	X	X	X	X	X
(ii) Nonornamental ferrous	X	SP	X	X	X	X	X
(iii) Ornamental metals	X	P	SP	X	X	X	X
Recycling of nonferrous metals	X	SP	X	X	X	X	X
Rolling mill	X	SP	X	X	X	X	X
Smelting	X	SP	X	X	X	X	X
(F) Miscellaneous Industrial, Manufacturing, and Related Uses:							
Brushes, brooms, combs, or lamp shades manufacturing	X	P	P	X	X	X	X
Cigarettes, cigars, or other tobacco products (chewing or smoking) manufacturing	X	X	X	X	X	X	X
Consolidated storage, in accordance with Section 27-475.04	X	SP	SP	X	X	X	X
Contractor's office (general) as a permanent use, including the businesses of siding, flooring, roofing, plumbing, air conditioning, heating, painting, carpentry, electrical work, landscaping, and the like, with buildings and uses accessory to the business (as well as the office) use:							

USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
(i) With storage of materials or equipment:							
(a) Indoors	X	P	SP	X	X	X	X
(b) Outdoors	X	X	X	X	X	X	X
(ii) Including the fabrication (only within a wholly enclosed building) of plumbing, air conditioning, heating, carpentry (and the like) parts for installation off-site	X	P	SP	X	X	X	X
(iii) Including the retail sale of parts and supplies as an accessory use	X	X	X	X	X	X	X
Contractors' plant or storage yard:							
(i) Those involving the operation or storage of heavy equipment over 17,000 pounds gross vehicle weight	X	X	X	X	X	X	X
(ii) All others	X	X	X	X	X	X	X
Explosives, fireworks, or gunpowder manufacturing	X	X	X	X	X	X	X
Gelatin manufacturing	X	X	X	X	X	X	X
Glue or size manufacturing	X	X	X	X	X	X	X
Heavy motorized equipment, motor vehicle, truck, boat, camping trailer, or trailer storage yard	X	X	X	X	X	X	X
Jewelry manufacturing	X	P	P	X	X	X	X
Junkyard	X	X	X	X	X	X	X
Machine shop	X	P	P	X	X	X	X
Matches manufacturing	X	X	X	X	X	X	X
Motion picture or broadcasting equipment manufacturing and production	X	P	P	X	X	X	X
Musical instruments, athletic goods, notions, novelties, sporting goods manufacturing	X	P	P	X	X	X	X
Pens, artist or drafting supplies, clerical equipment, business machines, or pencils manufacturing	X	P	P	X	X	X	X
Plastic products manufacturing, except as otherwise specified	X	X	X	X	X	X	X
Precious metalware manufacturing and plating	X	P	SP	X	X	X	X
Recycling plant, except as otherwise specified	X	SP	X	X	X	X	X
Storage building accessory to:							
(i) A permitted use	X	P	SP	X	X	X	X
(ii) A special permit use	X	SP	SP	X	X	X	X
Storage yard, except as otherwise specified	X	X	X	X	X	X	X
Tobacco packing, processing, and treatment	X	X	X	X	X	X	X
Transfer station	X	X	X	X	X	X	X
Trash removal services	X	X	X	X	X	X	X
Uses which are noxious or offensive by reason of odor, dust, fumes, smoke, gas, vibration, radiation, glare, or noise, or which constitute a public hazard by fire, explosion, or other means (in the discretion of the District Council)	X	SP	X	X	X	X	X
Vehicle salvage yard	X	X	X	X	X	X	X
Warehousing:							
(i) Subject to Section 27-471(g)	X	SP	X	X	X	X	X
(ii) All others	X	SP	X	X	X	X	X
Waste material separation and processing facility, in accordance with Section 27-475.05	X	X	X	X	X	X	X



USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
(G) Petroleum, Gas, and Related Products:							
Asphalt mixtures	X	X	X	X	X	X	X
Breaking bulk for home consumption or portable appliances	X	X	X	X	X	X	X
Distribution for home consumption	X	P	SP	X	X	X	X
Gasoline or gasohol bulk storage (other than gas station or vehicle repair and service station)	X	X	X	X	X	X	X
Gasohol fabrication	X	X	X	X	X	X	X
Liquid gas storage	X	X	X	X	X	X	X
Refinery	X	X	X	X	X	X	X
Tank farm, except as otherwise specified	X	X	X	X	X	X	X
(H) Printing, Publishing, Paper, and Related Industries:							
Bookbinder manufacturing	X	X	X	X	X	X	X
Bookbinding	X	P	P	X	X	X	X
Paper or paper board products (except containers) manufacturing	X	P	SP	X	X	X	X
Paper recycling collection center (only for collection, storage, and shipping)	X	X	X	X	X	X	X
Printing and engraving (which may include all processes)	X	P	SP	X	X	X	X
Publishing	X	P	SP	X	X	X	X
Pulp making	X	X	X	X	X	X	X
(I) Processing of Food and Kindred Products for Human Consumption:							
Bakery products manufacturing	X	P	SP	X	X	X	X
Beverage bottling (all containers):							
(i) Alcoholic	P	P	SP	X	X	X	X
(ii) Nonalcoholic	P	P	SP	X	X	X	X
Brewery, distillery, winery	P	P	SP	X	X	X	X
Cereals, grains, or spice processing	X	X	X	X	X	X	X
Coffee roasting							
(i) As auxiliary to coffee shop	P	P	SP	X	X	P	X
(ii) All others	X	P	SP	X	X	X	X
Confectionery products manufacturing	X	P	SP	X	X	X	X
Drying and dehydrating of food products	X	P	SP	X	X	X	X
Freezing of food products	X	X	X	X	X	X	X
Oil and fat manufacturing (edible)	X	X	X	X	X	X	X
Packaging, packing, and canning of food products	X	P	SP	X	X	X	X
Pickling	X	P	SP	X	X	X	X
Reduction plant	X	X	X	X	X	X	X
Sauce, seasoning, and dressing manufacturing	X	P	SP	X	X	X	X
Slaughterhouse	X	X	X	X	X	X	X
Smoking and curing	X	X	X	X	X	X	X
Sugar refinery	X	X	X	X	X	X	X
Syrup and flavor extract manufacturing	X	X	X	X	X	X	X
(J) Rubber and Synthetic Rubber Products:							
Fabrication of rubber products	X	X	X	X	X	X	X
Recycling of rubber products	X	X	X	X	X	X	X

USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
Rubber manufacturing	X	X	X	X	X	X	X
Rubberizing of wearing apparel or household items	X	X	X	X	X	X	X
Synthetic rubber (and similar products) manufacturing	X	X	X	X	X	X	X
Tire or tube manufacturing	X	X	X	X	X	X	X
Tire recapping	X	X	X	X	X	X	X
(K) Stone, Clay, Glass, and Cement Products:							
Cement manufacturing	X	X	X	X	X	X	X
Concrete batching or cement mixing plant	X	X	X	X	X	X	X
Cutting of nonstructural stone	X	P	P	X	X	X	X
Glass blowing and molding	X	P	P	X	X	X	X
Manufacturing of ornamental products made of clay, concrete, glass plaster, porcelain, or stone	X	P	P	X	X	X	X
Manufacturing or cutting of structural products made of clay, concrete, glass, stone, or similar materials	X	P	SP	X	X	X	X
(L) Textile Products:							
Fabrication of finished goods	X	P	P	X	X	X	X
Felt manufacturing	X	P	P	X	X	X	X
Fiber or thread manufacturing	X	P	P	X	X	X	X
Finishing of textile goods	X	P	P	X	X	X	X
Knitting	X	P	P	X	X	X	X
Lace (and similar products) manufacturing	X	P	P	X	X	X	X
Recycling of textiles	X	P	P	X	X	X	X
Synthetics manufacturing	X	SP	X	X	X	X	X
Weaving	X	P	P	X	X	X	X
(M) Wholesale Trade:							
Apparel, garments, or related products	X	P	SP	X	X	X	X
Automotive or motorized equipment (parts and accessories only)	X	X	X	X	X	X	X
Bakery products, wholesale	X	P	SP	X	X	X	X
Bulk raw materials	X	X	X	X	X	X	X
Chemical and related products (except as otherwise specified)	X	X	X	X	X	X	X
Electronic, glass, metal, paper, rubber, or wood products	X	P	SP	X	X	X	X
Food or kindred products	X	P	SP	X	X	X	X
Garden supplies or nursery stock							
(i) On properties of five acres or greater	X	X	X	X	X	X	P
(ii) All others	X	P	SP	X	X	X	X
Livestock	X	X	X	X	X	X	X
Ornamental products made of china, clay, concrete, glass, or stone	X	P	P	X	X	X	X
Petroleum products for home distribution	X	X	X	X	X	X	X
Structural products made of clay, concrete, or stone, with:							
(i) Indoor storage, only	X	P	P	X	X	X	X
(ii) Outdoor storage	X	X	X	X	X	X	X
Textile products	X	P	P	X	X	X	X
Tobacco and related products	X	X	X	X	X	X	X

USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
Wholesaling or distribution of materials (products) not used or produced on the premise:							
(i) Subject to Section 27-471(g)	X	X	X	X	X	X	X
(ii) All others	X	X	X	X	X	X	X
Wholesaling or distribution of materials (products) used or produced on the premises	X	P	P	X	X	X	X
Wholesaling or distribution use not listed	X	SP	X	X	X	X	X
<b>(4) INSTITUTIONAL/EDUCATIONAL</b>							
Adaptive reuse of a building previously occupied by an institutional or public use under 25,000 square feet in gross floor area for an institutional or arts-related use, where not otherwise allowed	SP	SP	SP	SP	SP	SP	SP
Adult day care center	P	X	X	SP	X	X	X
Adult rehabilitation center	P	X	X	SP	X	X	X
Assisted living facility, subject to the requirements of Section 27-464.04	P	X	X	SP	X	X	X
Chancery, on a lot having a net area of at least 15 acres	X	X	X	X	X	X	X
Church or similar place of worship, convent, or monastery	X	X	X	X	X	SP	SP
Community center or similar nonprofit social use; not publicly owned or operated	SP	SP	SP	SP	SP	SP	X
Day care center for children							
(A) On properties of five acres or greater	X	X	X	X	X	X	P
(B) All others	SP	SP	SP	SP	SP	P	X
Eleemosynary or philanthropic institution	SP	X	X	X	X	SP	SP
Family day care							
(A) On properties of five acres or greater	X	X	X	X	X	X	P
(B) All others	SP	SP	SP	SP	SP	P	X
Health campus	SP	X	X	X	X	SP	X
Hospital (may include a private spa)	P	X	X	X	X	SP	X
Medical/residential campus	SP	X	X	X	X	SP	X
Modular classroom as a temporary use, in accordance with Sections 27-260 and 27-261	P	P	P	P	P	P	P
Nursing or care home (may include a private spa)	P	X	X	SP	X	X	X
School, Private:							
(A) Driving school, automobile only	P	X	X	X	X	P	X
(B) For artistic or technical instruction and practice (may include a studio)							
(i) On properties of five acres or less	X	X	X	X	X	P	P
(ii) All others	P	P	SP	X	X	P	X
(C) Tutoring establishment							
(i) On properties of five acres or greater	X	X	X	X	X	X	P
(ii) All others	P	P	P	X	X	P	X
(D) Private college or university	P	P	SP	X	X	P	X
(E) Business	P	X	X	X	X	P	X
(F) Institution of higher learning (may include a private spa)	P	P	SP	X	X	P	X
(G) Scientific	P	P	SP	X	X	P	X



USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
(H) Trade	P	P	SP	X	X	P	X
(I) In accordance with Section 27-475.06.01, Section 27-463, or Section 27-443	P	P	SP	X	X	P	X
(J) All others	P	P	SP	X	X	P	
Small group child care center							
(A) On properties of five acres or greater	X	X	X	X	X	X	P
(B) All others	P	X	X	SP	SP	P	X
<b>(5) MISCELLANEOUS</b>							
Accessory structures and uses, except as otherwise provided	X	X	X	P	P	X	P
Adaptive reuse of a historic site or surplus public school, when not otherwise allowed	SP	SP	SP	SP	SP	SP	SP
Animals, not customarily household pets	X	X	X	X	X	X	X
Auction house	P	P	SP	X	X	P	X
Buildings and uses, serving public health purposes, on land owned by Prince George's County, Maryland, upon which hospitals or health centers are located, except if otherwise allowed as a permitted (P) use	SP	SP	SP	SP	SP	SP	SP
Carpentry, cabinet making, or other woodworking shop							
(A) A maximum of 15,000 square feet in gross floor area	X	P	P	X	X	X	X
(B) All others	X	P	SP	X	X	X	X
Cemetery or crematory	X	X	X	X	X	X	X
Collection of recyclable materials:							
(A) As a temporary use, in accordance with Sections 27-260 and 27-261	P	P	SP	X	X	P	X
(B) All others	X	SP	X	X	X	X	X
Commercial recreational development	X	X	X	X	X	X	X
Consolidated storage	X	SP	SP	X	X	X	X
Contractor's office (general) as a permanent use, including the businesses of siding, flooring, roofing, plumbing, air conditioning, heating, painting, carpentry, electrical work, landscaping and the like, with buildings and uses accessory to the business (as well as the office) use:							
(A) With no outdoor storage of materials or equipment	X	P	SP	X	X	X	X
(B) All others	X	X	X	X	X	X	X
Contractor's office (must include sanitary facilities), construction yard or shed, or storage building (in connection with a construction project) as a temporary use	P	P	P	P	P	P	P
Hardware fabrication and manufacturing of products from material produced elsewhere	X	P	P	X	X	X	X
Home occupations for residents, when not otherwise allowed	P	P	P	P	P	P	P
Ice vending machine for block ice	X	SP	X	X	X	SP	X
Increase in height of accessory building (not to exceed the height of the main structure), used for:							
(A) Servant, household help living quarters	X	P	P	X	P	X	X
(B) Agricultural purposes on a lot having a net area of less than five acres	X	X	X	X	X	X	X

USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
(C) Agricultural purposes on a lot having a net area of at least five acres	X	X	X	X	X	X	X
(D) Office	X	P	P	X	X	X	X
(E) Artist Studio	P	P	P	P	P	P	X
Mobile home, with use for which amusement taxes collected, as a temporary use	P	P	P	X	X	P	P
Recycling collection center as a temporary use, in accordance with Sections 27-260 and 27-261	P	P	SP	X	X	P	X
Recycling collection center, paper only (limited to collection, storage, and shipping):	X	X	X	X	X	X	X
Rental business for items permitted to be sold (except as otherwise specified)	SP	P	SP	X	X	SP	X
Sign, in accordance with Part 12:							
(A) Outdoor advertising (billboard)	X	X	X	X	X	X	X
(B) All others	P	P	P	P	P	P	P
Sign shop	P	P	P	X	X	P	X
Storage, wholly enclosed, accessory to an allowed use	P	P	P	P	P	P	P
Temporary shelter for commercial display, sale, or service use permitted (P) in the respective zones, as a temporary use, in accordance with Sections 27-260 and 27-261	P	P	P	X	X	P	P
Temporary structures and uses not otherwise allowed	SP	X	X	SP	X	SP	SP
Trash removal services	X	X	X	X	X	X	X
Wholesaling, distribution, and related storage	X	P	SP	X	X	X	X
Wholesaling of products incidental to the retail sales of the products on the premises	X	P	SP	X	X	X	X
<b>(6) PUBLIC:</b>							
Community building, except as otherwise provided	P	SP	X	P	SP	P	P
Library	P	SP	SP	P	X	P	SP
Post Office	P	SP	X	X	X	P	X
Public building and use, except as otherwise prohibited	SP	SP	SP	SP	SP	SP	SP
Sanitary landfill or rubble fill	X	X	X	X	X	X	X
Voluntary fire, ambulance, or rescue squad services	P	P	SP	X	X	P	X
<b>(7) RECREATIONAL/ENTERTAINMENT/SOCIAL/CULTURAL:</b>							
Amusement arcade:							
(A) Not exceeding 2,500 square feet of gross floor area, with adult supervision on the premises during all hours of operation; provided the use is located either within a wholly enclosed shopping mall, or within the main group of stores of an integrated shopping center having a minimum gross floor area of 150,000 square feet	P	SP	X	X	X	SP	X
(B) All others	SP	X	X	X	X	X	X
Amusement center	SP	X	X	X	X	X	X
Amusement park, as a temporary use	X	X	X	X	X	X	SP
Archery range, privately owned and commercially operated on land leased from, and owned by, a public agency	X	X	X	X	X	X	P
Arena or stadium (which may include a private spa):							
(A) On properties of five acres or greater	SP	X	X	X	X	X	P

USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
(B) All others	SP	X	X	X	X	X	X
Athletic field	X	X	X	SP	X	X	P
Auditorium:							
(A) On not less than five acres with no greater than 10 percent impermeable surface	P	P	SP	SP	X	X	P
(B) All others	P	P	SP	SP	X	X	X
Beach	X	SP	X	X	X	X	P
Billiard or pool parlor	P	P	SP	X	X	X	X
Boathouse (private) as an accessory use	X	X	X	X	X	X	SP
Boat ramp	X	X	X	X	X	X	P
Bowling alley	SP	P	SP	X	X	X	X
Carnival, circus, fair, or similar use, not exceeding 17 days duration and located at least 250 feet from any dwelling, as a temporary use in accordance with Sections 27-260 and 27-261:							
(A) On a parking lot	P	P	SP	X	X	X	P
(B) All others	P	P	SP	X	X	X	P
Club or lodge (private) except as otherwise provided	SP	X	X	X	X	SP	X
Commercial recreational attractions	SP	SP	SP	X	X	X	SP
Commercial recreational facilities (privately owned) on land leased from a public agency, except as otherwise allowed	SP	SP	SP	X	X	X	P
Community building or similar nonprofit social use, not publicly owned or operated							
(A) On not less than five acres with no greater than ten percent impermeable surface	X	X	X	X	X	X	SP
(B) All others	SP	X	X	X	X	SP	X
(C) Community multipurpose facility which may include the following uses: reducing/exercise salon or health club, swimming pool, indoor, day care center for children, on properties less than five acres, park or playground (not public, associated with the day care), recreational program before and after-school, summer camp, community center or similar nonprofit social use (not publicly owned or operated)	P	X	X	X	X	X	X
Courts (indoor or outdoor) (tennis, handball, racquetball, or volleyball), not including courts accessory to a dwelling	SP	SP	X	SP	X	X	SP
Dog park	P	X	X	P	SP	X	P
Employees' recreational facilities (private, nonprofit) accessory to an allowed use	SP	SP	X	X	X	X	SP
Fishing pier	X	X	X	X	X	X	P
Go-cart track	X	X	X	X	X	X	X
Golf course or country club	X	X	X	X	X	X	SP
Golf course, miniature (indoor or outdoor)	P	SP	X	X	X	SP	P
Golf driving, archery, or baseball batting range	X	X	X	X	X	X	SP
Homes Association Recreational Use, in accordance with Section 27-445	SP	X	X	SP	SP	X	X
Marina	X	X	X	X	X	X	SP



USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
Museum, aquarium, art gallery, cultural center, or similar facility	P	P	SP	X	X	P	P
Park or playground	P	P	SP	P	P	P	P
Performance arts center, in accordance with Sections 27-445.09, 27-464.05, or 27-475.06.04	P	P	SP	X	X	X	P
Racetrack	X	X	X	X	X	X	X
Recreational campground (in the C-M Zone subject to paragraphs (1) through (7) of Section 27-400(a))	X	X	X	X	X	X	SP
Recreational or entertainment establishment of a commercial nature, if not otherwise specified	SP	SP	X	X	X	X	SP
Recreational program, before- and after-school	P	SP	X	SP	X	P	P
Recreational use (nonprofit) not publicly owned or operated, when not otherwise allowed	SP	X	X	X	X	X	SP
Reducing/exercise salon or health club	P	X	X	X	X	SP	SP
Riding stable	X	X	X	X	X	X	SP
Rifle, pistol, or skeet shooting range	X	X	X	X	X	X	X
Sauna or steam bath	P	SP	X	SP	X	X	X
Saunas, solariums, and health clubs, noncommercial, for the sole use of residents and their guests	P	SP	X	P	X	X	X
Skating facility	X	X	X	X	X	X	SP
Skate park	SP	SP	SP	SP	SP	SP	P
Spa (private)	P	X	X	X	X	X	X
Spa (public)	P	X	X	X	X	X	X
Stable, private	X	X	X	X	X	X	P
Summer camp	P	SP	X	X	SP	X	P
Swimming pool							
(A) Indoor	P	X	X	P	P	P	SP
(B) All others	P	X	X	P	P	P	P
Tennis, basketball, handball, or similar court	SP	SP	SP	SP	SP	SP	SP
Theater:							
(A) Indoor	P	P	SP	X	X	X	X
(B) Outdoor (including drive-in)	SP	SP	X	X	X	X	SP
Zoo, not publicly owned	X	X	X	X	X	X	X
<b>(8) RESIDENTIAL/LODGING:</b>							
Apartment hotel	SP	X	SP	X	X	X	X
Apartment housing for elderly or handicapped families in a building other than a surplus public school building (with provisions for increased density and reduced lot size in multifamily zones)	P	X	X	P	X	P	X
Apartment housing for elderly or handicapped families in a surplus public school building	P	X	X	P	X	P	X
Apartment housing for the elderly or physically handicapped	P	X	X	P	X	P	X
Artists' residential studios	P	P	P	P	P	P	X
Boarding house	P	X	X	X	X	X	X
Congregate living facility for more than eight elderly or physically handicapped residents	P	SP	SP	P	SP	SP	X

USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
Congregate living facility for not more than eight elderly or physically handicapped residents	SP	X	X	SP	P	P	X
Convent or monastery	X	X	X	P	X	X	X
Conversion of one-family detached dwelling to a building containing up to three dwelling units (not considered as a two-family, three-family, or multifamily dwelling)	SP	SP	X	P	SP	SP	X
Country inn	P	X	X	X	SP	P	X
Dwelling, provided that it was legally erected prior to the date upon which the property was classified in a Commercial Zone, or was legally erected in a Commercial Zone under prior regulations	P	P	P	P	P	P	P
Dwelling	P	X	X	X	P	P	X
Dwelling unit above the first floor within a building containing commercial, industrial, or arts-related uses	P	P	SP	X	X	P	X
Dwelling unit on the first floor:							
(A) Facing streets wider than two lanes	SP	X	X	P	P	X	X
(B) Facing streets a total of two lanes of traffic or less	P	SP	X	P	P	X	X
Dwelling unit on the first floor as an adaptation of a building from an industrial or vehicle-related use and not an arts-related use into loft units	P	P	SP	X	X	X	X
Dwelling, multifamily	P	SP	P	P	X	P	X
Dwelling, one-family attached, for the elderly	P	X	X	X	X	X	X
Dwelling, one-family detached, cluster development	P	X	X	X	P	X	X
Dwelling, one-family detached (in general)	P	X	X	X	P	X	X
Dwelling, one-family semidetached:							
(A) If allowed in the underlying zone	P	X	X	X	P	X	X
(B) All other	P	X	X	X	X	X	X
Dwelling, two-family detached:	P	X	X	X	X	X	X
(A) If allowed in the underlying zone	P	X	X	X	P	X	X
(B) All other	P	X	X	X	P	X	X
Dwelling, three-family:	P	SP	X	P	X	X	X
Dwellings, one-family triple-attached (in general)							
(A) If allowed in the underlying zone	P	X	X	X	P	X	X
(B) All other	P	X	X	X	X	X	X
Dwelling, quadruple-attached:							
(A) If allowed in the underlying zone	P	X	X	X	P	X	X
(B) All other	P	X	X	X	X	X	X
Flag lot development	SP	SP	X	SP	SP	X	X
Fraternity or sorority house	X	X	X	X	X	X	X
Granny flat, carriage house, or in-law suite, provided the owner continues to occupy the main structure <sup>2</sup>	P	X	X	X	P	X	X
Group residential facility for mentally handicapped dependent persons in accordance with Section 27-107.01(109)(D)	P	SP	X	P	X	X	X
Guest house, as an accessory use	P	SP	X	P	X	X	X
Hotel or motel	P	P	XSP	X	X	X	X
Mobile home used as a dwelling for emergency purposes as a temporary use, in accordance with Sections 27-260 and 27-261	X	X	X	X	X	X	X
Mobile home used as a one-family detached dwelling	X	X	X	X	X	X	X

<sup>2</sup> The city of Hyattsville is exempt from this provision.

USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
Mobile home, with use for which amusement taxes collected	P	P	P	P	P	P	P
Planned retirement community	SP	X	X	SP	X	X	X
Recreational community development, in accordance with Section 27-444	X	X	X	X	X	X	X
Rental of guest rooms (by the residents):	X	X	X	X	X	X	X
Residential Revitalization: Comprising any form of proposed multifamily, attached one-family or detached one-family dwellings, in a residential revitalization project, as shown on a detailed site plan approved in accordance with Section 27 445.09	SP	X	X	SP	X	X	X
Rooming houses	X	X	X	X	X	X	X
Tourist cabin camp	X	X	X	X	X	X	X
Tourist home	X	X	X	X	X	X	X
Townhouse, in general							
(A) If allowed in the underlying zone	P	P	P	P	P	X	X
(B) All other	P	X	X	X	X	X	X
Loft-style housing above the first floor	P	P	SP	P	X	X	X
Transitional shelter for the homeless	SP	X	X	X	X	X	X
<b>(9) RESOURCE PRODUCTION/RECOVERY:</b>							
Agricultural preservation development, in accordance with Section 27-445.01	X	X	X	X	X	X	P
Agricultural uses	X	X	X	X	X	X	SP
Sand and gravel wet-processing	X	X	X	X	X	X	X
Sawmill	X	X	X	X	X	X	X
Surface mining	X	X	X	X	X	X	X
<b>(10) TRANSPORTATION/PARKING/COMMUNICATIONS/ UTILITIES:</b>							
Airport, airpark, airfield, airstrip, heliport, helistop	SP	X	X	X	X	X	X
Antennas and related equipment buildings and enclosures, other than satellite dish antennas:	X	SP	X	X	X	X	X
(A) In or associated with a public building	P	P	P	P	P	P	P
(B) All others	X	SP	X	X	X	X	X
Broadcasting studio (without tower)	P	P	SP	X	X	P	X
Bus station or terminal	P	X	X	X	X	X	X
Farm vehicles and farm machinery used on farm premises	X	X	X	X	X	X	SP
Monopoles and related equipment buildings and enclosures	X	SP	X	X	X	X	X
Motor freight receiving or shipping (loading) facilities	X	X	X	X	X	X	X
Parking lot or garage, or loading area as an accessory to a permitted use	P	P	P	P	P	P	SP
Parking lot used in accordance with Part 11 to serve a use in an adjacent Commercial, Industrial, or M-X-T Zone	SP	SP	SP	X	X	SP	SP
Parking lot, required, serving adjacent Commercial or Industrial Zone	SP	SP	SP	X	X	X	X
Parking of mobile home, except as otherwise specified	X	X	X	X	X	X	X
Parking of a mobile home in a public right-of-way	X	X	X	X	X	X	X
Parking of vehicles accessory to an allowed use	P	P	P	P	P	P	P



USE	CHARACTER AREA						
	TC	APE	NAP	MRC	TRN	NC	SVP
Parking of vehicles owned or used by the occupants of the premises or their bona fide guests:							
(A) Boats and boat trailers	X	P	P	X	P	P	X
(B) Buses, on the same lot with, and accessory to, the principal use, such as a school or church	P	P	P	X	X	P	P
(C) Camping trailer (unoccupied)							
(i) Not more than one	X	P	SP	X	P	P	X
(ii) Unlimited number	X	X	X	X	X	X	X
(D) Not more than one commercial vehicle:							
(i) Commercial vehicles not exceeding a manufacturer's gross vehicle weight specification of 8,500 pounds; containing no advertising other than a firm name or similar designation not more than 4 inches high; and excluding vehicles exceeding 300 cubic feet of load space, stake platform trucks, dump trucks, crane or tow trucks, or vehicles with dual rear wheels	P	P	P	X	X	P	P
(ii) All others	X	P	SP	X	X	P	X
(F) Private passenger vehicles	P	P	P	P	P	P	P
Public utility uses or structures							
(A) Underground pipelines and electric power and telephone lines; structures commonly known as telephone poles; and railroad tracks (but not a yard) and passenger stations	P	P	P	P	P	P	P
(B) Antennas, monopoles, and related equipment buildings for mobile telephone transmission or other telecommunications, excluding satellite dish antenna:							
(i) In accordance with Section 27-445.04	SP	SP	SP	SP	SP	SP	X
(ii) All others	SP	SP	SP	X	X	SP	SP
Satellite dish antenna, in accordance with Section 27-424.02, 27-451.01, or 27-468.01:							
(A) Up to ten feet in diameter, to serve only one dwelling unit	P	P	P	P	P	P	X
(B) More than ten feet in diameter, to serve only one dwelling unit	X	SP	X	X	X	X	X
(C) All others	X	SP	SP	X	X	X	X
Storage of any motor vehicle which is wrecked, dismantled or not currently licensed, except where specifically allowed	X	X	X	X	X	X	X
Taxicab dispatching station:							
(A) Without cab storage, repair, or servicing	P	P	SP	X	X	P	X
(B) With cab storage	X	X	X	X	X	X	X
(C) With cab repair or servicing within a wholly enclosed building	X	X	X	X	X	X	X
Taxicab stand	P	P	SP	X	X	P	X
Telegraph or messenger service	P	P	SP	X	X	P	X
Towers or poles (electronic, public utility when not otherwise permitted, radio, or television, transmitting or receiving)	SP	SP	SP	X	X	SP	SP
Trucking operations, except as otherwise specified	X	X	X	X	X	X	X

## The Secretary of the Interior’s Standards for Preservation, Restoration, and Rehabilitation

The Mount Rainier M-U-TC Design Review Committee should adhere to *The Secretary of the Interior’s Standards for Rehabilitation, Preservation, and Restoration* when considering the appropriateness of proposed changes to historic buildings in historic districts. The Secretary of the Interior’s Standards for the Treatment of Historic Properties are common sense principles in non-technical language. They were developed to help protect our nation’s irreplaceable cultural resources by promoting consistent preservation practices.

The standards may be applied to all properties listed in the National Register of Historic Places: buildings, sites, structures, objects, and districts.

The standards are a series of concepts about maintaining, repairing and replacing historic materials, as well as designing new additions or making alterations. They cannot, in and of themselves, be used to make decisions about which features of a historic property should be preserved and which might be changed. But once an appropriate treatment is selected, the standards provide philosophical consistency to the work.

Rehabilitation acknowledges the need to alter or add to a historic property to meet continuing or changing uses while retaining the property’s historic character.

Preservation focuses on the maintenance and repair of existing historic materials and retention of a property’s form as it has evolved over time. (Protection and stabilization have now been consolidated under this treatment.)

Restoration depicts a property at a particular period of time in its history, while removing evidence of other periods.

### Standards for Rehabilitation

Rehabilitation is defined as the act or process of making possible a compatible use for a property through repair, alterations, and additions while preserving those

portions or features which convey its historical, cultural, or architectural values.

1. A property will be used as it was historically or be given a new use that requires minimal change to its distinctive materials, features, spaces, and spatial relationships.
2. The historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces, and spatial relationships that characterize a property will be avoided.
3. Each property will be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other historic properties, will not be undertaken.
4. Changes to a property that have acquired historic significance in their own right will be retained and preserved.
5. Distinctive materials, features, finishes, and construction techniques or examples of craftsmanship that characterize a property will be preserved.
6. Deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in design, color, texture, and, where possible, materials. Replacement of missing features will be substantiated by documentary and physical evidence.
7. Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.
8. Archeological resources will be protected and preserved in place. If such resources must be disturbed, mitigation measures will be undertaken.
9. New additions, exterior alterations, or related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work will be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and its environment.
10. New additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form

and integrity of the historic property and its environment would be unimpaired.

### **Standards for Preservation**

Preservation is defined as the act or process of applying measures necessary to sustain the existing form, integrity, and materials of an historic property. Work, including preliminary measures to protect and stabilize the property, generally focuses upon the ongoing maintenance and repair of historic materials and features rather than extensive replacement and new construction. New exterior additions are not within the scope of this treatment; however, the limited and sensitive upgrading of mechanical, electrical, and plumbing systems and other code-required work to make properties functional is appropriate within a preservation project.

1. A property will be used as it was historically, or be given a new use that maximizes the retention of distinctive materials, features, spaces, and spatial relationships. Where a treatment and use have not been identified, a property will be protected and, if necessary, stabilized until additional work may be undertaken.
2. The historic character of a property will be retained and preserved. The replacement of intact or repairable historic materials or alteration of features, spaces, and spatial relationships that characterize a property will be avoided.
3. Each property will be recognized as a physical record of its time, place, and use. Work needed to stabilize, consolidate, and conserve existing historic materials and features will be physically and visually compatible, identifiable upon close inspection, and properly documented for future research.
4. Changes to a property that have acquired historic significance in their own right will be retained and preserved.
5. Distinctive materials, features, finishes, and construction techniques or examples of craftsmanship that characterize a property will be preserved.
6. The existing condition of historic features will be evaluated to determine the appropriate level of intervention needed. Where the severity of deterioration requires repair or limited replacement of a distinctive feature, the new material will match the old in composition, design, color, and texture.
7. Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.
8. Archeological resources will be protected and preserved in place. If such resources must be disturbed, mitigation measures will be undertaken.

### **Standards for Restoration**

Restoration is defined as the act or process of accurately depicting the form, features, and character of a property as it appeared at a particular period of time by means of the removal of features from other periods in its history and reconstruction of missing features from the restoration period. The limited and sensitive upgrading of mechanical, electrical, and plumbing systems and other code-required work to make properties functional is appropriate within a restoration project.

1. A property will be used as it was historically or be given a new use which reflects the property's restoration period.
2. Materials and features from the restoration period will be retained and preserved. The removal of materials or alteration of features, spaces, and spatial relationships that characterize the period will not be undertaken.
3. Each property will be recognized as a physical record of its time, place, and use. Work needed to stabilize, consolidate, and conserve materials and features from the restoration period will be physically and visually compatible, identifiable upon close inspection, and properly documented for future research.
4. Materials, features, spaces, and finishes that characterize other historical periods will be documented prior to their alteration or removal.
5. Distinctive materials, features, finishes, and construction techniques or examples of craftsmanship that characterize the restoration period will be preserved.
6. Deteriorated features from the restoration period will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in design, color, texture, and, where possible, materials.
7. Replacement of missing features from the restoration period will be substantiated by documentary and



physical evidence. A false sense of history will not be created by adding conjectural features, features from other properties, or by combining features that never existed together historically.

8. Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.
9. Archeological resources affected by a project will be protected and preserved in place. If such resources must be disturbed, mitigation measures will be undertaken.
10. Designs that were never executed historically will not be constructed.

## Historic Preservation Definitions

**Alter or Alteration:** A change in the exterior appearance of a building or structure or its site, other than demolition, for which a permit is required; these terms may also apply to changes in any interior space that has been specifically designated as historic.

**Character-Defining Feature:** The form, materials, and details of those architectural features identified as important to a building's historic character and significance and whose retention will preserve that character and significance.

1. The character of a historic building may be defined by exterior features such as façades, roofs, porches, and windows, and exterior materials such as masonry, wood, glass, and metal.
2. The character of a historic building may be defined by its structural features such as bearing walls, floor and roof framing, or structural materials such as brick, steel, and wood.
3. The character of a historic interior may be defined by features such as room configurations, spatial relationships, stairs, trim, and decoration, and by materials such as partitions, woodwork, plaster, and finishes.
4. The site and setting of a historic property may be defined by features such as views to and from the property, landscaping, walls, fencing, and materials such as brick, stone and vegetation.

**Compatible:** Possessing characteristics that allow for a harmonious relationship. Compatibility does not require the matching or replication of attributes, and may involve a relationship of dissimilar things that are juxtaposed to produce an agreeable effect.

**Contributing Building, Structure, or Site:** A building, structure, or site located within a historic district that reflects the district's historic, architectural and/or cultural significance, and its sense of time, place, and historical development.

The following considerations apply when:

1. Determining whether a property meets the definition of a contributing building, structure, or site; and
2. Determining whether a property or site can be subject to compatible alterations and additions or be replaced in whole or in part through compatible new development:
  - a. A building, structure or site has been identified as contributing to a district by virtue of its integrity of location, design, setting, materials, workmanship, feeling, and association.
  - b. An archeological site adds to the archeological values for which a property is significant because it was present during the period of significance and is capable of yielding information about the period.

**Demolish or Demolition:** The razing or destruction of an entire principal or secondary façade—or a significant part of it—or the removal of more than 50 percent of the interior structural elements, principal façades, roofs, major character defining features, etc., of a building or structure. For the purposes of these standards and guidelines, the relocation of a structure in whole or in part shall be considered demolition.

**Demolition by Neglect:** Failure to maintain, repair, or secure a historic building or structure in a historic district that results in avoidable deterioration of an exterior feature of the building or structure, or loss of the structural integrity of the building or structure.

**Design:** Either of the following, depending upon context:

1. The sum of elements that create the physical form, plan, space, structure, and architectural style of a property.

2. Exterior architectural features including height, appearance, texture, color, and nature of materials.

**Elevation:** The exterior wall of a building or structure (also see “secondary elevation”).

**Façade:** The primary building elevation that possesses significant architectural composition or features such as the entrance (also see “principal façade”).

**Historic Building or Property:** A general reference to a structure or piece of land either listed in the county’s Inventory of Historic Resources as a historic site, historic resource, or as a contributing building within a designated historic district.

**Historic District:** A defined area included in the Prince George’s County Inventory of Historic Resources as a locally designated historic district, or a defined area listed in the National Register of Historic Places as a historic district, or both; a historic district listed only in the National Register of Historic Places is not subject to the regulations and benefits associated with a locally designated historic district.

**Historic Preservation Commission (HPC):** The Prince George’s County Historic Preservation Commission was established pursuant to the Prince George’s County Historic Preservation Ordinance (Subtitle 29 of the Prince George’s County Code). The HPC is the body charged with implementing the Historic Preservation Ordinance through adopted policies, procedures and guidelines.

**Historic Preservation Organization(s):** Membership organizations that include among their purposes the promotion of historic preservation in Prince George’s County, as expressed in the organization’s articles of incorporation, articles of association, charter, or bylaws.

**Historic Preservation Section or HPS:** The professional staff of the Prince George’s County Planning Department, M-NCPPC, that provides technical support to the Prince George’s County Planning Board and the Prince George’s County Historic Preservation Commission.

**Historic Site:** A designated building, site, structure, object, or feature, and its associated property, listed

in the Prince George’s County Inventory of Historic Resources.

**Incompatible:** Existing or new development, building, or site characterized by clashing or conflicting qualities that lead to an incongruous, discordant, or disagreeable relationship. Diversity, variety, or a contrast of qualities does not necessarily imply incompatibility as long as harmonious relationships are maintained.

**Integrity:** Authenticity of a property’s historic, architectural, and cultural identity, evidenced by the survival of physical characteristics that existed during the period of significance associated with a building, site, structure, object, or feature.

**Materials:** As a quality of integrity, the physical elements that were combined or deposited in a particular pattern or configuration to form a historic property.

**National Historic Landmark (NHL):** A building, site, structure, object, or district that possesses exceptional value and quality in illustrating or interpreting the heritage of the United States, as designated by the Secretary of the Interior pursuant to the Historic Sites Act of 1935 (16 U.S.C. §461–467) and the National Historic Preservation Act.

**National Historic Preservation Act:** The National Historic Preservation Act of 1966, as amended (80 Stat. 915 et seq.; 16 U.S.C. 470 et seq.).

**National Register of Historic Places (National Register or NRHP):** The nation’s official record of buildings, sites, structures, objects, and districts that are significant to American history, architecture, archeology, engineering, and culture, as maintained by the Secretary of the Interior pursuant to § 101 of the National Historic Preservation Act.

**Non-contributing Building, Structure, or Site:** A building, structure, or site that does not reflect the historic, architectural, or cultural significance of the historic district within which it is located.

The following considerations apply when:

1. Determining whether a property meets the definition of a non-contributing building, structure, or site; and

2. Determining whether a property or site can be subject to compatible alterations and additions or be replaced in whole or in part through compatible new development:
  - a. A building, structure or site has been identified as non-contributing to a district by virtue of irretrievable loss of integrity of location, design, setting, materials, workmanship, feeling, and association, or a building structure or site within a district that was constructed outside of the identified period of significance for the district.
  - b. An archeological site does not contribute to the archeological values for which a district is significant if it postdates the district's period of significance, does not relate to its documented significance, or is no longer capable of yielding important information about the period due to disturbance or other changes.

**Period of Significance:** The span of time in which a historic property attained the significance for which it has been determined to meet established criteria for historic site or historic districts designation.

**Prince George's County Inventory of Historic Resources:** The official list of those historic properties subject to provisions of Subtitle 29, The Prince George's County Historic Preservation Ordinance. The Inventory of Historic Resources is maintained by the county's Historic Preservation Commission and its staff in accordance with the requirements of Subtitle 29.

**Principal Façade:** A building elevation that faces a street or public open space, or any other major building elevation that possesses significant architectural composition or features. For example, a mid-block rowhouse has a single principal façade (the front), while a corner rowhouse has two principal façades (the front and side). On a freestanding building, based on the level of architectural detail, all elevations may be considered principal façades.

**Raze:** The destruction or removal of a structure in its entirety (also see "demolition").

**Rehabilitate or Rehabilitation:** The process of returning a property to a state of utility, through repairs or alterations, which makes possible an efficient

contemporary use while preserving those portions and features of the property which are significant to its historic, architectural, and cultural values.

**Replacement-in-Kind:** Replacement of an architectural feature with a feature of the same material that replicates the feature in proportion, appearance, texture, design, detail, and dimensions.

**Restore or Restoration:** The act or process of accurately depicting the form, features, and character of a property as it appeared at a particular period of time, either through the removal of features from other periods, or the reconstruction of missing features, or both.

**Secondary Elevation:** A wall of a building that does not face a street or public open space, and that does not possess significant architectural composition or features. Typically, the rear or alley side of a rowhouse is considered a secondary elevation. The sides and rear of a freestanding building may or may not be considered secondary elevations, depending on their function and architectural detail.

**Secretary's Standards:** *The Secretary of the Interior's Standards* (36 CFR § 67.7, as amended) are a set of technical statements and guidelines developed and promulgated by the National Park Service (Department of the Interior) to address a range of issues affecting historic properties.

**Special Merit:** A plan or building having significant benefits to the city or to the community by virtue of exemplary architecture, specific features of land planning, or social or other benefits having a high priority for community services.

**State Historic Preservation Officer (SHPO):** An individual designated by a governor to administer a state's federally and locally mandated historic preservation responsibilities and programs; the administrative head of a State Historic Preservation Office.

**Structure:** Anything constructed, including a building, which requires fixed location on the ground, or is attached to something having a fixed location on the ground.



## M-U-TC Building and Property Inventory



The M-U-TC Building and Property Inventory lists the number of stories, the built square footage, lot square footage, current use, year built, and repair/rehabilitation and development recommendations for each building and/or property within the M-U-TC boundary area.

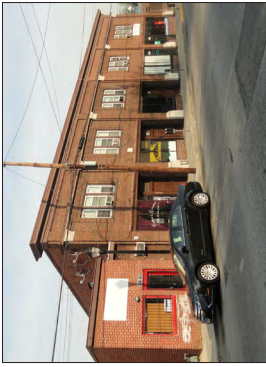



**M-U-TC Building and Property Inventory—Upper 34th Street**

Notes:  
 DS = design standards  
 HA = historically appropriate  
 HP = historic preservation

ADDRESS	PHOTOGRAPH	NUMBER OF STORIES	BUILDING SQUARE FOOTAGE	LOT SQUARE FOOTAGE	CURRENT USE	YEAR BUILT	REPAIR/REHABILITATION AND DEVELOPMENT RECOMMENDATIONS
4010 34th Street		1	1,049	7,667	Commercial	1910	Provide new storefront, doors windows. Provide HA/DS lighting, signage and/or sign band; repair, point brick as required. Remove incompatible front addition and repair existing wood frame building in the medium term.
4008 34th Street		1	1,950	3,964	Commercial	1910	Restore clerestory windows above storefront. Repair existing or provide new HA storefront, doors windows. Provide HA/DS lighting, signage and/or sign band. Repair or replace cornice, coping. Repair and point brick as required.
4006 34th Street		1	2,184	3,790	Commercial	1910	Restore clerestory windows above storefront. Repair existing or provide new HA storefront, doors windows; provide HA/DS lighting, signage and/or sign band. Repair or replace cornice, coping. Repair and point brick as required.
4000–4002 34th Street		1	4,392	12,937	Commercial	1910	Restore clerestory windows above storefront. Provide new HA storefront, doors, and windows. Provide HA/DS lighting. Relocate/redesign awning/signage and/or sign band. Repair and point brick as required.

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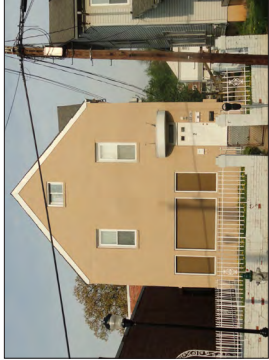
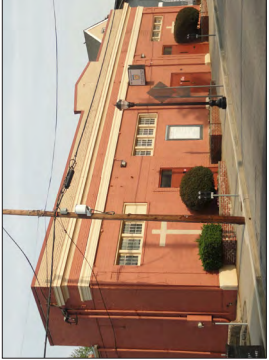


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ADDRESS	PHOTOGRAPH	NUMBER OF STORIES	BUILDING SQUARE FOOTAGE	LOT SQUARE FOOTAGE	CURRENT USE	YEAR BUILT	REPAIR/REHABILITATION AND DEVELOPMENT RECOMMENDATIONS
3840–3456 34th Street		2	9,760	5,489	Commercial	1927	Provide new HA storefront, doors, and windows. Provide HA/DS lighting, signage and/or sign band. Repair/replace cornice, coping. Replace second floor windows with HA windows. Repair and point brick as required.
3838 34th Street		3	1,568	3,790	Residential	1980	Repair wood elements and porch. Repair gutters and downspouts as required. Repair siding. Replace windows and doors with HA elements.
3830 34th Street		3	2,737	6,011	Commercial	1910	Repair wood elements. Repair gutters and downspouts as required. Repair siding. Replace windows and doors with HA elements. Replace storefront with DS and HA elements. Remove and replace all non-contributing elements (bay finishes, doors, etc.) with HA elements.
3828 34th Street		3	1,152	6,098	Residential/ Institutional	1922	Repair wood elements. Repair gutters and downspouts as required. Repair siding. Replace windows and doors with HA elements. Replace storefront with DS elements.



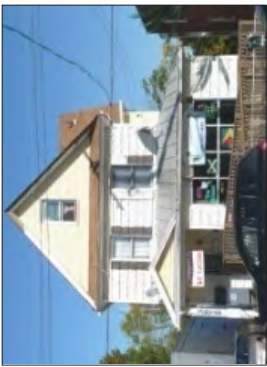



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3824 34th Street		3	3,107	5,271	Commercial/ Institutional	1913	Repair wood elements. Repair gutters and downspouts as required. Remove non-contributing elements at façade. Replace windows and doors with HA elements. Replace storefront with DS elements.
3820–3824 34th Street		2	6,624/ 1,540	9,017/ 3,006	Commercial/ Institutional	1920/ 1940	Restore façade. Replace windows and doors with HA elements; provide DS consistent signage if any; repair and point brick as required.
Vacant Lot between 3817 and 3827 34th Street		N/A	0	4,748	Vacant Land	N/A	Provide DS fencing at perimeter. Two to three-story development is possible in the long term.
3827 34th Street		2	3,150	6,142	Commercial/ Apartments	1939	Repair or replace windows and doors with HA elements. Repair brick/stone. Repair trim bands. Replace/restore HA awning at entry and cornice at entry. Repair coping. Two- to three-story future compatible addition or replacement building possible in the long term.

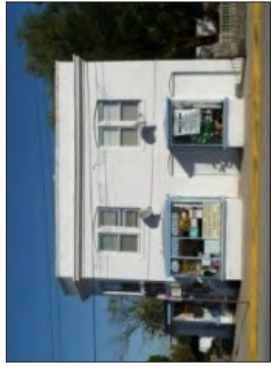



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3829 34th Street		3	2,530	6,839	Commercial	1920	Add new historically-compatible front addition to expand interior retail space. Remove existing ramp and install more functional new one. Remove vinyl siding. Provide historically compatible signage. Repair wood elements.
3833 34th Street		2	3,666	6,665	Commercial	1920	Provide DS consistent signage, lighting in the short term; Two to three-story development possible in the long term.
3837 34th Street		3	1,890	6,186	Residential	1905	Repair wood elements. Replace windows and doors as required with HA elements. Replace gutters and down spouts as required.
3841 34th Street		N/A	0	5,619	Vacant Land/ Parking Lot	N/A	Provide DS fencing at perimeter. May be a shared parking location in the short term. Two to three-story development possible in the long term.

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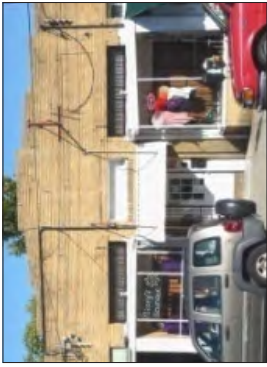


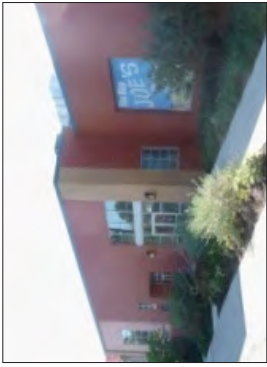
**M-U-TC Building and Property Inventory—Upper 34th Street**

ADDRESS	PHOTOGRAPH	NUMBER OF STORIES	BUILDING SQUARE FOOTAGE	LOT SQUARE FOOTAGE	CURRENT USE	YEAR BUILT	REPAIR/REHABILITATION AND DEVELOPMENT RECOMMENDATIONS
3401 Bunker Hill Road		2	4,720	8,712	Commercial	1912	Provide new HA storefront, doors, and windows. Provide HA/DS lighting, signage and/or sign band. Repair and point brick as required in the short term. Remove incompatible front addition and repair existing wood frame/masonry building in the medium term. Re-install HA porch, roof, and window elements.
3400 Bunker Hill Road		2	1,356	3,572	Residential	Unknown	Repair wood elements. Replace windows and doors as required with HA elements.
4001 34th Street		1	1,482	2,047	Commercial	1910	Paint and provide DS consistent signage. Repair storefront elements. Provide consistent awning design; Provide outdoor displays if possible. Repair brick. See notes at 4009 34th Street for restoration and/or replication of bayfront elements if existing or proposed.
4005 34th Street		1	1,300	1,612	Commercial	1915	Paint and provide DS consistent signage. Repair storefront elements. Provide consistent awning design. Provide outdoor displays if possible. Repair brick. See notes at 4009 34th Street for restoration and/or replication of bayfront elements if existing or proposed.




M-U-TC Building and Property Inventory—Upper 34th Street

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4007 34th Street		1	1,300	1,612	Commercial	1915	Paint and provide DS consistent signage. See notes at 4009 34th Street for restoration and/or replication of bayfront elements if existing or proposed.
4009 34th Street		1	1,300	1,612	Commercial	1937	Building should serve as a model for the "restoration" of bayfront elements. Paint and provide DS consistent signage.
3303 Bunker Hill Road		N/A	0	6,098	Vacant Land/ Parking Lot	N/A	Parking for recently rehabilitated/ constructed arts venue (should remain).
3305 - 3309 Bunker Hill Road		2	3,360 16,798	4,661/ 8,756	Commercial Art	1959/ 1964	Recently rehabilitated arts venue with no short term recommendations.



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3321 Bunker Hill Road		2	3,566	4,095	Commercial/ Residential	1905	Repair masonry, wood elements. Replace windows/doors as required with HA elements. Two- to three-story development possible in the long term.

M-U-TC Building and Property Inventory—Rhode Island Avenue

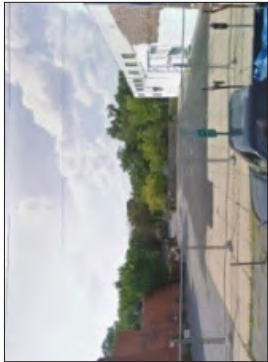


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3703-3705 Eastern Avenue		N/A	0	4,748/ 4,356	Vacant Land/ Parking Lot	N/A	Provide perimeter DS fencing along the perimeter in the short term. Three- to four-story development possible in the short to medium term.
3701 Eastern Avenue		2	864	4,792	Commercial/ Funeral Home	1917	Provide DS fencing along the perimeter in the short term. Repair deteriorated wood elements. Confirm historic integrity/contributing nature. Three- to four-story development possible in the short to medium term.
3200 Rhode Island Avenue		2	6,560	5,837	Commercial/ Vacant Funeral Home	1920	Provide DS fencing along the perimeter in the short term. Three- to four-story development possible in the short to medium term with up to five-stories at the corner of the intersection of Rhode Island Avenue and Eastern Avenue.
3204 Rhode Island Avenue		N/A	0	4,356	Vacant Land/ Parking Lot	N/A	Provide DS fencing along the perimeter in the short term. Three- to four-story development possible in the short to medium term.




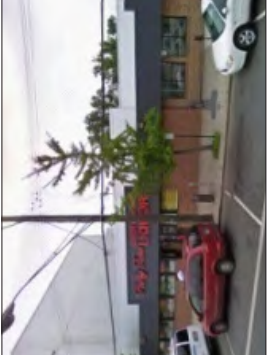


**M-U-TC Building and Property Inventory—Rhode Island Avenue**

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ADDRESS	PHOTOGRAPH	NUMBER OF STORIES	BUILDING SQUARE FOOTAGE	LOT SQUARE FOOTAGE	CURRENT USE	YEAR BUILT	REPAIR/REHABILITATION AND DEVELOPMENT RECOMMENDATIONS
3208 Rhode Island Avenue		N/A	0	8,756	Vacant Land/ Parking Lot	N/A	Provide DS fencing along the perimeter in the short term. Three- to four-story development possible in the short to medium term.
3210 Rhode Island Avenue		2	8,910	4,356	Commercial	1940	Repair/replace windows as required; repair parapet cap; provide compatible (period) doors; provide DS signage, lighting and/or awning in the short term; three- to four-story compatible development (additions or replacements) possible in the short to medium term.
3212 Rhode Island Avenue		N/A	0	8,494	Vacant Land/ Parking Lot	N/A	Provide DS fencing along the perimeter in the short term. Three- to four-story development possible in the short to medium term.
Vacant Lot between 3212 and 3220 Rhode Island Avenue		N/A	0	2,483	Vacant Land/ Parking Lot	N/A	Provide DS fencing along the perimeter in the short term. Three- to four-story development possible in the short to medium term.





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3220 Rhode Island Avenue		1	520	5,401	Commercial/ Vacant Gas Station	1934	Immediate stabilization required. Research/document original condition: roof and steel beam at porte cochere. Repair clay tile roof. Brick restoration required. Window and door replacement restoration required. Repair frieze. Adjacent development should be carefully analyzed. Storm drain needs to added at alley entry and regraded as necessary.
3301 Rhode Island Ave		1	4,280	4,835	Commercial	1950	Provide DS signage and lighting. Provide awning. Paint or remove vertical aluminum siding at roof. Compatible addition to three- to four-stories or new development at three- to four-stories possible in the long term.
3249 Rhode Island Ave		1	7,600	9,148	Commercial	1940	Recently rehabilitated. Provide DS lighting. Historically compatible one-story roof top development that is set back is possible in the long term.
3239 Rhode Island Ave		2	8,000	9,148	Commercial	1965	Repair or replace storefront elements. Establish consistent signage location. Provide awning and repair or replace upper level windows. Remove closed grille. Provide DS lighting. Remove siding. One-story roof top or three- to four-story replacement development possible in the long term.

## M-U-TC Building and Property Inventory—Rhode Island Avenue




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3233-3237 Rhode Island Ave		2	7,000	7,797	Commercial	1940	Repair or replace storefront elements. Establish consistent signage location. Provide awning and repair or replace upper level windows. Remove closed grille. Provide DS lighting. Remove siding. One-story roof top addition or three to four-story replacement development possible in the long term.
3231 Rhode Island Ave		1	2,010	6,403	Commercial	1920	Repair or replace storefront elements. Establish consistent signage location. Provide awning. Remove closed grille. Provide DS lighting. Two-story roof top or three- to four-story replacement development possible in the long term.
3225 Rhode Island Avenue		2	2,400	2,657	Commercial	1925	Point brick. Replace coping, gutters and down spouts. Locate coordinated sign band. Replace awning to one compatible with DS. Provide period compatible (brushed natural) aluminum storefront system. Provide DS exterior lighting. One-story addition/development possible in the long term.
3221 Rhode Island Avenue		1	816	2,352	Commercial	1920	Point brick. Replace coping, gutters and down spouts. Locate coordinated sign band. Replace awning to one compatible with DS. Provide period compatible (brushed natural) aluminum storefront system. Provide DS exterior lighting. One- to two-story addition/development possible in the long term.





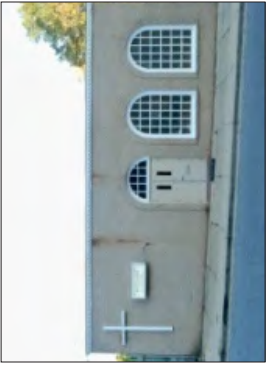

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# M-U-TC Building and Property Inventory—Rhode Island Avenue

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3201 Rhode Island Avenue		3	34,133	13,634	Commercial/ Apartments	1938	Replace vinyl windows with uniform wood/metal. Point brick. Replace coping, gutters and down spouts. Locate coordinated sign band. Repair/restore brushed aluminum awning or introduce a consistent awning. Remove grilles. Provide exterior lighting. Provide new storefronts of brushed/natural aluminum. Provide proper drainage at rear.
3206 Otis Street		2	1,554	2,744	Commercial/ Residential	1929	Provide DS fencing. Repair deteriorated wood elements. Replacement windows should be to DS (wood); Confirm historic integrity/contributing nature.
3623 Eastern Avenue		2	1,623	6,752	Institutional	1930	Provide DS fencing. Repair deteriorated wood elements. Confirm historic integrity/contributing nature; Two to three-story development possible in the long term.





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### M-U-TC Building and Property Inventory—Civic Core

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3311 Rhode Island Avenue		4	68,441	39,901	Commercial/ Apartments	2005	Consistent signage, wayfinding and lighting required.
3721 34th Street		N/A	0	5,489	Vacant Land/ Church Parking Lot	N/A	Shared parking lot in the short term. Two- to three-story development possible in the long term.
3723 34th Street		1	3,600	5,489	Institutional	1948	Two- to three-story development possible in the long term.
3401 Perry Street		1	1,875	3,746	Institutional/ Store Front	1930	Repair storefront elements. Provide coordinated signage. Point brick. Restore or repair awning. Provide lighting.

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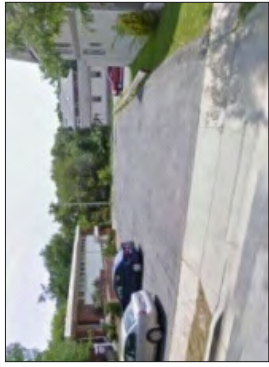

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3403 Perry Street		2	2,006	2,134	Commercial	1900	If it is a commercial structure provide consistent signage. Paint and repair deteriorated wood or masonry elements.; Replace windows and doors as required with HA elements. Two- to three-story replacement building/complex possible in the medium to long term.
3407 Perry Street		2	1,176	5,576	Commercial	1918	If it is a commercial structure provide consistent signage. Paint and repair deteriorated wood or masonry elements. Replace windows and doors as required with HA elements. Two- to three-story replacement building or complex possible in the medium to long term.
3409 Perry Street		2	2,372	5,881	Residential	1920	If it is a commercial structure provide consistent signage. Paint and repair deteriorated wood or masonry elements; replace windows and doors as required with HA elements. Remove incompatible front addition and install redesigned compliant front addition. Two- to three-story replacement building or complex possible in the medium to long term.
3411 Perry Street		N/A	0	5,358	Commercial	N/A	If it is a commercial structure provide consistent signage. Paint and repair deteriorated wood or masonry elements. Replace windows and doors as required with HA elements. Two- to three-story replacement building or complex possible in the medium to long term.







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3415 Perry Street		3	2,276	6,360	Commercial	1930	If it is a commercial structure provide consistent signage. Paint and repair deteriorated wood or masonry elements. Replace windows and doors as required with HA elements. Two- to three-story replacement building or complex possible in the medium to long term.
3501 Perry Street		2	3,328	6,229	Commercial	1910	If it is a commercial structure provide consistent signage. Paint and repair deteriorated wood or masonry elements. Replace windows and doors as required with HA elements. Two- to three-story replacement building or complex possible in the medium to long term.
3503 Perry Street		N/A	0	6,229	Vacant/Parking Lot	N/A	If it is a commercial structure provide consistent signage. Paint and repair deteriorated wood or masonry elements. Replace windows and doors as required with HA elements. Two- to three-story replacement building or complex possible in the medium to long term.
3505 Perry Street		1	3,966	6,229	Commercial/Medial	1964	If it is a commercial structure provide consistent signage. Paint and repair deteriorated wood or masonry elements. Replace windows and doors as required with HA elements. Two- to three-story replacement building or complex possible in the medium to long term.

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Bus Transit Plaza Area		N/A	0	2,528	Vacant/Bus Stop	N/A	Remove transit related activities this area and relocate. Redevelop as park or open space for civic core.
Bus Transit Plaza		N/A	0	392	Bus Transit Plaza	N/A	Remove transit related activities this area and relocate. Redevelop as park or open space for civic core.
1 Municipal Place		3	2,901	3,006	Public/City Hall		Coordinate façade and entry with new park or open space elements. Coordinate entry, lighting and wayfinding with future civic core additions and open space elements. Additions or development possible in the medium to long term.
3405 Rhode Island Avenue		N/A	4,088	2,614	Commercial/Civic	1902	Redesign Rhode Island Avenue façade. As a restoration project, research and restore original façade elements. As an alternate, redesign or provide compatible addition due to current lack of integrity. Additions or development possible in the near term.

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3409 Rhode Island Avenue		2	4,040	5,619	Public Library	1915	Confirm the contributing nature of the library site. In the short term, provide DS compatible awning and signage. Replace windows and doors with historically appropriate (HA) storefront framing. Repair or replace planter if not HA. Provide DS consistent lighting. Compatible two to three-story addition possible in the medium term.
3228 Rhode Island Avenue		N/A	0	6,534	Vacant/Parking Lot	N/A	Repair wall as required and provide DS consistent fence at upper level of wall as screen in the short term. Potential location for three to four-story infill parking lot at the rear of the site in the medium term.
3424 Rhode Island Avenue		N/A	0	6,142	Vacant/Parking Lot	N/A	Repair wall as required and provide DS consistent fence at upper level of wall as screen in the short term. Potential location for three to four-story infill parking lot at the rear of the site in the medium term.
3416 Rhode Island Avenue		2	2,300	2,483	Commercial	1935	Provide new storefront, doors, and windows. Provide HA/DS lighting, signage and/or sign band. Remove defunct signage. Replace second floor windows with HA windows. Point brick as required. Replace gutters and downspout.



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3414 Rhode Island Avenue		2	2,392	2,091	Commercial/ Residential	1927	Remove siding. Expose brick if existing; repair trim work and freize. Confirm historicity of bays. If bays original, require HA elements at street level. If bays are not original, require compatible elements at street level. Replace windows and doors with HA elements; provide DS consistent signage and lighting.
3402–3412 Rhode Island Avenue		2	6,004	4,879	Commercial	1935	Provide new storefront, doors, and windows. Provide HA/DS lighting, signage, and sign band. Repair or replace cornice and coping. Replace second floor windows with HA. Repair and point brick as required.
3801 34th Street		2	2,016	2,396	Commercial	1900	Repair and refurbish windows, doors, and brickwork. Provide DS consistent signage.
3805 34th Street		N/A	0	871	Vacant Land	N/A	Compatible infill development site with a maximum height of three-stories.


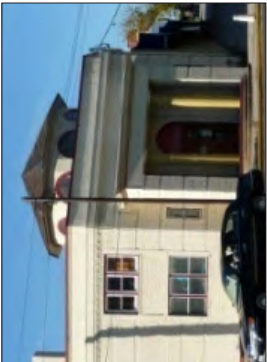


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### M-U-TC Building and Property Inventory—Civic Core

ADDRESS	PHOTOGRAPH	NUMBER OF STORIES	BUILDING SQUARE FOOTAGE	LOT SQUARE FOOTAGE	CURRENT USE	YEAR BUILT	REPAIR/REHABILITATION AND DEVELOPMENT RECOMMENDATIONS
3807–3809 34th Street		2	1,900	1,220	Commercial	1905	Repair or refurbish bays. Paint and repair trim elements. Provide DS consistent signage and lighting. Remove incompatible elements, modern bow front, siding (expose brick if existing); repair and replace cornices and trim bands as required; no compatible additions recommended.
3813–3815 34th Street		2	2,264	1,655	Commercial	1910	Repair and refurbish bays; paint and repair trim elements; provide DS consistent signage and lighting; remove incompatible elements, modern bow front, siding (expose brick if existing); repair and replace cornices and trim bands as required; no compatible additions recommended.
3817 34th Street		2	2,737	1,742	Commercial	1915	Repair and refurbish bays; paint and repair trim elements; provide DS consistent signage and lighting; Remove incompatible elements such as the modern bow front, siding etc. (expose brick if existing). Repair and replace cornices and trim bands as required.
Vacant lot between 3812 and 3820 34th Street		N/A	0	5,787	Vacant/Parking Lot		Potential opportunity for shared parking lot.

Notes:  
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3802-3812 34th Street		1	6,481	17,860	Commercial	1910	Provide DS consistent signage, lighting and awning. Repair storefront as required. Two to three-story compatible addition or replacement building possible.
3800 34th Street		2	3,024	1,699	Commercial (Vacant)	1922	Repair or replace windows and doors with HA elements. Repair brick and stone. Repair trim bands. Replace or restore HA cornice at entry.
3316 Perry Street		3	1,916	6,011	Commercial	1902	Repair wood elements. Replace windows and doors as required with HA elements. Compatible replacement building possible in the long term.
3314 Perry Street		2	1,819	3,398	Residential	1902	Repair wood elements. Replace windows and doors as required with HA elements. Compatible replacement building possible in the long term.







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ADDRESS	PHOTOGRAPH	NUMBER OF STORIES	BUILDING SQUARE FOOTAGE	LOT SQUARE FOOTAGE	CURRENT USE	YEAR BUILT	REPAIR/REHABILITATION AND DEVELOPMENT RECOMMENDATIONS
3306–3312 Perry Street		2	3,322	6,011	Commercial	1920	Repair masonry and wood elements. Replace windows and doors as required with HA elements.
3304 Perry Street		3	1,320	6,011	Residential	1922	Repair wood elements. Replace windows and doors as required with HA elements.
3300 Perry Street		3	N/A	6,011	Residential	N/A	No development recommendations for recently rehabilitated apartment building.
3803–3805 33rd Street		3	5,000	7,492	Residential	N/A	Repair wood elements. Replace windows and doors as required with HA elements.


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ADDRESS	PHOTOGRAPH	NUMBER OF STORIES	BUILDING SQUARE FOOTAGE	LOT SQUARE FOOTAGE	CURRENT USE	YEAR BUILT	REPAIR/REHABILITATION AND DEVELOPMENT RECOMMENDATIONS
Vacant Lot east of 3210 Perry Street		N/A	0	7,797	Vacant	N/A	May be utilized as a community park or as two- to three-story infill residential development.
Vacant Lot east of 3208 Perry Street		N/A	0	4,182	Vacant Land	N/A	May be utilized as a community park or as two- to three-story infill residential development.
3300 Rhode Island Avenue		3	11,849	5,881	Commercial/ Vacant Apartment Building	1936	Repair or replace windows and doors with consistent brushed aluminum (or historically accurate) metal windows. Clean, point and repair brick. Repair stone cap. Resolve front drainage issues. Locate a sign band area. Provide lighting. One-story roof top addition or development is possible in the long term.
3308 Rhode Island Avenue		2	2,200	1,481	Commercial	1925	Repair or restore stucco. Replace gutters and downspouts; repair clay tile roofing. Locate coordinated sign band or awning element. Replace windows and doors with historically accurate elements One-story roof top addition/development possible in the long term.

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### M-U-TC Building and Property Inventory—Civic Core

ADDRESS	PHOTOGRAPH	NUMBER OF STORIES	BUILDING SQUARE FOOTAGE	LOT SQUARE FOOTAGE	CURRENT USE	YEAR BUILT	REPAIR/REHABILITATION AND DEVELOPMENT RECOMMENDATIONS
3310 Rhode Island Avenue		1	1,650	1,525	Commercial	1966	Document façades. Replace original cornice and trim at entry. Repair stairs and replace windows and doors with historically accurate elements. Addition and development are possible in the long term.





# Acknowledgments

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## **SPECIAL THANKS TO:**

Mount Rainier M-U-TC Design Review Committee  
Mount Rainier Design Review Board  
Mount Rainier Business Association  
Bunker Hill Fire Station  
Gateway CDC  
Neighborhood Design Center  
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Bolan Smart Associates

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\*Former employee



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